

# SEATTLE CITY COUNCIL

## Council Briefing

**December 15, 2025 - 2:00 PM**

### **Meeting Location:**

Council Chamber, City Hall, 600 4th Avenue, Seattle, WA 98104

### **Committee Website:**

<http://www.seattle.gov/council/>

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Only written public comment will be accepted at this meeting. Please submitted written comments prior to 10 a.m. to ensure that they are distributed to Councilmembers prior to the start of the meeting. Comments may be submitted at [Council@seattle.gov](mailto:Council@seattle.gov) or at Seattle City Hall, Attn: Council Public Comment, 600 4th Ave., Floor 2, Seattle, WA 98104. Comments received after 10 a.m. will be distributed after the meeting to Councilmembers and included as part of the public record.

Please Note: Times listed are estimated.

### **1. Approval of the Minutes**

#### **Council Briefing Minutes (2025)**

[December 8, 2025](#)

### **2. President's Report**

2:00 p.m. - 2:05 p.m.

**3. Presentation on Council Bill 120985, Council Bill 120993, and Resolution 32183 relating to the Comprehensive Plan**

2:05 p.m. - 2:35 p.m.

**Presenter:** Lish Whitson, Council Central Staff

[CB 120985](#)

[Amendment B to CB 120985](#)

[CB 120993](#)

[Amendment A to CB 120993](#)

[Res 32183](#)

**4. Signing of Letters and Proclamations**

2:35 p.m. - 2:40 p.m.

**5. Preview of City Council Actions, Council and Regional Committees**

2:40 p.m. - 3:30 p.m.

**City Council Agenda (2025)**

[Preliminary December 16, 2025, Agenda](#)



Legislation Text

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**File #:** Inf 2609, **Version:** 1

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Council Briefing Minutes (2025)



Council Chamber, City Hall  
600 4th Avenue  
Seattle, WA 98104

# Seattle Council Committee Report Council Briefing

Monday, December 8, 2025, 2:00 PM

**Meeting Start Time:** 2:03 p.m.

**Presiding Officer:** Council President Nelson

**Present:** 9 - Sara Nelson, Joy Hollingsworth, Debora Juarez, Robert Kettle, Eddie Lin, Alexis Mercedes Rinck, Maritza Rivera, Rob Saka, Dan Strauss

## 1. Approval of the Minutes

**Inf 2609** Council Briefing Minutes (2025)

The December 1, 2025, Council Briefing Minutes were approved.

## 2. President's Report

3. **Inf 2808** Discussion on Proposed Amendments to Resolution 32187 relating to the 2026 State Legislative Agenda

The Information Item (Inf) was heard in Committee.

4. **Inf 2809** Presentation on Council Bills 121132 and 121133 relating to Collective Bargaining Agreements with Seattle Police Management Association (SPMA) and Seattle Police Officers' Guild (SPOG)

The Information Item (Inf) was heard in Committee.

## 5. Signing of Letters and Proclamations

## 6. Preview of City Council Actions, Council and Regional Committees

**Inf 2610** City Council Agenda (2025)

The Information Item (Inf) was not heard in Committee.

7. Executive Session I on Pending, Potential, or Actual Litigation\*

8. Executive Session II on Pending, Potential, or Actual Litigation\*

9. Executive Session III on Pending, Potential, or Actual Litigation\*

*\*Executive Sessions are closed to the public*

At 4:39 p.m., Council President Nelson announced that the Council would convene in Executive Session to discuss pending, potential, or actual litigation with an estimated end time of 7:25 p.m. The Executive Session concluded at 6:49 p.m.

**Meeting Adjournment Time:** 6:49 p.m.

**Prepared by:** Phillip Wood-Smith, Deputy City Clerk



Legislation Text

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**File #:** Inf 2812, **Version:** 1

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Presentation on Council Bill 120985, Council Bill 120993, and Resolution 32183 relating to the Comprehensive Plan

Amendment B Version #1 to CB 120985 OPCD One Seattle Plan Comprehensive Plan Update  
ORD

**Sponsor:** Councilmember Hollingsworth

Amend the Comprehensive Plan to clarify policies on pavement and bridge condition

**Effect:** This amendment would amend two new policies in the Comprehensive Plan’s Transportation Element related to pavement and bridge condition. The amendments would recognize that the availability of funding may place constraints on the City’s ability to meet the standards listed.

Amend Attachment 1 to Council Bill 120985 to amend policies T 9.5 and T 9.6 under “Operating and Maintaining the Transportation System” in the Transportation Element as follows:

- T 9.5 Seek to achieve (~~Achieve~~) and maintain an average Pavement Condition Index of 80, at a minimum, for arterial streets, subject to funding availability.
- T 9.6 Seek to achieve (~~Achieve~~) and maintain a Federal Highway Administration Bridge Rating of good for at least 30% of vehicle bridges and a rating of Fair for at least 60% of vehicle bridges, subject to funding availability.

## Amendment A Version #1 to CB 120993 OPCD Permanent State Zoning Compliance ORD

**Sponsor:** Councilmember Hollingsworth

Substitute Bill – Technical Corrections

**Effect:** This amendment would substitute Version 4a for Council Bill 120993 for Version 2, which passed out of the Select Committee on the Comprehensive Plan on September 18. Version 2 incorporated the Committee’s amendments.

Version 4a includes a number of technical corrections and edits to the bill, including:

- Redrafting sections of the code that were amended by multiple amendments so that they are as clear as possible;
- Restoring provisions that were inadvertently deleted, such as continuing to allow Accessory Dwelling Units to be located in rear setbacks;
- Maintaining consistency in language and policy intent across the land use code;
- Updating references to regional centers and urban centers;
- Adding references to new code sections;
- Incorporating changes that were made by bills adopted since the introduction of Council Bill 120993; and
- Correcting typos.

In particular, the substitute would:

1. Increase the new family housing bonus in Neighborhood Residential (NR) zones to be consistent with other FAR bonuses. With all of the different changes to the Floor Area Ratio section, it ended up that family housing would have a lower FAR limit than other housing types, which was the opposite of the intention of that provision. (Section 23.44.050)
2. Allow pitched roofs and other rooftop features to extend above the roof for projects in NR zones where 42 foot roof heights are allowed. Currently these features are allowed above 32 foot roofs. Version 4a would extend that longstanding provision to the higher height limits that the Committee’s bill allows. (Section 23.44.070)
3. Limit the flexibility that will be provided to projects that include Type A accessible units to projects with less than 10 stacked dwelling units. Projects with 10 or more units are already required to have ADA-compliant units. (Section 23.44.080)
4. Add setback limits that would apply to nonresidential structures, like churches or schools, in NR zones. (Section 23.44.090)
5. Amend the setback limits to allow detached ADUs within 5 feet of the rear lot line in NR zones, consistent with current regulations. (Section 23.44.090)



Lish Whitson  
City Council  
December 12, 2025  
D#1

6. In Lowrise (LR) zones, allow “corner stores” to be located on any lot, consistent with the Committee’s direction for NR zones (Section 23.44.110)

Version 4a of Council Bill 120993 is attached. Changes included in Version 3a, which was published for public comment on November 22, are in red. Additional, minor changes, incorporated in Version 4a are in purple.

Attachment 1 is a table listing each of the changes incorporated into Version 4a of the bill.

Substitute Version 4a of Council Bill 120993 for Version 2, as shown on the following pages.

**CITY OF SEATTLE**

**ORDINANCE \_\_\_\_\_**

**COUNCIL BILL \_\_\_\_\_**

..title

AN ORDINANCE relating to land use and zoning; implementing a major update of Neighborhood Residential zones and modifying development standards in other zones to comply with various state laws; amending Chapter 23.32 of the Seattle Municipal Code at pages 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 98, 99, 100, 102, 103, 104, 105, 106, 107, 111, 112, 113, 114, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 216, 217, 219, 220, and 221 of the Official Land Use Map; amending Chapters 6.600, 14.08, 14.09, 15.32, 21.49, 22.214, 22.801, 22.907, 23.22, 23.24, 23.28, 23.30, 23.34, 23.42, 23.45, 23.47A, 23.48, 23.49, 23.50, 23.51A, 23.51B, 23.53, 23.54, 23.58C, 23.60A, 23.66, 23.72, 23.75, 23.76, 23.80, 23.84A, 23.86, 23.90, 23.91, 25.09, and 25.11 of the Seattle Municipal Code; renumbering existing subsection 23.54.015.K of the Seattle Municipal Code as Section 23.54.037 and further amending the section; renumbering existing subsections 23.54.030.F, 23.54.030.G, 23.54.030.K, and 23.54.030.L as Sections 23.54.031, 23.54.032, 23.54.033, and 23.54.034 and further amending the sections; repealing Chapter 23.44 and Sections 23.34.010, 23.34.012, 23.34.013, 23.34.072, 23.42.130, 23.45.512, 23.45.531, 23.86.010, and 25.09.260 of the Seattle Municipal Code; adding a new Chapter 23.44 and new Sections 23.42.024, 23.42.047, 23.42.132, 23.45.519, 23.45.560, ~~23.80.006~~, ~~23.80.008~~, 23.80.010, 23.80.012, 23.80.014, 25.09.055, and 25.11.025 to the Seattle Municipal Code; and repealing Ordinance 127219.

..body

WHEREAS, the Office of Planning and Community Development, in cooperation with other City agencies including the Seattle Planning Commission, began in 2022 a series of programs and events, under the title One Seattle Plan, to engage the public in discussions about potential changes to the Comprehensive Plan, consistent with the One Seattle Plan Public Participation Plan and documented in the One Seattle Plan Public Engagement Report; and

1 WHEREAS, in April 2021, the Washington State Legislature passed Chapter 300, Laws of 2021  
2 (also known as House Bill 1287), which directed the Building Code Council to adopt  
3 rules for electric vehicle infrastructure requirements; and

4 WHEREAS, the Office of Planning and Community Development held a scoping period for the  
5 Environmental Impact Statement from June 23 to August 22, 2022; and

6 WHEREAS, in April 2023, the Washington State Legislature passed Chapter 322, Laws of 2023  
7 (also known as House Bill 1110), which amended the Growth Management Act to require  
8 certain cities, including Seattle, to allow the development of “middle housing” in all  
9 residential areas, including at least four units on each lot and at least six units per lot near  
10 transit or when at least two units are affordable; and

11 WHEREAS, in April 2023, the Washington State Legislature passed Chapter 333, Laws of 2023  
12 (also known as House Bill 1293), which imposes limits on design review and requires  
13 that design standards be clear and objective; and

14 WHEREAS, in April 2023, the Washington State Legislature passed Chapter 334, Laws of 2023  
15 (also known as House Bill 1337), which requires cities to remove regulatory barriers to  
16 accessory dwelling units; and

17 WHEREAS, in March 2024, the Washington State Legislature passed Chapter 152, Laws of  
18 2024 (also known as House Bill 2321), which clarified standards implemented through  
19 House Bill 1110; and

20 WHEREAS, in March 2024, the Washington State Legislature passed Chapter 274, Laws of  
21 2024 (also known as Senate Bill 6015), which imposes restrictions on parking  
22 requirements; and

1 WHEREAS, in March 2024, the Office of Planning and Community Development published a  
2 Draft Environmental Impact Statement analyzing the potential effects of five different  
3 growth alternatives in the city through 2044 and a “no action” alternative, conducted two  
4 public hearings, and received comments from the public on this document; and

5 WHEREAS, in March 2024, the Office of Planning and Community Development published a  
6 Draft Comprehensive Plan rooted in a deliberate approach to creating more housing,  
7 encouraging density near amenities and frequent transit, and preventing displacement;  
8 and

9 WHEREAS, in Spring 2024, the Office of Planning and Community Development held open  
10 houses across all seven council districts and received input from residents and community  
11 groups over a two-month public comment period on the draft plan and an initial proposal  
12 for updating Neighborhood Residential zones as documented in the One Seattle Plan  
13 Public Engagement Report; and

14 WHEREAS, in Fall 2024, the Office of Planning and Community Development held open  
15 houses across all seven council districts and received input from residents and community  
16 groups over a two-month public comment period on a revised proposal for updating  
17 Neighborhood Residential zones and draft legislation as documented in the One Seattle  
18 Plan Public Engagement Report; and

19 WHEREAS, in January 2025, the Office of Planning and Community Development published a  
20 Final Environmental Impact Statement that included analysis of a preferred growth  
21 strategy alternative that increased potential housing supply in the city by doubling  
22 residential development capacity and that promoted housing supply, variety, and

1           affordability by adding new and expanded areas for growth in neighborhoods across the  
2           city; and

3 WHEREAS, on March 27, 2025, the Office of Planning and Community Development  
4           transmitted legislation to the City Council which would adopt the One Seattle Plan;

5           NOW, THEREFORE,

6 **BE IT ORDAINED BY THE CITY OF SEATTLE AS FOLLOWS:**

7           Section 1. The Official Land Use Map, Chapter 23.32 of the Seattle Municipal Code, is  
8 amended to rezone properties on pages 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18,  
9 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44,  
10 45, 46, 47, 48, 49, 50, 51, 52, 53, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71,  
11 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 98,  
12 99, 100, 102, 103, 104, 105, 106, 107, 111, 112, 113, 114, 117, 118, 119, 120, 121, 122, 123,  
13 124, 125, 126, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 143, 144, 145, 146, 147, 148,  
14 149, 150, 151, 152, 153, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 170,  
15 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 186, 187, 188, 189, 190,  
16 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 203, 204, 205, 206, 207, 208, 209, 210,  
17 211, 212, 213, 214, 216, 217, 219, 220, and 221 of the Official Land Use Map as follows:

18           A. Properties identified for rezones as shown on Attachment 1 to this ordinance are  
19 rezoned as shown in those maps.

20           B. Except for properties identified to be rezoned as shown on Attachment 1 to this  
21 ordinance, all areas identified as “existing zoning” in Table A for Section 1 are rezoned as shown  
22 under the “New zoning” column in Table A for Section 1.



1 Section 3. Section 14.08.020 of the Seattle Municipal Code, last amended by Ordinance  
2 126767, is amended as follows:

3 **14.08.020 Definitions**

4 Definitions as used in this Chapter 14.08, unless additional meaning clearly appears from the  
5 context, shall have the meanings subscribed:

6 "Accessory dwelling unit" has the meaning defined in (~~Chapter 23.84A.032's definition~~  
7 ~~of "Residential use."~~) Section 23.84A.008.

8 \* \* \*

9 (~~"Detached accessory dwelling unit" has the meaning defined in Chapter 23.84A.032's~~  
10 ~~definition of "Residential use".~~)

11 \* \* \*

12 Section 4. Section 14.09.010 of the Seattle Municipal Code, last amended by Ordinance  
13 126080, is amended as follows:

14 **14.09.010 Definitions**

15 "Accessory dwelling unit" has the meaning defined in Section (~~23.84A.032's definition~~  
16 ~~of "Residential use."~~) 23.84A.008.

17 \* \* \*

18 (~~"Detached accessory dwelling unit" has the meaning defined in Section 23.84A.032's~~  
19 ~~definition of "Residential use".~~)

20 \* \* \*

21 "Single family dwelling unit" has the meaning as defined in Section 22.204.200.A.

22 \* \* \*

1 Section 5. Section 15.32.200 of the Seattle Municipal Code, last amended by Ordinance  
2 126509, is amended as follows:

3 **15.32.200 At-grade communication cabinets**

4 \* \* \*

5 F. The applicant for a new at-grade communication cabinet proposal that is more than 36  
6 inches in height including footings or bases as measured from the grade of the surrounding  
7 public place, or has a maximum volume of more than 18 cubic feet, shall: (1) send notice of a  
8 Seattle Department of Transportation application by first-class mail to all business entities,  
9 property owners, and residents located within a 100-foot radius from where the communication  
10 cabinet is proposed to be located; and (2) post notice of the new application at the proposed site.  
11 The notice shall be displayed towards the nearest public place that abuts the site and is viewable  
12 by the public and shall be maintained on the site for the duration of the public notice period.

13 1. If the new at-grade communication cabinet proposal is more than 36 inches in  
14 height including footings or bases as measured from the grade of the surrounding public place, or  
15 has a maximum volume of more than 18 cubic feet, and is abutting a lot zoned (~~NR1, NR2,~~  
16 ~~NR3, RSL,~~) NR, LR1, LR2, or LR3 as these zoning designations are defined under subsection  
17 23.30.010.A and the abutting zoning does not have an RC classification as shown on the Official  
18 Land Use Map, Chapter 23.32 ("residentially zoned parcels"), the communication cabinet shall  
19 be fully screened from the public place and abutting private property. If it is not feasible to install  
20 mitigation screening due to physical site constraints, the applicant shall provide an alternative  
21 mitigation proposal within 200 feet of the project. If the alternative mitigation cannot be located  
22 within 200 feet of the project, the applicant shall propose an alternative location that the Director  
23 shall review and may approve. All mitigation screening shall comply with setback standards in





1 Section 6. Section 15.32.250 of the Seattle Municipal Code, last amended by Ordinance  
2 126732, is amended as follows:

3 **15.32.250 Communication cabinet standards and setbacks**

4 \* \* \*

5 C. If the at-grade communication cabinet is to be installed in a planting strip it shall be  
6 placed in proximity to and in line with existing utility or street light poles, street signs, or other  
7 existing structures within the planting strip in order to create a physical and visual alignment.  
8 The communication cabinet shall not impair the line of sight for vehicles exiting adjacent alleys,  
9 streets, or driveways as provided in (~~subsection 23.54.030.G~~) Section 23.54.032 or other sight  
10 triangle requirements adopted by City code or rule.

11 \* \* \*

12 Section 7. Section 21.49.110 of the Seattle Municipal Code, last amended by Ordinance  
13 125171, is amended as follows:

14 **21.49.110 Electric service connection provisions**

15 \* \* \*

16 E. Prohibition of master metering((-))

17 1. The Department shall not supply electricity for any new service to a duplex or  
18 multiple-dwelling building for the purpose of master metering the energy usage of the dwelling  
19 units, a central space heating system or HVAC system, or a central domestic water heating  
20 system. The Department shall not supply electricity for any larger service to an existing duplex  
21 or multiple-dwelling building for the purpose of master metering new central or individual space  
22 heating or HVAC systems. The existence of alternative laundry or dining arrangements for  
23 residents of multiple-dwelling buildings (such as central kitchens and dining rooms where



1 "Accessory dwelling unit" or "ADU" (~~means an "Accessory dwelling unit" or a~~  
2 ~~"Detached accessory dwelling unit" or "DADU" as~~) has a meaning defined ((under "Residential  
3 ~~use"))~~ in Section ((~~23.84A.032~~) 23.84A.008.

4 \* \* \*

5 Section 9. Section 22.801.200 of the Seattle Municipal Code, last amended by Ordinance  
6 126509, is amended as follows:

7 **22.801.200 "S"**

8 \* \* \*

9 "Sidewalk" means "sidewalk" as defined in Section 23.84A.036.

10 "Sidewalk project" means a project for the creation of a new sidewalk or replacement of  
11 an existing sidewalk, including any associated planting strip, apron, curb ramp, curb, or gutter,  
12 and necessary roadway grading and repair. If the total new plus replaced hard surface in the  
13 roadway exceeds 10,000 square feet, the entire project is a roadway project.

14 "Single-family residential project" means a project that constructs one ((~~Single-family~~  
15 ~~Dwelling Unit~~)) principal detached or attached dwelling unit as defined in ((~~subsection~~  
16 ~~23.84A.032~~) 23.84A.008 and any associated accessory dwelling unit located in land classified  
17 as being Neighborhood Residential ((~~1 (NR1), Neighborhood Residential 2 (NR2), or~~  
18 ~~Neighborhood Residential 3 (NR3)~~)) pursuant to Section 23.30.010, and the total new plus  
19 replaced hard surface is less than 5,000 square feet.

20 \* \* \*

21 Section 10. Section 22.907.030 of the Seattle Municipal Code, last amended by  
22 Ordinance 125873, is amended as follows:

23 **22.907.030 Notice of proposed sale of low-income multi-family rental building**



1 individual (~~(apartment)~~) stacked dwelling units, in all zones in which these uses are permitted, or  
2 any combination of the above types of residential development as permitted in the applicable  
3 zones.

4 \* \* \*

5 Section 12. Section 23.24.045 of the Seattle Municipal Code, last amended by Ordinance  
6 127211, is amended as follows:

7 **23.24.045 Unit lot subdivisions**

8 A. The provisions of this Section 23.24.045 apply exclusively to the unit subdivision of  
9 land for residential development including (~~(single family dwelling units, townhouse, rowhouse,~~  
10 ~~and cottage housing developments,)) attached and detached dwelling units and existing  
11 (~~(apartment)~~) structures containing stacked dwelling units built prior to January 1, 2013, but not  
12 individual (~~(apartment)~~) stacked dwelling units, in all zones in which these uses are permitted, or  
13 any combination of the above types of residential development as permitted in the applicable  
14 zones.~~

15 \* \* \*

16 Section 13. Section 23.28.030 of the Seattle Municipal Code, last amended by Ordinance  
17 126157, is amended as follows:

18 **23.28.030 Criteria for approval**

19 A. The Director shall approve an application for a lot boundary adjustment if it is  
20 determined that:

21 1. No additional lot, tract, parcel, site, or division is created by the proposed  
22 adjustment;

1                   2. No lot contains insufficient area and dimensions to meet the minimum  
2 requirements for development as calculated under the development standards of the zone in  
3 which the lots affected are situated(~~(, except as provided in Section 23.44.010,))~~ and under any  
4 applicable regulations for siting development on parcels with riparian corridors, wetlands,  
5 wetland buffers, or steep slopes in Chapter 25.09 or Section 23.60A.156. Adjusted lots shall  
6 continue to be regarded as existing lots for purposes of Chapter 25.09. Any required  
7 nondisturbance area shall be legibly shown and described on the site plan, and a covenant shall  
8 be required as set out in Section 25.09.335;

9                   3. Every proposed adjusted lot shall conform to the following standards for lot  
10 configuration, unless a modification is authorized under subsection 23.28.030.A.4:

11                   a. If an adjusted lot is proposed with street frontage, then one lot line shall  
12 abut the street for at least 10 feet; (~~and~~)

13                   b. No adjusted lot shall be less than 10 feet wide for a distance of more  
14 than 10 feet as measured at any point; (~~and~~)

15                   c. No adjusted lot shall have more than six separate lot lines. The lot lines  
16 shall be straight lines unless the irregularly shaped lot line is caused by an existing right-of-way  
17 or existing lot line; and

18                   d. If a lot to be adjusted abuts upon an alley, and that alley is either  
19 improved or required to be improved according to the standards of Section 23.53.030, then no  
20 adjusted lot shall be proposed that does not provide alley access, except that access from a street  
21 to an existing use or structure is not required to be changed to alley access. Either the proposed  
22 adjusted lots shall have sufficient frontage on the alley to meet access standards for the zone in

1 which the property is located or an access easement from the adjusted lot or lots shall be  
2 provided to the alley that meets access standards for the zone in which the property is located.

3 4. Modification. The standards of subsection 23.28.030.A.3 may be modified if at  
4 least one of the following criteria applies:

5 a. One or more of the existing lots prior to the lot boundary adjustment is  
6 irregular in shape;

7 b. Topography, natural obstructions, configuration of existing lot lines  
8 prior to lot line adjustment, existing platting patterns, or street alignment prevent the  
9 reconfiguration of one or more lots according to the standards of subsection 23.28.030.A.3;

10 c. Location of existing principal structures that are retained on lots existing  
11 prior to the proposed lot boundary adjustment require a reconfiguration of one or more lots that  
12 cannot reasonably meet the standards of subsection 23.28.030.A.3;

13 d. Location of existing easements or feasibility of access to portions of the  
14 property prevents the reconfiguration of lot lines that meet the standards of subsection  
15 23.28.030.A.3; or

16 e. The lot boundary adjustment establishes an irregular lot line that  
17 resulted from an adverse possession claim.

18 5. No adjusted lot shall be approved for development without a determination that  
19 it is capable of being served by existing or extended infrastructure for drainage; a determination  
20 that the lot has water supply and sanitary sewage disposal; and a determination that there is  
21 access for vehicles, utilities, and fire protection;





<b>Zones</b>	<b>Abbreviated</b>
Neighborhood Commercial 1	NC1
Neighborhood Commercial 2	NC2
Neighborhood Commercial 3	NC3
Master Planned Community—Yesler Terrace	MPC-YT
Seattle Mixed—South Lake Union	<del>((SMU-SLU))</del> <u>SM-SLU</u>
Seattle Mixed—Dravus	SM-D
Seattle Mixed—North Rainier	SM-NR
Seattle Mixed - Rainier Beach	SM-RB
Seattle Mixed—University District	SM-U
Seattle Mixed—Uptown	SM-UP
Seattle Mixed—Northgate	SM-NG
Commercial 1	C1
Commercial 2	C2
Downtown Office Core 1	DOC1
Downtown Office Core 2	DOC2
Downtown Retail Core	DRC
Downtown Mixed Commercial	DMC
Downtown Mixed Residential	DMR
Pioneer Square Mixed	PSM
International District Mixed	IDM

<b>Zones</b>	<b>Abbreviated</b>
International District Residential	IDR
Downtown Harborfront 1	DH1
Downtown Harborfront 2	DH2
Pike Market Mixed	PMM
General Industrial 1	IG1
General Industrial 2	IG2
Industrial Buffer	IB
Industrial Commercial	IC
Maritime Manufacturing and Logistics	MML
Industry and Innovation	II
Urban Industrial	UI

1

\* \* \*

2 Section 15. Section 23.34.010 of the Seattle Municipal Code, last amended by Ordinance  
3 126509, is repealed:

4 ~~((23.34.010 Designation of NR1, NR2, and NR3 zones~~

5 A. ~~Except as provided in subsection 23.34.010.B, areas zoned NR1, NR2, or NR3 may be~~  
6 ~~rezoned to zones more intense than NR3 only if the City Council determines that the area does~~  
7 ~~not meet the locational criteria for NR1, NR2, or NR3 zones.~~

8 B. ~~Areas zoned NR1, NR2, or NR3 that meet the locational criteria contained in~~  
9 ~~subsections 23.34.011.B.1 through 23.34.011.B.3 may only be rezoned to zones more intense~~

1 ~~than NR3 if they are located within the adopted boundaries of an urban village, and the rezone is~~  
2 ~~to a zone that is subject to the provisions of Chapter 23.58B and Chapter 23.58C.))~~

3 Section 16. Section 23.34.011 of the Seattle Municipal Code, last amended by Ordinance  
4 126509, is amended as follows:

5 **23.34.011 ((NR1, NR2, and NR3 zones)) NR zone, function((,)) and locational criteria**

6 A. Function. An area that provides ~~((predominantly detached single family structures on~~  
7 ~~lot sizes compatible with the existing pattern of development and the character of neighborhood~~  
8 ~~residential areas))~~ for the development of detached, attached, and stacked dwelling units within a  
9 predominately three-story height limit.

10 B. Locational criteria. An ~~((NR1, NR2, or NR3))~~ NR zone designation is most  
11 appropriate in areas that are ~~((outside of urban centers and villages and meet the following~~  
12 ~~criteria))~~ generally characterized by the following conditions:

13 ~~((1. Areas that consist of blocks with at least 70 percent of the existing structures,~~  
14 ~~not including detached accessory dwelling units, in single family residential use; or~~

15 ~~2. Areas that are designated by an adopted neighborhood plan as appropriate for~~  
16 ~~single family residential use; or~~

17 ~~3. Areas that consist of blocks with less than 70 percent of the existing structures,~~  
18 ~~not including detached accessory dwelling units, in single family residential use but in which an~~  
19 ~~increasing trend toward single family residential use can be demonstrated; for example:~~

20 ~~a. The construction of single family structures, not including detached~~  
21 ~~accessory dwelling units, in the last five years has been increasing proportionately to the total~~  
22 ~~number of constructions for new uses in the area, or~~

1                                    ~~b. The area shows an increasing number of improvements and~~  
2 ~~rehabilitation efforts to single-family structures, not including detached accessory dwelling units,~~  
3 ~~or~~

4                                    ~~e. The number of existing single-family structures, not including detached~~  
5 ~~accessory dwelling units, has been very stable or increasing in the last five years, or~~

6                                    ~~d. The area's location is topographically and environmentally suitable for~~  
7 ~~single-family residential developments.))~~

8                                    1. The area is located outside of a regional center, an urban center, ((~~urban~~  
9 ~~village~~)) or Station Area Overlay District;

10                                   2. The area is characterized by residential structures of generally three stories or  
11 less; and

12                                   3. One or more of the following conditions are present:

13                                   a. The area is not located near a major transit stop or on streets abutting  
14 frequent transit routes where higher density development might be more appropriate;

15                                   b. A significant portion of the area contains environmentally critical areas;  
16 or

17                                   c. The area is characterized by limited local access and circulation that  
18 make the area less suitable for higher density development.

19                                   ~~((C. An area that meets at least one of the locational criteria in subsection 23.34.011.B~~  
20 ~~should also satisfy the following size criteria in order to be designated as a NR1, NR2, or NR3~~  
21 ~~zone:~~

22                                   ~~1. The area proposed for rezone should comprise 15 contiguous acres or more, or~~  
23 ~~should abut existing NR1, NR2, or NR3 zones.~~

1                   2. ~~If the area proposed for rezone contains less than 15 contiguous acres, and does~~  
2 ~~not abut existing NR1, NR2, or NR3 zones, then it should demonstrate strong or stable single-~~  
3 ~~family residential use trends or potentials such as:~~

4                           a. ~~That the construction of single family structures, not including detached~~  
5 ~~accessory dwelling units, in the last five years has been increasing proportionately to the total~~  
6 ~~number of constructions for new uses in the area, or~~

7                           b. ~~That the number of existing single family structures, not including~~  
8 ~~detached accessory dwelling units, has been very stable or increasing in the last five years, or~~

9                           c. ~~That the area's location is topographically and environmentally suitable~~  
10 ~~for single family structures, or~~

11                           d. ~~That the area shows an increasing number of improvements or~~  
12 ~~rehabilitation efforts to single family structures, not including detached accessory dwelling units.~~

13                   D. ~~Half blocks at the edges of NR1, NR2, or NR3 zones which have more than 50~~  
14 ~~percent single family structures, not including detached accessory dwelling units, or portions of~~  
15 ~~blocks on an arterial which have a majority of single family structures, not including detached~~  
16 ~~accessory dwelling units, shall generally be included. This shall be decided on a case-by-case~~  
17 ~~basis, but the policy is to favor including them.))~~

18                   Section 17. Section 23.34.012 of the Seattle Municipal Code, last amended by Ordinance  
19 126855, is repealed:

20 ~~((23.34.012 Neighborhood Residential Small Lot (RSL) zone, function, and locational~~  
21 ~~criteria~~

1           ~~A. Function. An area within an urban village that provides for the development of homes~~  
2 ~~on small lots that may be more affordable compared to detached homes on larger lots and~~  
3 ~~appropriate for households with children.~~

4           ~~B. Locational criteria. An RSL zone is most appropriate in areas generally characterized~~  
5 ~~by the following:~~

6                   1. ~~The area is similar in character to neighborhood residential zones;~~

7                   2. ~~The area is located inside an urban center, urban village, or Station Area~~  
8 ~~Overlay District where it would provide opportunities for a diversity of housing types within~~  
9 ~~these denser environments;~~

10                  3. ~~The area is characterized by, or appropriate for, a mix of single family dwelling~~  
11 ~~units, multifamily structures that are similar in scale to single family dwelling units, such as~~  
12 ~~duplex, triplex, rowhouse, and townhouse developments, and single family dwelling units that~~  
13 ~~have been converted to multifamily residential use or are well suited to conversion;~~

14                  4. ~~The area is characterized by local access and circulation that can accommodate~~  
15 ~~low density development oriented to the ground level and the street, and/or by narrow roadways,~~  
16 ~~lack of alleys, and/or irregular street patterns that make local access and circulation less suitable~~  
17 ~~for higher density multifamily development;~~

18                  5. ~~The area is within a reasonable distance of frequency transit service, but is not~~  
19 ~~close enough to make higher density multifamily development more appropriate.~~

20                  6. ~~The area would provide a gradual transition between neighborhood residential~~  
21 ~~zoned areas and multifamily or neighborhood commercial zoned areas; and~~

22                  7. ~~The area is supported by existing or projected facilities and services used by~~  
23 ~~residents, including retail sales and services, parks, and community centers.))~~

1 Section 18. Section 23.34.013 of the Seattle Municipal Code, last amended by Ordinance  
2 126509, is repealed:

3 ~~((23.34.013 Designation of multifamily zones~~

4 ~~An area zoned neighborhood residential that meets the criteria of Section 23.34.011 for~~  
5 ~~designation as NR1, NR2 or NR3 may not be rezoned to multifamily except as otherwise~~  
6 ~~provided in Section 23.34.010.B.))~~

7 Section 19. Section 23.34.014 of the Seattle Municipal Code, last amended by Ordinance  
8 126509, is amended as follows:

9 **23.34.014 Lowrise 1 (LR1) zone, function and locational criteria**

10 A. Function. The function of the LR1 zone is to provide opportunities ~~((for low density~~  
11 ~~multifamily housing, primarily rowhouse and townhouse developments, through infill~~  
12 ~~development that is compatible with single family dwelling units, or through the conversion of~~  
13 ~~existing single family dwelling units to duplexes or triplexes)) for the development of detached,~~  
14 ~~attached, and stacked dwelling units within a predominately three-story height limit at a higher~~  
15 ~~intensity than Neighborhood Residential zones.~~

16 B. Locational ~~((Criteria))~~ criteria. The LR1 zone is most appropriate in areas generally  
17 characterized by the following conditions:

18 1. ~~((The area is similar in character to neighborhood residential zones;~~

19 2.)) The area is ~~((either))~~:

20 a. ~~((located))~~ Located outside of an urban center, urban village, or Station  
21 Area Overlay District;



1                                    b. ~~((a))~~ A limited area within an urban center, urban village, or Station  
2 Area Overlay District that would provide opportunities for a diversity of housing types within  
3 these denser environments; or

4                                    c. ~~((located))~~ Located on a collector or minor arterial;  
5                                    ~~((3.))~~ 2. The area is characterized by ~~((a mix of single family dwelling units,~~  
6 ~~multifamily structures that are similar in scale to single family dwelling units, such as rowhouse~~  
7 ~~and townhouse developments, and single family dwelling units that have been converted to~~  
8 ~~multifamily residential use or are well suited to conversion))~~ residential structures of generally  
9 three stories or less;

10                                    ~~((4.))~~ 3. The area is characterized by local access and circulation that can  
11 accommodate low density ~~((multifamily))~~ development ~~((oriented to the ground level))~~ and the  
12 street, and/or by narrow roadways, lack of alleys, and/or irregular street patterns that make local  
13 access and circulation less suitable for higher density ~~((multifamily))~~ development;

14                                    ~~((5. The area would provide a gradual transition between neighborhood residential~~  
15 ~~zoned areas and multifamily or neighborhood commercial zoned areas; and~~

16                                    ~~6.))~~ 4. The area is supported by existing or projected facilities and services used  
17 by residents, including retail sales and services, parks, and community centers.

18                                    Section 20. Section 23.34.072 of the Seattle Municipal Code, last amended by Ordinance  
19 126509, is repealed:

20 ~~**((23.34.072 Designation of commercial zones.**~~

21                                    ~~A. The encroachment of commercial development into residential areas shall be~~  
22 ~~discouraged.~~

1 ~~B. Areas meeting the locational criteria for a neighborhood residential designation may~~  
2 ~~be designated as certain neighborhood commercial zones as provided in Section 23.34.010.~~

3 ~~C. Preferred configuration of commercial zones shall not conflict with the preferred~~  
4 ~~configuration and edge protection of residential zones as established in Sections 23.34.010 and~~  
5 ~~23.34.011 of the Seattle Municipal Code.~~

6 ~~D. Compact, concentrated commercial areas, or nodes, shall be preferred to diffuse,~~  
7 ~~sprawling commercial areas.~~

8 ~~E. The preservation and improvement of existing commercial areas shall be preferred to~~  
9 ~~the creation of new business districts.))~~

10 Section 21. Section 23.42.022 of the Seattle Municipal Code, enacted by Ordinance  
11 127211, is amended as follows:

12 **23.42.022 Accessory dwelling units**

13 A. ~~((Attached and detached accessory))~~ Accessory dwelling units are ~~((permitted))~~  
14 allowed as a housing use in all zones where ~~((single family dwelling units are permitted))~~  
15 housing uses are allowed. In the Shoreline District, accessory dwelling units shall comply with  
16 Chapter 23.60A.

17 ~~((B. A maximum of two accessory dwelling units may be located on the same lot as a~~  
18 ~~principal dwelling unit. Either or both accessory dwelling units may be attached or detached.~~  
19 ~~Two detached accessory dwelling units may be located in one structure.))~~

20 B. Accessory dwelling units may not be accessory to residential uses other than housing  
21 uses.

22 C. No lot may have more than two accessory dwelling units.

23 D. Accessory dwelling units may be attached, detached, or stacked.

1 E. Unless otherwise provided in the standards of the underlying zone, accessory dwelling  
2 units shall be subject to the same standards as principal dwelling units.

3 F. Accessory dwelling units must be located on the same lot as the principal dwelling  
4 unit.

5 ~~((C. Floor area limit in all zones and floor area ratio in Neighborhood Residential zones))~~

6 G. Maximum size

7 1. Gross floor area limit

8 a. The gross floor area of an accessory dwelling unit with up to two  
9 bedrooms may not exceed 1,000 square feet, ~~except as provided in subsection 23.42.022.G.1.c.~~

10 b. The gross floor area of an accessory dwelling unit with three or more  
11 bedrooms may not exceed 1,200 square feet, ~~except as provided in subsection 23.42.022.G.1.c.~~

12 c. The gross floor area of an accessory dwelling ~~unit with any number~~  
13 regardless of number of bedrooms may not exceed 1,500 square feet if ~~the following~~  
14 requirements are met:

15 ~~((a.))~~ 1) The lot is located in a LR zone;

16 ~~((b.))~~ 2) The lot is located in a frequent transit service area; and

17 ~~((c.))~~ 3) The lot has not been purchased for ~~((value))~~ more than  
18 ~~\$1,000~~ in the past 20 years.

19 2. The following are not included in the gross floor area limit:

20 a. Up to 250 square feet of gross floor area in an attached garage;

21 b. ~~((Exterior only accessed storage areas;~~

22 ~~ε-)) All stories, or portions of stories, that are underground; and~~



1                   1. ~~In all zones detached accessory dwelling units have no required setback from~~  
2 ~~any lot line that abuts an alley.~~

3                   2. ~~Neighborhood Residential zones~~

4                   a. ~~A detached accessory dwelling unit and appurtenant architectural~~  
5 ~~elements may be located in the rear yard so long as the structure is no closer than 5 feet to any lot~~  
6 ~~line that does not abut an alley. When a detached accessory dwelling unit is located within a rear~~  
7 ~~yard, the following features may also be located within 5 feet of any lot line:~~

8                   1) ~~External architectural details with no living area, such as~~  
9 ~~chimneys, eaves, cornices, and columns, may be located no closer than 3 feet from a property~~  
10 ~~line.~~

11                   2) ~~Bay windows no more than 8 feet in width may be located no~~  
12 ~~closer than 3 feet from a property line.~~

13                   3) ~~Other projections that include interior space, such as garden~~  
14 ~~windows, may be located no closer than 3.5 feet from a property line starting a minimum of 30~~  
15 ~~inches above furnished floor, and with maximum dimensions of 6 feet in height and 8 feet in~~  
16 ~~width.~~

17                   b. ~~On a through lot, when yards or setbacks cannot be determined, the~~  
18 ~~Director shall designate a rear yard or rear setback for the purpose of allowing an accessory~~  
19 ~~dwelling. In designating a rear yard or rear setback, the Director shall consider factors including~~  
20 ~~but not limited to the location of the yards and setbacks for adjacent structures on the same block~~  
21 ~~face, vehicular and pedestrian access, platting patterns in the vicinity, and topography.~~

1                   3. ~~Lowrise zones. Detached accessory dwelling units are excluded from setback~~  
2 ~~averaging provisions and are subject to the minimum setback provision for a principal dwelling~~  
3 ~~unit.~~

4                   F. ~~Rooftop decks that are portions of an accessory dwelling unit are allowed up to the~~  
5 ~~applicable height limit, including additions allowed to a detached accessory dwelling unit under~~  
6 ~~subsection 23.44.014.C.4.~~

7                   G.) H. Conversions of existing structures

8                   1. For purposes of this subsection (~~(23.42.022.G)~~) 23.42.022.H, the term  
9 "conversion" means keeping an existing structure intact, adding to or altering an existing  
10 structure, or removing and rebuilding an existing structure, provided that any expansion or  
11 relocation of the structure complies with the development standards for accessory dwelling units  
12 in this Section 23.42.022 and the provisions of the applicable zone, unless otherwise allowed by  
13 this subsection (~~(23.42.022.G)~~) 23.42.022.H.

14                   2. For the purposes of this subsection (~~(23.42.022.G)~~) 23.42.022.H, the term  
15 “existing accessory structure” means an accessory structure existing prior to July 23, 2023 or an  
16 accessory structure existing prior to July 23, 2023 that was subsequently replaced to the same  
17 configuration.

18                   3. Existing accessory structures. An existing accessory structure may be converted  
19 into a detached accessory dwelling unit if it meets the following:

20                   a. To facilitate the conversion of and additions to an existing accessory  
21 structure, the Director may allow waivers and modifications as a Type I decision to the  
22 provisions for accessory dwelling units in this Section 23.42.022 and the development standards  
23 of the applicable zone.

1                                   b. Conversion of an existing accessory structure to a detached accessory  
2 dwelling unit is permitted notwithstanding applicable lot coverage or yard or setback provisions  
3 in this Section 23.42.022 or the applicable zone. The converted accessory structure shall comply  
4 with the minimum standards set forth in Sections 22.206.020 through 22.206.140.

5                                   4. Existing principal structures. The gross floor area of an attached accessory  
6 dwelling unit may exceed 1,000 square feet if the portion of the structure in which the attached  
7 accessory dwelling unit is located existed as of July 23, 2023.

8                                   ~~((H. Building separation~~

9                                   ~~1. Neighborhood Residential zones. A detached accessory dwelling unit shall be~~  
10 ~~separated from its principal dwelling unit by a minimum of 5 feet measured from eave to eave.~~  
11 ~~To be considered attached, an accessory dwelling unit must be connected to the principal~~  
12 ~~dwelling unit by an enclosed space that is at least 3 feet wide, 3 feet tall, and 3 feet long.~~

13                                   ~~2. All other zones. A detached accessory dwelling unit shall be separated from its~~  
14 ~~principal dwelling unit by a minimum of 3 feet measured from eave to eave. To be considered~~  
15 ~~attached, an accessory dwelling unit must be connected to a principal dwelling unit by an~~  
16 ~~enclosed space that is at least 3 feet wide, 3 feet tall, and 3 feet long.))~~

17                                   I. No off-street motor vehicle parking is required for an accessory dwelling unit.

18                                   J. When calculating density, the number of dwelling units shall include both accessory  
19 dwelling units and principal dwelling units.

20                                   ~~((J.))~~ K. Title 23 shall not be interpreted or applied to prohibit the sale or other  
21 conveyance of a condominium unit on the grounds that the condominium unit was originally  
22 built as an accessory dwelling unit.

1           ~~((K-))~~ L. Unless provided otherwise in this Section 23.42.022, the provisions of the  
2 applicable zone and overlay district apply. In the event of conflict with provisions elsewhere in  
3 Title 23 other than Chapter 23.60A, this Section 23.42.022 shall prevail.

4           Section 22. A new Section 23.42.024 is added to the Seattle Municipal Code as follows:

5 **23.42.024 Adult family homes**

6 Adult family homes are allowed as a home occupation in all zones where housing uses are  
7 allowed.

8           Section 23. A new Section 23.42.047 is added to the Seattle Municipal Code, as follows:

9 **23.42.047 Sensitive land uses near highways and major truck routes**

10 Any dwelling unit, school, or child care center that is located within 600 feet of an interstate,  
11 highway, or designated major truck street right-of-way, or railroad (~~((right-of-way))~~) must  
12 incorporate the following features to maintain indoor air quality and reduce noise intrusion:

13           A. Sound-insulating windows or other noise-insulating features sufficient to maintain  
14 interior sound levels at 45 decibels or below in consideration of existing environmental noise  
15 levels at the site. The applicant shall submit an analysis of existing noise levels and  
16 documentation of the sound insulating capabilities of windows or other noise-insulating features  
17 as part of the permit application; and

18           B. A permanently installed air cooling system and a balanced ventilation system, which  
19 may be combined. The ventilation system shall filter any outdoor air supply through filters rated  
20 MERV 13 or higher as determined by the American Society of Heating, Refrigerating, and Air



1 Conditioning Engineers (ASHRAE). The air cooling and ventilation systems shall be indicated  
2 on the plan.

3 Section 24. Section 23.42.050 of the Seattle Municipal Code, last amended by Ordinance  
4 126845, is amended as follows:

5 **23.42.050 Home occupations**

6 A home occupation of a person residing in a dwelling unit is permitted outright in all zones as an  
7 accessory use to any residential use permitted outright or to a permitted residential conditional  
8 use, subject to the following requirements:

9 \* \* \*

10 G. A maximum of three passenger vehicles, vans, and similar vehicles, associated with  
11 the home occupation, each not exceeding a gross vehicle weight of 10,000 pounds are permitted  
12 to be at the home occupation site, independent of commercial deliveries and pickups. For lots  
13 developed with a ~~((single-family))~~ residential dwelling unit in NR zones, this limit is in addition  
14 to the outdoor parking limit in subsection ~~((23.44.016.C.3))~~ 23.44.160.E.

15 \* \* \*

16 Section 25. Section 23.42.106 of the Seattle Municipal Code, last amended by Ordinance  
17 126685, is amended as follows:

18 **23.42.106 Expansion of nonconforming uses**

19 \* \* \*

20 B. In addition to the standards in subsection 23.42.106.A, a structure in a ~~((neighborhood~~  
21 ~~residential))~~ Neighborhood Residential zone occupied by a nonconforming residential use may  
22 be allowed to expand subject to the following:



1                   1. A structure occupied by a nonconforming nonresidential use may be  
2 maintained, repaired, renovated, or structurally altered but shall not be expanded or extended  
3 except as otherwise required by law, as necessary to improve access for the elderly or disabled or  
4 as specifically permitted elsewhere in this Code.

5                   2. In ~~((the))~~ Seattle Mixed zones, general manufacturing uses exceeding 25,000  
6 square feet of gross floor area and heavy manufacturing uses may be expanded or extended by an  
7 amount of gross floor area not to exceed 20 percent of the existing gross floor area of the use,  
8 provided that this exception may be applied only once to any individual business establishment.

9                   3. The Seattle Asian Art Museum building and use located in Volunteer Park, as it  
10 exists on January 1, 2017, may be expanded subject to the following development standards:

11                   a. Except as provided in this subsection 23.42.106.D.3, the development  
12 standards of Chapter 23.44 do not apply.

13                   b. The building may be expanded one or more times but the gross floor  
14 area of all expansions combined and occurring after January 1, 2017, may not exceed 15,000  
15 square feet.

16                   c. No expansion may be located in a freestanding building that lacks a  
17 common wall with the building either as it existed on January 1, 2017, or as subsequently  
18 expanded.

19                   d. No expansion may exceed the elevation of the highest point of the  
20 building as it existed on January 1, 2017.

21                   e. Parking and loading for the proposed expansion is required as provided  
22 in Sections 23.54.015 and 23.54.035. As a Type I decision, the Director may reduce parking and  
23 loading requirements to an amount not less than the amount needed to provide adequate parking

1 and loading facilities, as demonstrated to the satisfaction of the Director by a parking and loading  
2 study prepared by a licensed professional engineer and submitted to the Director by the  
3 applicant.

4 f. Bicycle parking for the proposed expansion shall be provided in  
5 accordance with (~~subsection 23.54.015.K~~) Section 23.54.037.

6 g. The street and sidewalk requirements of Chapter 23.53 do not apply.

7 h. Exterior lighting shall be shielded or directed away from adjacent  
8 residentially zoned lots.

9 i. Nothing in this Section 23.42.106 alters the authority of the Landmarks  
10 Preservation Board pursuant to the City's Landmarks Preservation Ordinance.

11 \* \* \*

12 Section 26. Section 23.42.110 of the Seattle Municipal Code, last amended by Ordinance  
13 126509, is amended as follows:

14 **23.42.110 Change from one nonconforming use to another nonconforming use**

15 A nonconforming use may be converted by an administrative conditional use authorization to  
16 another use not otherwise permitted in the zone subject to the following limitations and  
17 conditions.

18 A. (~~In neighborhood residential and residential small lot zones, a nonconforming  
19 multifamily residential use may not be converted to any nonresidential use not otherwise  
20 permitted in the zone.~~

21 B.)) The proposed new use must be no more detrimental to properties in the zone and  
22 vicinity than the existing use. This determination shall be based on consideration of the  
23 following factors:

- 1                   1. The zones in which both the existing use and the proposed new use are
- 2 allowed;
- 3                   2. The number of employees and clients associated or expected with the proposed
- 4 use;
- 5                   3. The relative parking, traffic, light, glare, noise, odor and similar impacts of the
- 6 two uses and how these impacts could be mitigated.

7           ~~((C))~~ B. The existence of a single residential unit, such as a caretaker's or proprietor's  
8 unit, accessory to a nonconforming commercial use shall not be treated as having established a  
9 residential use, and such a unit may be converted or changed provided that it is the only  
10 residential use in the structure and comprises less than half of the total floor area of the structure.

11           ~~((D))~~ C. Parking requirements for the proposed use shall be determined by the Director.

12           ~~((E))~~ D. If the new use is permitted, the Director may require mitigation measures,  
13 including but not limited to landscaping, sound barriers or fences, mounding or berming,  
14 adjustments to ~~((yards))~~ setback or parking standards, design modification, or limiting hours of  
15 operation.

16           Section 27. Section 23.42.124 of the Seattle Municipal Code, last amended by Ordinance  
17 126509, is amended as follows:

18 **23.42.124 Light and glare standards nonconformity**

19 When nonconforming exterior lighting is replaced, new lighting shall conform to the  
20 requirements of the light and glare standards of the respective zone. See ~~((subsection~~  
21 ~~23.44.008.H))~~ Section 23.44.150 for ~~((neighborhood residential))~~ Neighborhood Residential  
22 zones; Section 23.45.534 for multifamily zones; Section 23.46.020 for residential-commercial

1 zones; Section 23.47A.022 for C zones or NC zones; Section 23.48.075 for SM zones; Section  
2 23.49.025 for downtown zones; and Section 23.50.046 for IB and IC zones.

3 Section 28. Section 23.42.130 of the Seattle Municipal Code, last amended by Ordinance  
4 127099, is repealed:

5 ~~((23.42.130 Nonconforming solar collectors~~

6 ~~The installation of solar collectors that do not conform to development standards or that increase~~  
7 ~~an existing nonconformity may be permitted as follows:~~

8 ~~A. In neighborhood residential zones, pursuant to subsection 23.44.046.B;~~

9 ~~B. In multifamily zones, pursuant to subsection 23.45.545.E;~~

10 ~~C. In NC zones or C zones, pursuant to subsection 23.47A.012.E.))~~

11 Section 29. A new Section 23.42.132 is added to the Seattle Municipal Code as follows:

12 **23.42.132 Columbariums, garden wall crypts, and mausoleums**

13 Columbariums, garden wall crypts, and mausoleums are permitted only as accessory to existing  
14 cemeteries, except that columbariums and garden wall crypts may also be accessory to religious  
15 facilities. In addition, no interment openings shall abut or be directly across the street from  
16 property other than cemetery property. For columbariums, garden wall crypts, and mausoleums  
17 accessory to existing cemeteries, any border between structures and the property line shall be  
18 landscaped and maintained by the owner in good condition.

19 Section 30. Chapter 23.44 of the Seattle Municipal Code, last amended by Ordinance  
20 127099, is repealed as shown in Attachment 2 to this ordinance.

21 Section 31. A new Chapter 23.44 is added to the Seattle Municipal Code as follows:

22 **Chapter 23.44 NEIGHBORHOOD RESIDENTIAL**

23 **23.44.010 Scope of provisions**

1           A. This Chapter 23.44 establishes regulations for the Neighborhood Residential (NR)  
2 zone.

3           B. Some land in these zones may be regulated by Subtitle III, Division 3, Overlay  
4 Districts, of this Title 23 in addition to the standards of this Chapter 23.44.

5           C. Other regulations may apply to development proposals, including but not limited to  
6 general use provisions (Chapter 23.42); transportation concurrency and transportation impact  
7 mitigation (Chapter 23.52); requirements for streets, alleys, and easements (Chapter 23.53);  
8 standards for access, off-street parking, and solid waste storage (Chapter 23.54); sign regulations  
9 (Chapter 23.55); communication regulations (Chapter 23.57); shoreline regulations (Chapter  
10 23.60A); and environmental protection and historic preservation (Title 25).

11           D. Congregate residences are subject to additional requirements as specified in Section  
12 23.42.049.

13 **23.44.020 Permitted and prohibited uses**

14           A. All uses are permitted outright, prohibited, or permitted as a conditional use according  
15 to Table A for 23.44.020 and this Section 23.44.020. Uses not referred to in Table A for  
16 23.44.020 are prohibited, unless otherwise indicated in this Chapter 23.44 or Chapters 23.51A,  
17 23.51B, or 23.57. Communication utilities and accessory communication devices, except as  
18 exempted in Section 23.57.002, are subject to this Chapter 23.44 and Chapter 23.57. Public  
19 facilities are subject to Section 23.51A.004.

20           B. All permitted uses are allowed as a principal use or as an accessory use, unless  
21 otherwise indicated in this Chapter 23.44.

<b>Table A for 23.44.020 Permitted and prohibited uses</b>	
<b>Uses</b>	<b>Permitted and prohibited uses</b>
A. Residential uses except as listed below	P
A.1. Assisted living facilities	X
A.2. Caretaker's quarters	X
A.3. Congregate residences	X/P <sup>1</sup>
B. Institutions except as listed below	P/CU <sup>2</sup>
B.1. Adult care centers	X
B.2. Colleges	X
B.3. Hospitals	X
B.4. Institutes for advanced study	X
B.5. Museums	X
B.6. Private clubs	X/CU/P <sup>3</sup>
B.7. Vocational or fine arts schools	X
C. Uses in existing or former public schools	
C.1. Preschools, public or private schools, colleges, and community centers in existing or former public schools	P
C.2. Uses not otherwise permitted in existing or former public schools	P <sup>4</sup>
D. Parks and open space uses	P
E. Ground-floor commercial uses	P <sup>5</sup>
F. Human service uses	X



**Table A for 23.44.020  
 Permitted and prohibited uses**

Uses	Permitted and prohibited uses
G. Cemeteries	P/X <sup>6</sup>
H. Community gardens	P
I. Rail transit facilities and railroads	P
J. Park and ride facilities	CU <sup>7</sup>
K. Commercially operating horse farms in existence before July 1, 2000	P <sup>8</sup>
L. Uses not otherwise permitted if located in Landmark structures	CU <sup>9</sup>
M. Uses not otherwise permitted if located in structures unsuited to permitted uses	CU <sup>10</sup>
N. All other uses	X

Key to Table A for 23.44.020

P = Permitted outright

CU = Permitted as an administrative conditional use

X = Prohibited

Footnotes to Table A for 23.44.020

<sup>1</sup> Congregate residences are allowed within a major transit service area and prohibited in other areas.

<sup>2</sup> Institutions meeting development standards including but not limited to Section 23.44.180 are permitted outright. Public schools that do not meet development standards are regulated by Chapter 23.51B and Chapter 23.79. Institutions other than public schools that do not meet development standards may be permitted as administrative conditional uses pursuant to Section 23.44.030.

<sup>3</sup> New private clubs are prohibited. Existing private clubs are permitted provided that the use is not expanded. Existing private clubs may be expanded as a conditional use only if the expansion would not result in the gross floor area or the number of surface parking spaces exceeding the amount existing on the effective date of this ordinance by more than 25%.

<sup>4</sup> Pursuant to procedures in Chapter 23.78.

**Table A for 23.44.020**  
**Permitted and prohibited uses**

**Uses**

**Permitted and  
prohibited uses**

<sup>5</sup> Ground-floor commercial uses are only allowed if they meet the standards of subsection 23.44.020.E.

<sup>6</sup> Pursuant to subsection 23.44.020.D

<sup>7</sup> Pursuant to subsection 23.44.030.F.

<sup>8</sup> Provided that they are located on lots greater than 10 acres and conform to the limits on the number and location of farm animals and structures containing them set forth in Section 23.42.052.

<sup>9</sup> Pursuant to subsection 23.44.030.D.

<sup>10</sup> Pursuant to subsection 23.44.030.E.

C. Accessory uses

1. Except as otherwise provided in this subsection 23.44.020.C, accessory uses customarily incidental to principal uses permitted outright are permitted outright.

2. All accessory uses and structures, except for urban farms and structures in urban farm use, must be located on the same lot as the principal use or structure unless otherwise specifically provided.

3. Urban farms with planting area not more than 4,000 square feet are permitted outright as an accessory use. Urban farms with more than 4,000 square feet of planting area may be permitted as an administrative conditional use accessory to any principal use permitted outright or as a conditional use, pursuant to Section 23.42.051.

4. Piers and floats are permitted, provided they comply with Chapter 23.60A.

5. Bed and breakfast uses are permitted outright if:

a. The bed and breakfast use has a valid business license tax certificate issued by the Department of Finance and Administrative Services;

1                               b. The bed and breakfast use is operated by the primary resident of the  
2 dwelling unit where the bed and breakfast is located or the resident operator;

3                               c. There is no evidence of the bed and breakfast use visible from the  
4 exterior of the dwelling unit except for a sign permitted by subsection 23.55.020.D.1; and

5                               d. The bed and breakfast use has no more than five guest rooms, provided  
6 that this limitation does not apply to bed and breakfast uses that were established on or before  
7 April 1, 1987.

8                               6. Accessory dwelling units are permitted, provided they comply with Section  
9 23.42.022.

10                              7. Human service uses accessory to institutional uses are permitted outright.

11                              D. Existing cemeteries are permitted and are prohibited from expanding. New cemeteries  
12 are prohibited. For purposes of this Section 23.44.020, a change in a cemetery boundary is not  
13 considered an expansion in size and is permitted provided that:

14                              1. The change does not increase the net land area occupied by the cemetery;

15                              2. The land being added to the cemetery is contiguous to the existing cemetery  
16 and is not separated from the existing cemetery by a public street or alley whether or not  
17 improved; and

18                              3. The use of the land being added to the cemetery will not result in the loss of  
19 housing.

20                              E. All ground-floor commercial uses permitted pursuant to this Section 23.44.020 shall  
21 meet the following conditions:

22                              1. The commercial use is limited to the following:

23                                  a. Food processing and craft work;

- 1                                   b. General sales and services; and
- 2                                   c. Restaurants;
- 3                                   2. The gross floor area of commercial uses does not occupy more than 2,500
- 4 square feet of gross floor area;
- 5                                   3. The commercial use is located only on or below the ground floor of a structure;
- 6                                   4. Vents for venting of odors, vapors, smoke, gas and fumes, and exterior heat
- 7 exchangers and other similar devices (e.g., related to ventilation, air conditioning, refrigeration)
- 8 shall be at least 10 feet above finished sidewalk grade and directed away to the extent possible
- 9 from residential uses within 50 feet of the vent;
- 10                                  5. Drive-in businesses are prohibited as a principal or accessory use;
- 11                                  6. Outdoor sales of food or beverages must be located at least 50 feet from
- 12 adjacent lots;
- 13                                  7. Outdoor service of food or beverages must be located at least 50 feet from
- 14 adjacent lots; and
- 15                                  8. Businesses may not be open between the hours of 10 p.m. and 6 a.m.

16 **23.44.030 Administrative conditional uses**

17           A. Uses permitted as administrative conditional uses in Section 23.44.020 may be  
18 permitted by the Director when the provisions of Section 23.42.042 and this Section 23.44.030  
19 are met.

20           B. Unless otherwise specified in this Chapter 23.44, conditional uses shall meet the  
21 development standards for uses permitted outright. If an existing structure is nonconforming to  
22 development standards, no conditional use is required for any alterations that do not increase the  
23 nonconformity.

1 C. Institutions other than public schools that do not meet the development standards of  
2 this Chapter 23.44, including Major Institution uses as provided in Chapter 23.69, and the  
3 expansion of existing private clubs may be permitted subject to the following:

4 1. Bulk and siting. In order to accommodate the special needs of the proposed  
5 institution, and to better site the facility with respect to its surroundings, the Director may modify  
6 the applicable development standards. In determining whether to allow such modifications, the  
7 Director shall balance the needs of the institution against the compatibility of the proposed  
8 institution with the residential scale and character of the surrounding area.

9 2. Noise, Light and Glare. The Director may condition the permit in order to  
10 mitigate potential noise, light and glare impacts. Measures the Director may require for this  
11 purpose include, but are not limited to the following: visual screening, landscaping, sound  
12 barriers, fences, berms, adjustments to setbacks or the location of refuse storage areas, location  
13 of parking areas and access, structural design modifications, limiting exterior lighting fixture  
14 type, location and height to mitigate light trespass, and regulating hours of use.

15 3. Transportation plan. A transportation plan is required for proposed new  
16 institutions and for those institutions proposing to expand larger than 4,000 square feet of gross  
17 floor area and/or to provide 20 or more new parking spaces. The Director may condition a permit  
18 to mitigate potential traffic and parking impacts pursuant to a Transportation Management Plan  
19 or Program as described in Director's rules governing such plans or programs. The Director will  
20 determine the level of detail to be disclosed in the transportation plan based on the probable  
21 impacts and/or scale of the proposed institution.

22 D. A use not otherwise permitted in a Neighborhood Residential zone within a structure  
23 designated as a Seattle Landmark that is subject to controls and incentives imposed by a

1 designating ordinance, when the owner of the Landmark has executed and recorded an  
2 agreement acceptable in form and content to the Landmarks Preservation Board providing for the  
3 restoration and maintenance of the historically significant features of the structure, may be  
4 permitted subject to the following:

5           1. The use is compatible with the existing design and/or construction of the  
6 structure without significant alteration;

7           2. Uses permitted by the zone are impractical because of structure design and/or  
8 that no permitted use can provide adequate financial support necessary to sustain the structure in  
9 reasonably good physical condition; and

10           3. The use shall not be detrimental to other properties in the zone or vicinity or to  
11 the public interest.

12           E. Uses in structures unsuited to uses permitted outright

13           1. A use not otherwise permitted in a Neighborhood Residential zone may be  
14 permitted as an administrative conditional use in structures unsuited to uses permitted outright in  
15 Neighborhood Residential zones. The determination that a use may be permitted shall be based  
16 on the following factors:

17                   a. The design of the structure is not suitable for conversion to a use  
18 permitted outright in a Neighborhood Residential zone;

19                   b. The structure contains more than 4,000 square feet; and

20                   c. The proposed use will provide a public benefit.

21           2. Parking requirements for uses permitted under this subsection 23.44.030.E shall  
22 be determined by the Director.

1                   3. The Director may require measures to mitigate impacts such as noise, odor,  
2 parking, or traffic impacts. Mitigating measures may include but are not limited to landscaping,  
3 sound barriers, fences, mounding or berming, adjustments to development standards, design  
4 modifications, or setting hours of operation.

5                   4. In the case of an existing or former public school, permissible uses other than  
6 those permitted outright in the zone and their development standards including parking  
7 requirements shall be established only pursuant to procedures for establishing criteria for joint  
8 use or reuse of public schools in Chapter 23.78.

9                   F. A park and ride facility under the management of a public agency responsible for  
10 commuter pooling efforts may be permitted if the Director determines that:

- 11                   1. It is to be located on an existing parking lot;
- 12                   2. That parking proposed for the park and ride facility is not needed by the  
13 principal use or its accessory uses during the hours proposed for park and ride use; and
- 14                   3. The park and ride use shall not interfere or conflict with the peak-hour  
15 activities associated with the principal use and its accessory uses. The Director may control the  
16 number and location of parking spaces to be used.

17                   G. Any use that was previously authorized by a conditional use permit but which has  
18 been discontinued shall not be re-established or re-commenced except pursuant to a new  
19 conditional use permit, provided that such permit is required for the use at the time re-  
20 establishment or re-commencement is proposed. Vacant property, except for dead storage of  
21 materials or equipment of the conditional use, shall not be considered as being devoted to the  
22 authorized conditional use. The expiration of licenses necessary for the conditional use shall be  
23 evidence that the property is not being devoted to the conditional use. A conditional use in a

1 residential structure or a multitenant commercial structure shall not be considered as  
2 discontinued unless all units are either vacant or devoted to another use. The following shall  
3 constitute conclusive evidence that the conditional use has been discontinued:

4           1. A permit to change the use of the property has been issued and the new use has  
5 been established; or

6           2. The property has not been devoted to the authorized conditional use for more  
7 than 24 consecutive months.

8           H. Minor structural work that does not increase usable gross floor area or seating capacity  
9 and that does not exceed the development standards applicable to the use shall not be considered  
10 an expansion and does not require approval as a conditional use unless the work would exceed  
11 the height limit of the zone for uses permitted outright. Such work includes but is not limited to  
12 roof repair or replacement and construction of uncovered decks and porches, facilities for  
13 barrier-free access, bay windows, dormers, and eaves.

14 **23.44.040 General provisions**

15           A. An exception from one specific standard does not relieve the applicant from  
16 compliance with any other standard.

17           B. Any structure occupied by a permitted principal use other than residential use may be  
18 converted to residential use even if the structure does not conform to the development standards  
19 for residential uses in the Neighborhood Residential zone.

20           C. Assisted living facilities, congregate residences, and structures containing ground floor  
21 commercial uses shall meet the development standards for stacked dwelling units unless  
22 otherwise specified.



1 D. If more than one category of residential use is located on a lot, and if different  
2 development standards apply to the different categories of use, then each category's percentage  
3 of the total limit imposed by the development standard shall be calculated based on each  
4 category's percentage of total structure footprint area as follows:

5 1. Calculate the footprint, in square feet, for each category of residential use. For  
6 purposes of this calculation, "footprint" is defined as the horizontal area enclosed by the exterior  
7 walls of the structure.

8 2. Calculate the total square feet of the footprint of all categories of residential  
9 uses on the lot.

10 3. Divide the square footage of the footprint for each category of residential  
11 structure in subsection 23.44.040.D.1 by the total square feet of the footprint of all residential  
12 uses in subsection 23.44.040.D.2.

13 4. Multiply the percentage calculated in subsection 23.44.040.D.3 for each  
14 housing category by the area of the lot. The result is the area of the lot devoted to each housing  
15 category.

16 5. The total limit for each category of residential use is the applicable limit for  
17 that use multiplied by the percentage calculated in subsection 23.44.040.D.4.

18 E. As a Type I decision, the Director may waive or modify the standards of Sections  
19 23.44.110 ((~~7~~)) and 23.44.130, ((~~23.44.140~~)) and subsections 23.44.160 .A and 23.44.160.B for  
20 the conversion of a residential structure within a development from one dwelling unit to two or  
21 more dwelling units. For the purposes of this subsection 23.44.040.E, conversion means keeping  
22 an existing residential structure intact without the addition of interior floor area.

23 **23.44.050 Floor area**

1 A. Gross floor area. In Neighborhood Residential zones, gross floor area includes exterior  
 2 corridors, breezeways, and stairways that provide building circulation and access to dwelling  
 3 units or sleeping rooms. Balconies, patios, and decks that are associated with a single dwelling  
 4 unit or sleeping room and that are not used for common circulation are not considered gross floor  
 5 area.

6 B. Floor area ratio (FAR) limits. The FAR limit in Neighborhood Residential zones for  
 7 lots with residential uses (~~(, except for stacked dwelling units that meet the requirements in~~  
 8 ~~subsection 23.44.050.D,))~~) is as shown in Table A for 23.44.050, except that structures on lots  
 9 with less than 5,000 square feet of lot area can include up to 2,500 square feet of total chargeable  
 10 floor area or the amount of total chargeable floor area allowed by the FAR limit shown in Table  
 11 A for 23.44.050, whichever is greater. The FAR limit in Neighborhood Residential zones for lots  
 12 without residential uses is 1.2. The applicable FAR limit applies to the total chargeable floor area  
 13 of all structures on the lot.

<b>Table A for 23.44.050</b>	
<b>Floor area ratio (FAR) in NR zones (<del>(, except for stacked dwelling units that meet the requirements in subsection 23.44.050.D,))</del>)</b>	
<b>Density (dwelling units per lot size)</b>	<b>FAR</b>
Less dense than 1 unit / 4,000 square feet	0.6
1 unit / 4,000 square feet to 1 unit / 2,201 square feet	0.8, <u>or 1.0 if the development meets the standards of subsection 23.44.050.D</u>
1 unit / 2,200 square feet to 1 unit / 1,601 square feet	1.0, <u>or 1.2 if the development meets the standards of subsection 23.44.050.D</u>
1 unit / 1,600 square feet or denser	1.6 <u>for attached and detached dwelling units, except that it is:</u> <ul style="list-style-type: none"> <li>• <u>1.8 for development on lots located within a frequent transit service area that consist entirely of attached or detached dwelling units in structures that are less than three stories and that are arranged</u></li> </ul>

<b>Table A for 23.44.050</b>	
<b>Floor area ratio (FAR) in NR zones ((<del>except for stacked dwelling units that meet the requirements in subsection 23.44.050.D</del>))</b>	
<b>Density (dwelling units per lot size)</b>	<b>FAR</b>
	<p><u>on up to three sides of a common, ground-level amenity area equal to at least 20 percent of the lot area that includes usable, contiguous community green space and trees;</u></p> <ul style="list-style-type: none"> <li>• <u>1.8 for stacked dwelling units that do not meet the standards of subsection 23.44.050.D or 23.44.050.E; or</u></li> <li>• <u>2.0 for stacked dwelling units that meet either the standards of subsection 23.44.050.D or 23.44.050.E.</u></li> </ul> <p><del>((a. as provided in 23.44.050.E for stacked dwelling units and b. 1.6 for development consisting entirely of dwelling units in structures that are less than three stories and that are arranged on up to three sides of a common, ground level amenity area equal to at least 20 percent of the lot area that includes usable, contiguous community green space and trees on lots located within a frequent transit service area.))</del></p>

- 1 C. The following floor area is exempt from FAR limits:
- 2 1. All stories, or portions of stories, that are underground.
- 3 2. All portions of a story that extend no more than 4 feet above existing or
- 4 finished grade, whichever is lower, excluding access.
- 5 3. Common walls separating individual attached dwelling units.
- 6 4. Square footage of dwelling units that are Type A units as defined in the Seattle
- 7 Building Code.
- 8 D. The FAR limit for lots with stacked dwelling units that meet the following
- 9 requirements is as shown in Table A (~~B~~) for 23.44.050:

- 1 1. The lot is within one quarter mile of an elementary or secondary school;
- 2 2. At least 25 percent of the stacked dwelling units have a minimum of three
- 3 bedrooms and a minimum floor area of 1,050 square feet.

~~((Table B for 23.44.050  
Floor area ratio (FAR) in NR zones for stacked dwelling units that meet the requirements  
in subsection 23.44.050.D~~

<del>Density (dwelling units per lot size)</del>	<del>FAR</del>
<del>Less dense than 1 unit / 4,000 square feet</del>	<del>0.6</del>
<del>1 unit / 4,000 square feet to 1 unit / 2,201 square feet</del>	<del>1.0</del>
<del>1 unit / 2,200 square feet to 1 unit / 1,601 square feet</del>	<del>1.2</del>
<del>1 unit / 1,600 square feet or denser</del>	<del>1.4))</del>

4 E. The FAR limit for lots with stacked dwelling units with a density of 1 unit per 1,600  
5 square feet of lot size or denser (~~(is 1.8, except that it is 2.0 if it)~~) that meet(~~(s)~~) one of the  
6 following criteria is as shown in Table A for 23.44.050 2.0:

- 7 1. Retain a Tier 1 tree, as defined in Section 25.11.130;
- 8 2. Retain two Tier 2 trees, as defined in Section 25.11.130; or
- 9 3. Meet a Green Factor score of 0.6, as measured in Section 23.86.019.

#### 10 **23.44.060 Maximum density and minimum lot size**

11 A. Except as provided in subsection 23.44.060.C, the maximum density is:

- 12 1. For stacked dwelling units, one dwelling unit per 600 square feet of lot area;
- 13 2. For stacked dwelling units that meet one of the following criteria, one dwelling  
14 unit per 500 square feet of lot area:
  - 15 a. Retain a Tier 1 tree, as defined in Section 25.11.130;
  - 16 b. Retain two Tier 2 trees, as defined in Section 25.11.130; or
  - 17 c. Meet a Green Factor score of 0.6, as measured in Section 23.86.019;
- 18 3. Within a frequent transit service area, for development consisting entirely of  
19 dwelling units in structures that are less than three stories and that are arranged on up to three

1 sides of a common ground-level amenity area equal to at least 20 percent of the lot area that  
2 includes usable, contiguous community green space and trees, one dwelling unit per 650 square  
3 feet of lot area;

4 4. For all other dwelling units, one dwelling unit per 1,250 square feet of lot area.

5 B. The minimum lot size for lots created after the effective date of this ordinance is 5,000  
6 square feet.

7 C. Maximum density exceptions

8 1. A lot that is less than 5,000 square feet may be developed with up to four  
9 dwelling units provided that the lot does not contain any riparian corridors; wetlands and their  
10 buffers; submerged lands and areas within the shoreline setback; or designated non-disturbance  
11 area in steep slopes.

12 2. A lot that is less than 7,500 square feet and within one-quarter mile walking  
13 distance of a stop on a major transit service may be developed with up to six dwelling units if the  
14 lot does not contain any riparian corridors; wetlands and their buffers; submerged lands and areas  
15 within the shoreline setback; or designated non-disturbance area in steep slopes.

16 3. A lot that is less than 7,500 square feet and located more than one-quarter mile  
17 walking distance from a stop on a major transit service may be developed with up to six dwelling  
18 units if the lot meets the following criteria:

19 a. The lot does not contain any riparian corridors; wetlands and their  
20 buffers; submerged lands and areas within the shoreline setback; or designated non-disturbance  
21 area in steep slopes;

22 b. At least two principal dwelling units are low-income units subject to a  
23 regulatory agreement, covenant, or other legal instrument enforceable by The City of Seattle;

1 c. The low-income units are generally distributed throughout the  
2 development and have substantially the same functionality as unrestricted units in the  
3 development;

4 d. To the extent practicable, the low-income units are comparable to  
5 unrestricted units in terms of square footage and number of bedrooms and bathrooms;

6 e. The tenure (i.e., rental or ownership) of low-income units and  
7 unrestricted units is the same;

8 f. The regulatory agreement, covenant, or other legal instrument contains  
9 criteria and policies to maintain public benefit if the property is demolished or converted to a  
10 non-residential use;

11 g. For ownership housing, the low-income units are stewarded by a  
12 qualified non-profit organization, which for purposes of this subsection 23.44.060.C.3 means a  
13 non-profit organization that the Office of Housing determines as experienced in the development  
14 and stewardship of permanently affordable homes, including:

15 1) Pre-purchase verification of income and other requirements for  
16 eligible households, affordable sale price calculations for approval by the Office of Housing, and  
17 execution of legal restrictions on the property; and

18 2) Post-purchase support for homeowners by facilitating resales,  
19 monitoring compliance with financial, owner occupancy, and other legal requirements, and clear  
20 communication of program guidelines and restrictions; and

21 h. At such times as may be required by the Director of Housing but no less  
22 than annually, the property owner (for rental housing) or the qualified non-profit organization  
23 (for ownership housing) agrees to file property reports with the Office of Housing, verified upon

1 oath or affirmation, which shall contain such information as the Office of Housing may deem  
2 necessary to determine compliance with this subsection 23.44.060.C.3 and the regulatory  
3 agreement, covenant, or other legal instrument.

4           4. For lots that contain any riparian corridors, wetlands and their buffers,  
5 submerged lands and areas within the shoreline setback, or designated non-disturbance area in  
6 steep slopes, applicants may choose to develop the lot with the number of dwelling units  
7 provided in the density limits in subsection 23.44.060.A or with the number of dwelling units  
8 calculated as follows:

9                   a. Determine the number of units that would be allowed under subsections  
10 23.44.060.C.1 through 23.44.060.C.3 if no environmentally critical areas were located on the lot;

11                   b. Determine the percentage of the lot that is not covered by riparian  
12 corridors, wetlands and their buffers, submerged lands and areas within the shoreline setback, or  
13 designated non-disturbance area in steep slopes; and

14                   c. Calculate the number of dwelling units by multiplying the number of  
15 units determined in subsection 23.44.060.C.4.a by the percentage of the lot calculated in  
16 subsection 23.44.060.C.4.b. At least one dwelling unit is allowed on all lots in existence as of the  
17 effective date of this ordinance.

18           5. Square footage of dwelling units that are Type A units, as defined in the Seattle  
19 Building Code, do not count toward maximum density.

20           D. Measurement of minimum lot size and maximum density

21                   1. When calculation of the number of dwelling units allowed results in a fraction  
22 of a unit, any fraction over 0.85 constitutes one additional unit.

1                   2. Congregate residence sleeping rooms shall be treated as one-fourth of a  
2 dwelling unit for purposes of calculating density.

3                   3. In the case of a development within a unit lot subdivision, the density limit  
4 shall be applied to the parent lot as a whole.

5                   4. If dedication of right-of-way is required, permitted density shall be calculated  
6 before the dedication is made.

7                   5. When calculating density, the number of dwelling units shall include both  
8 accessory dwelling units and principal dwelling units.

9                   6. Areas not counted in calculating the lot size. The following areas shall not be  
10 counted in calculating the area of lots for the purpose of calculating the maximum density and  
11 the minimum lot size:

12                   a. Riparian corridors;

13                   b. Wetlands and their buffers;

14                   c. Submerged lands and areas within the shoreline setback; and

15                   d. Designated non-disturbance area in steep slopes.

16                   E. For the purpose of this Section 23.44.060, designated non-disturbance area in steep  
17 slopes shall include all portions of steep slope hazard areas except the following:

18                   1. Areas that are granted relief from the prohibition of development according to  
19 Section 25.09.090;

20                   2. Areas where development is allowed under a small project waiver according to  
21 Section 25.09.090; and

22                   3. Areas where intrusion into the steep slope erosion hazard area and buffer is  
23 allowed by steep slope erosion hazard area variance according to Section 25.09.290.



1 **23.44.070 Structure height**

2 A. Maximum height established

3 1. Subject to the exceptions allowed in this Section 23.44.070, ~~and except as~~  
4 ~~provided in subsection 23.44.070.A.2,~~ the height limit ~~is:~~

5 ~~a. ((for any structure in NR zones is)) 32 feet((=)) for any structure except~~  
6 ~~as otherwise provided in this subsection not listed in subsections 23.44.070.A.2 or~~  
7 ~~23.44.070.A.3;~~

8 2. The height limit is 42 for the following types of development:

9 ~~b. ((2. The height limit for)) 42 feet for a. Any~~ development with three or  
10 more principal dwelling units and a front setback of at least 20 feet ~~((is 42 feet.)) ; or~~

11 ~~e.((3. The height limit)) 42 feet for stacked b. Stacked~~ dwelling units that  
12 meet the requirements in subsection 23.44.050.D ~~((is 42 feet.)) ;~~

13 ~~d. 42 feet for stacked c. Stacked dwelling units on lots that meet a Green~~  
14 ~~Factor score of 0.6 or higher as measured in Section 23.86.019; or~~

15 ~~e. 42 feet for structures d. Structures on lots that meet one of the following~~  
16 ~~criteria:~~

17 ~~i. retains-1) Retain a Tier 1 or a Tier 2 tree, as defined in Section~~  
18 ~~25.11.130; or~~

19 ~~ii. achieves-2) Achieve a tree point score under Section 23.44.120,~~  
20 ~~through planting or preserving medium/large or large trees that would result in at least ten~~  
21 ~~percent canopy coverage for the site at tree maturity.~~

22 ~~((4.)) 2. 3.~~ The height limit for accessory structures that are located in required  
23 setbacks is 12 feet, except as follows:

1                                   a. The ridge of a pitched roof may extend up to 3 feet above the 12-foot  
2 height limit provided that all parts of the roof above the height limit shall be pitched at a rate of  
3 not less than 4:12. No portion of a shed roof is permitted to extend beyond the 12-foot height  
4 limit.

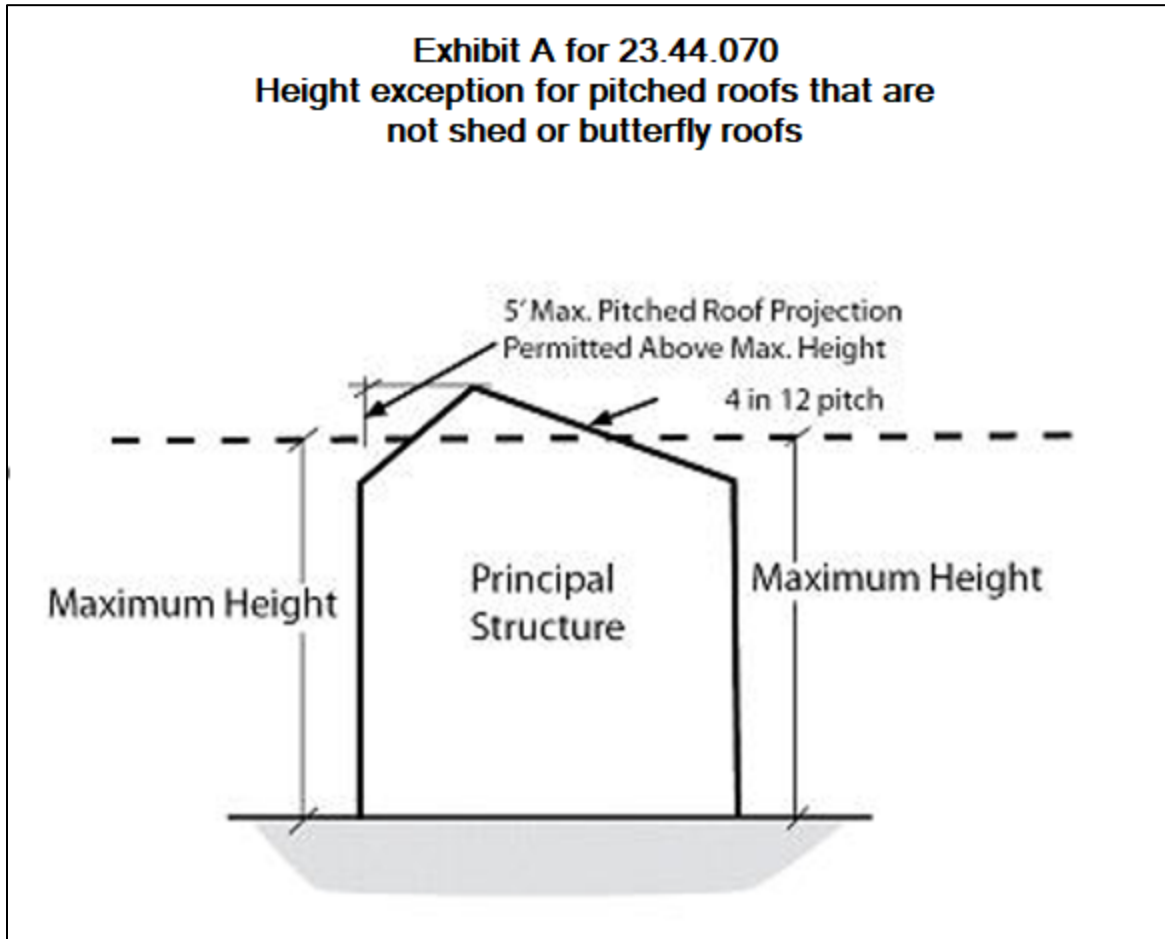
5                                   b. Freestanding flagpoles and religious symbols for religious institutions  
6 are exempt from height controls except as regulated in Chapter 23.64, provided they are no  
7 closer to any lot line than 50 percent of their height above existing grade.

8                                   B. Standards for pitched roofs

9                                   1. The ridge of a pitched roof that is not a shed or butterfly roof may extend up to  
10 5 feet above the maximum height limit, as determined under subsection 23.44.070.A. All parts of  
11 the roof above the height limit must be pitched at a rate of not less than 4:12 (see Exhibit A for  
12 23.44.070).

1 **Exhibit A for 23.44.070**

2 **Height exception for pitched roofs that are not shed or butterfly roofs**



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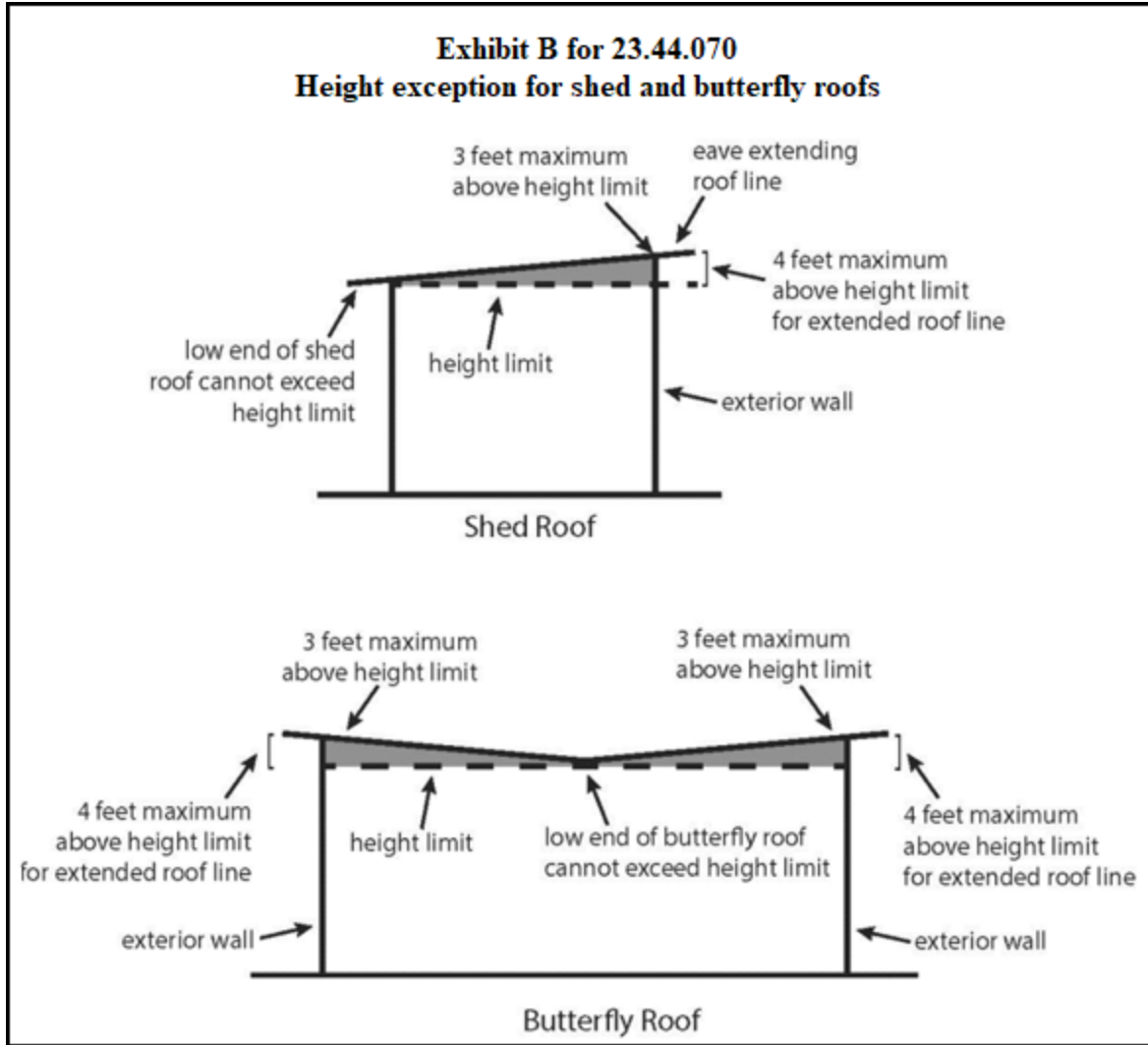
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2. The high side(s) of a shed or butterfly roof may extend 3 feet above the maximum height limit, as determined under subsection 23.44.070.A, provided that the low side(s) of the shed or butterfly roof are no higher than the height limit (see Exhibit B for 23.44.070). The roof line of a shed or butterfly roof may be extended in order to accommodate eaves, provided that the highest point of the roof extension is no more than 4 feet above the height limit.

10 **Exhibit B for 23.44.070**

11 **Height exception for shed and butterfly roofs**



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C. Height limit exceptions

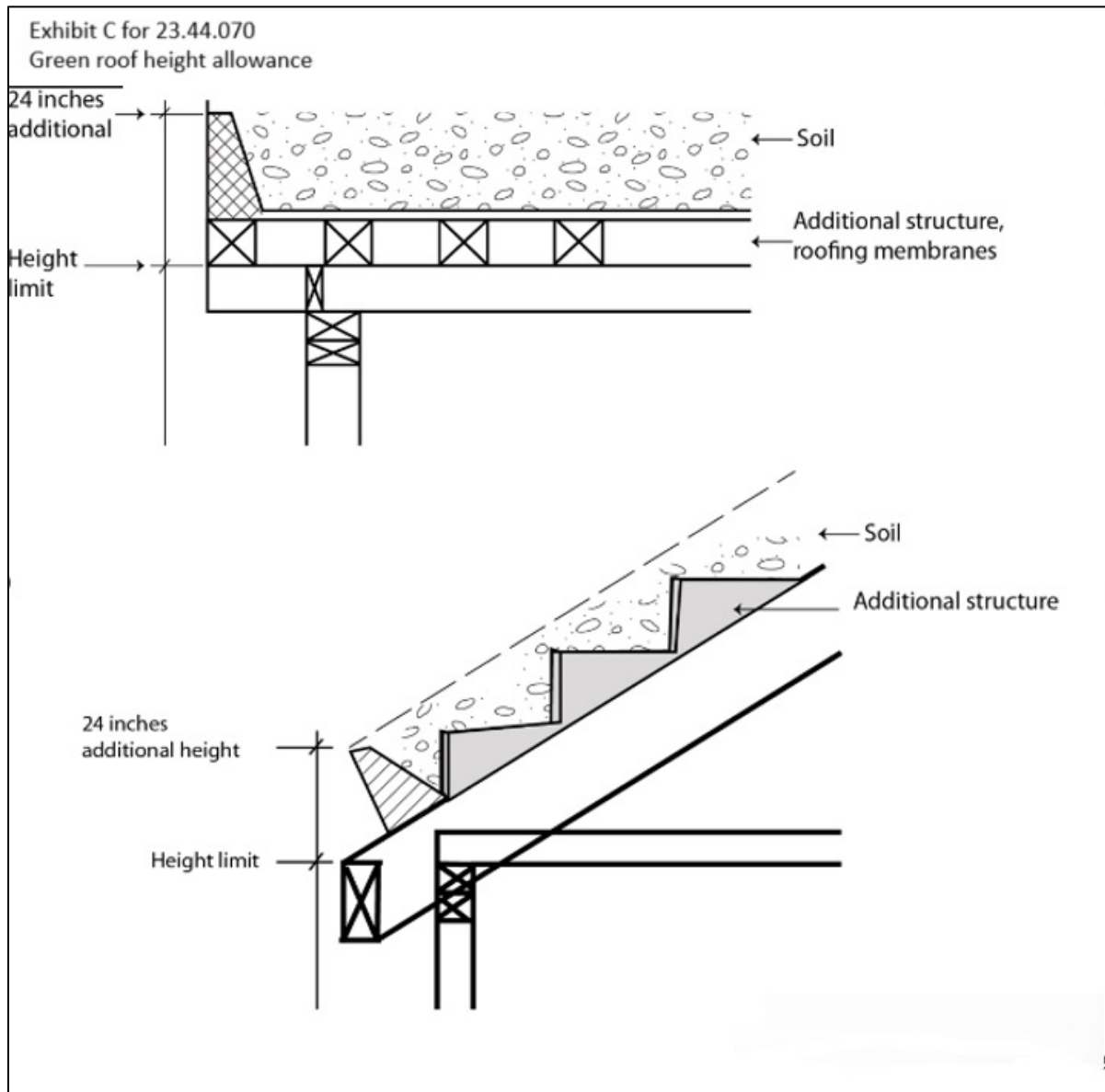
1. Except in the Airport Height Overlay District, flagpoles are exempt from height limits, provided that they are no closer to any adjoining lot line than 50 percent of their height above existing grade, or, if attached only to a roof, no closer than 50 percent of their height above the roof portion where attached.

2. Open railings, planters, greenhouses not dedicated to food production, parapets, and firewalls may extend 4 feet above the height limit in subsection 23.44.070.A. Planters on flat roofs shall not be located within 4 feet of more than 25 percent of the perimeter of the roof.

1                    3. Green roofs may extend 2 feet above the height limit in subsection 23.44.070.A  
2 or above a pitched roof allowed in subsection 23.44.070.B.

3 **Exhibit C for 23.44.070**

4 **Green roof height allowance**



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6                    4. Solar collectors may extend 4 feet above the height limit in subsection  
7 23.44.070.A or above a pitched roof allowed in subsection 23.44.070.B.

1                   5. For nonresidential principal uses, the following rooftop features may extend up  
2 to 10 feet above the height limit in subsection 23.44.070.A, as long as the combined total  
3 coverage of all features listed in this subsection 23.44.070.C.5 does not exceed 15 percent of the  
4 roof area or 20 percent of the roof area if the total includes screened or enclosed mechanical  
5 equipment:

- 6                   a. Stair and elevator penthouses;
- 7                   b. Mechanical equipment;
- 8                   c. Wind-driven power generators; or
- 9                   d. Chimneys.

10                  6. Devices for generating wind power may extend up to 10 feet above the height  
11 limit in subsection 23.44.070.A, provided that the combined total coverage of all features does  
12 not exceed 15 percent of the roof area.

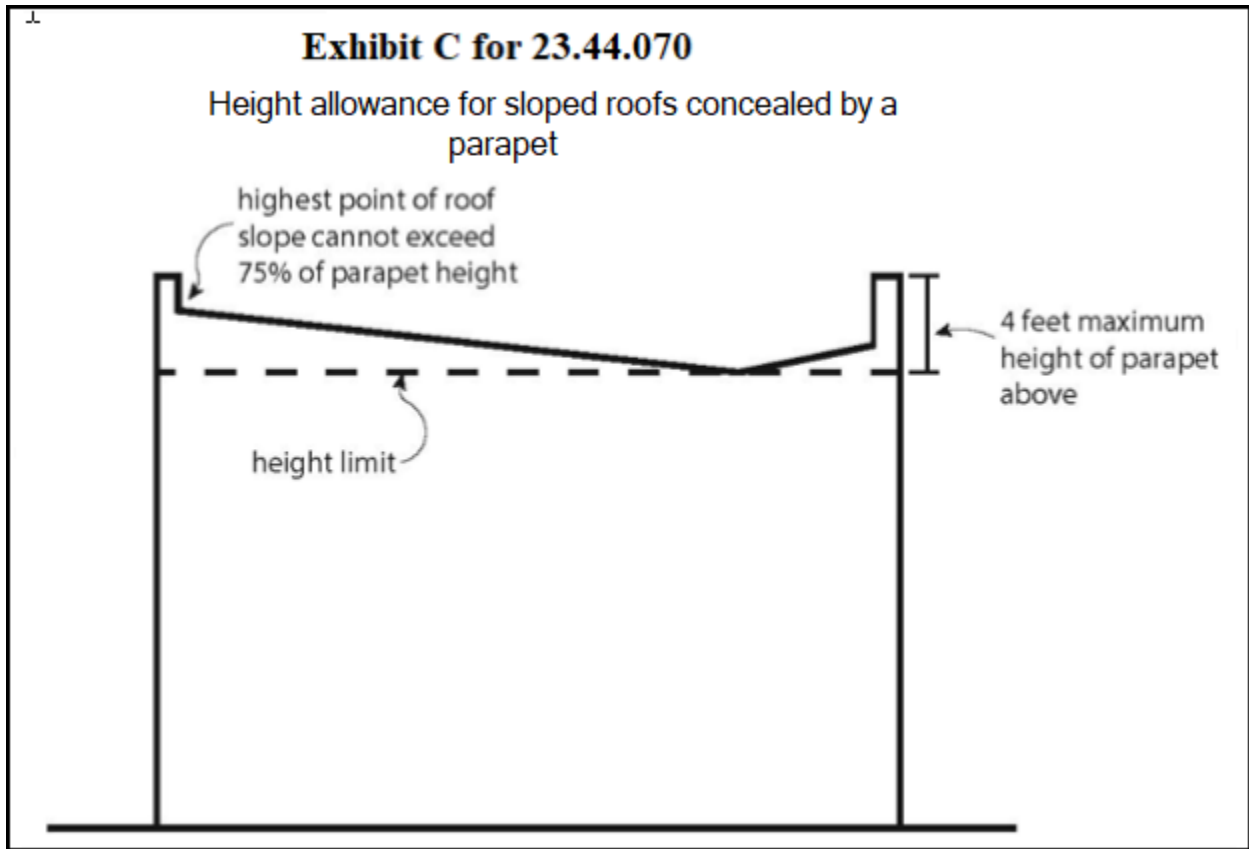
13                  7. For height limits and exceptions for communication utilities and accessory  
14 communication devices, see Section 23.57.010.

15                  8. Buildings existing prior to the effective date of this ordinance are permitted to  
16 extend up to 8 inches above the height limit in subsection 23.44.070.A or a pitched roof allowed  
17 in subsection 23.44.070.B solely for the purpose of adding insulation to an existing roof.

18                  9. Roofs enclosed by a parapet. Roof surfaces that are completely surrounded by a  
19 parapet may exceed the applicable height limit to allow for a slope, provided that the height of  
20 the highest elevation of the roof surface does not exceed 75 percent of the parapet height, and  
21 provided that the lowest elevation of the roof surface is no higher than the applicable height  
22 limit. See Exhibit C for 23.44.070.

1 **Exhibit C for 23.44.070**

2 **Height allowance for sloped roofs concealed by a parapet**



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~~((10. The height limit for stacked dwelling units that meet one of the following criteria is 42 feet:~~

~~a. Retain a Tier 1 tree, as defined in Section 25.11.130;~~

~~b. Retain two Tier 2 trees as defined in Section 25.11.130; or~~

~~c. Meet a Green Factor score of 0.6 as measured in Section 23.86.019.~~

~~11. The height limit is 42 feet for development that retains a Tier 2 tree, as defined in Section 25.11.130, or achieves a tree point score under Section 23.44.120, through planting or preserving medium and large trees that would result in a 10 percent canopy coverage for the site at tree maturity.))~~

1 **23.44.080 Lot coverage**

2 A. Except as otherwise provided in this Section 23.44.080, the maximum lot coverage  
3 allowed for structures is 50 percent.

4 B. The following areas shall not be counted in calculating the lot size for the purpose of  
5 calculating lot coverage in this Section 23.44.080:

- 6 1. Riparian corridors;
- 7 2. Wetlands and their buffers;
- 8 3. Submerged lands and areas within the shoreline setback; and
- 9 4. Designated non-disturbance area in steep slopes.

10 C. Structures not counted. The following structures and portions of structures are not  
11 counted in lot coverage calculations:

- 12 1. Underground structures;
- 13 2. The first 36 inches of architectural features such as cornices, eaves, gutters,  
14 roofs, fireplaces, chimneys, and other similar features that project from principal and accessory  
15 structures;
- 16 3. Decks or parts of a deck that are 36 inches or less above existing grade;
- 17 4. Unenclosed porches or steps no higher than 4 feet above existing grade, or the  
18 grade at the street lot line closest to the porch, whichever is lower; and
- 19 5. Unenclosed structures that meet the standards of subsection 23.44.090.H.
- 20 6. Square footage of all Type A dwelling units, as defined in the Seattle Building  
21 Code, on any floor of a structure with up to ten stacked dwelling units. Any dwelling units above  
22 or below a Type A unit are not required to be Type A units to utilize this exception ((~~that are~~  
23 Type A units as defined in Seattle Building Code)).



1 D. The lot coverage allowed on lots containing areas listed in subsection 23.44.080.B  
2 shall not be less than 625 square feet or an amount of lot coverage approved by the Director  
3 through an environmentally critical area reduction, waiver, or modification pursuant to Chapter  
4 25.09, whichever is greater.

5 E. For the purpose of this Section 23.44.080, designated non-disturbance area in steep  
6 slopes shall include all portions of steep slope hazard areas except the following:

7 1. Areas that are granted relief from the prohibition of development according to  
8 Section 25.09.090;

9 2. Areas where development is allowed under a small project waiver according to  
10 Section 25.09.090; and

11 3. Areas where intrusion into the steep slope erosion hazard area and buffer is  
12 allowed by steep slope erosion hazard area variance according to Section 25.09.290.

13 F. Within a frequent transit service area, for development consisting entirely of dwelling  
14 units in structures that are less than three stories and that are arranged on up to three(~~=~~) sides of  
15 a common ground-level amenity area equal to at least 20 percent of the lot area, that includes  
16 usable, contiguous community green space and trees, the maximum lot coverage allowed for  
17 structures is 60 percent.

18 G. The maximum lot coverage allowed on lots with stacked dwelling units is 60 percent.

19 **23.44.090 Setbacks**

20 A. Required setbacks for the NR zones are shown in Table A for 23.44.090.

**Table A for 23.44.090**  
**Required setbacks in Neighborhood Residential zones<sup>1</sup>**

Front <sup>2</sup>	Lots with one or two dwelling units: 15 feet; Lots with three or more dwelling units: 10 feet( <sup>+</sup> )
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**Table A for 23.44.090**  
**Required setbacks in Neighborhood Residential zones<sup>1</sup>**

Rear <sup>3</sup>	<p><del>((15 feet for lots))</del> <u>Lots</u> with one or two <u>principal</u> dwelling units not abutting an alley: <u>15 feet</u>;</p> <p><del>((10 feet for lots))</del> <u>Lots</u> with three or more <u>principal</u> dwelling units not abutting an alley: <u>10 feet</u>;</p> <p><del>((it is 5 feet for other structures))</del> <u>Lots under 5,000 square feet</u> within frequent transit service areas: <u>5 feet</u>; <del>((on lots under 5,000 square feet))</del></p> <p><del>((#))</del> <u>If the rear setback abuts an alley, no rear setback is required.</u></p>
Side	<p><del>((5 feet average, 3 feet minimum, except that it is 3 feet on lots))</del> <u>Lots</u> under 5,000 square feet within frequent transit service areas: <u>3 feet</u>;</p> <p><u>All other lots: 5 feet average, 3 feet minimum.</u></p>

Footnote for Table A for 23.44.090

<sup>1</sup> ~~Required setbacks for structures with nonresidential uses and no dwelling units are the required setbacks for lots with three or more dwelling units. Required setbacks for lots with nonresidential structures are the same as the required setbacks for lots with three or more dwelling units.~~

<sup>2</sup> For lots abutting landmark public right-of-way on Queen Anne Boulevard, front setbacks shall be 20 feet or the average of the front setbacks of the structures on abutting lots, whichever is less, except that if the natural gradient or slope (as measured from the front line of the lot for a distance of 60 feet or the full depth of the lot, whichever is less) is in excess of 35 percent, the required front setback depth shall be the lesser of: 20 feet less one foot for each one percent of gradient or slope in excess of 35 percent; or the average of the front setbacks on the abutting lots.

<sup>3</sup> The rear setback for accessory dwelling units is 5 feet, except that, if the rear setback abuts an alley, no rear setback is required.

1 B. Through lots. In the case of a through lot, each setback abutting a street, shall be a  
 2 front setback.

3 C. Other setback requirements. Additional structure setbacks may be required in order to  
 4 meet the provisions of Chapter 23.53.

5 D. Underground structures. Underground structures, measured from existing or finished  
 6 grade, whichever is lower, may be located within setbacks.

7 E. Projections from an enclosed structure allowed in required setbacks

1                   1. Architectural features such as cornices, eaves, gutters, roofs, fireplaces,  
2 chimneys, and other similar features may project into required setbacks a maximum of 2 feet if  
3 they are no closer than 3 feet to any lot line.

4                   2. Garden windows and other similar features that do not provide floor area may  
5 project a maximum of 18 inches into required setbacks if they:

- 6                   a. Are a minimum of 30 inches above the finished floor;
- 7                   b. Are no more than 6 feet in height and 8 feet wide; and
- 8                   c. Combined with bay windows and other similar features that provide  
9 floor area, make up no more than 30 percent of the area of the facade.

10                  3. Bay windows and other similar features that provide floor area may project a  
11 maximum of 2 feet into required front and rear setbacks if they:

- 12                  a. Are no closer than 5 feet to any lot line;
- 13                  b. Are no more than 10 feet in width; and
- 14                  c. Combined with garden windows and other projections included in  
15 subsection 23.44.090.E.2, make up no more than 30 percent of the area of the facade.

16                  4. Unenclosed porches and steps

17                   a. Unenclosed porches or steps no higher than 4 feet above existing grade,  
18 or the grade at the street lot line closest to the porch, whichever is lower, may extend to within 5  
19 feet of a street lot line and 3 feet of a side lot line.

20                   b. Porches or steps may be covered, provided that:

21                   1) No portions of the cover-structure, including any supports, are  
22 closer than 5 feet to any lot line;



1                   4. Garages and carports allowed in required setbacks shall comply with all of the  
2 following standards:

3                   a. The area of a garage or carport in front setbacks, is limited to 300  
4 square feet with 14-foot maximum width if one space is provided, and 600 square feet with 24-  
5 foot maximum width if two spaces are provided.

6                   b. Roof eaves and gutters that project up to 2 feet are excluded from the  
7 maximum coverage and size limits.

8                   c. The roof shall not be used as a balcony or deck in rear or side setbacks.

9                   H. Other unenclosed structures allowed in setbacks

10                  1. All unenclosed structures not more than 18 inches above existing or finished  
11 grade, whichever is lower, are allowed in any required setback including but not limited to decks,  
12 swimming pools, and hot tubs.

13                  2. Barrier-free access. Access facilities for the disabled and elderly, are allowed in  
14 any required setback.

15                  3. Freestanding signs, bike racks, play structures, and similar unenclosed  
16 structures that are 6 feet or less in height above existing or finished grade, whichever is lower,  
17 are allowed in any required setback, provided that:

18                   a. Signs meet the provisions of Chapter 23.55;

19                   b. Structures located in a side setback allow a 2.5-foot-wide pathway  
20 through the side setback; and

21                   c. Structures located within 5 feet of a front lot line are not more than 4  
22 feet in height.

23                  4. Fences

1                           a. Fences no greater than 6 feet in height are allowed in any required  
2 setback, except that fences in the required front setback extended to side lot lines or in street side  
3 setbacks extended to the front and rear lot lines may not exceed 4 feet in height. Fences located  
4 on top of a bulkhead or retaining wall are also limited to 4 feet. If a fence is placed on top of a  
5 new bulkhead or retaining wall used to raise grade, the maximum combined height is limited to  
6 9.5 feet.

7                           b. Except for fences in the required front setback extended to side lot lines  
8 or in street side setbacks extended to the front and rear lot lines, up to 2 feet of additional height  
9 for architectural features such as arbors or trellises on the top of a fence is allowed if the  
10 architectural features are predominately open.

11                          c. Fence height may be averaged along sloping grades for each 6-foot-long  
12 segment of the fence, but in no case may any portion of the fence exceed 8 feet in height when  
13 the height allowed by subsection 23.44.090.H.4.a is 6 feet, or 6 feet in height when the height  
14 allowed by subsection 23.44.090.H.4.a is 4 feet.

15                          5. Bulkheads and retaining walls

16                          a. Bulkheads and retaining walls used to raise grade are allowed in any  
17 required setback if they are limited to 6 feet in height, measured above existing grade.

18                          b. Bulkheads and retaining walls used to protect a cut into existing grade  
19 may not exceed the minimum height necessary to support the cut or 6 feet measured from the  
20 finished grade on the low side, whichever is greater. Any fence shall be set back a minimum of 3  
21 feet from such a bulkhead or retaining wall.

1                   6. Mechanical equipment. Heat pumps, charging devices for electric vehicles, and  
2 similar mechanical equipment, not including incinerators, are allowed in required setbacks if  
3 they are not located within 3 feet of any lot line.

4                   7. Access bridges. Uncovered, unenclosed access bridges are allowed as follows:

5                   a. Pedestrian bridges 5 feet or less in width, and of any height necessary  
6 for access, are permitted in required setbacks, except that in side setbacks an access bridge must  
7 be at least 3 feet from any side lot line.

8                   b. A driveway access bridge is permitted in the required setback abutting  
9 the street if necessary for access to parking. The vehicular access bridge shall be no wider than  
10 12 feet for access to one parking space or 22 feet for access to two or more parking spaces and of  
11 any height necessary for access. The driveway access bridge may not be located closer than 5  
12 feet to any side lot line.

13                   8. Unenclosed structures are allowed in the rear setback provided that the  
14 structure is:

15                   a. Not located within 5 feet of a rear lot line that is not an alley lot line;

16                   b. Not more than 12 feet in height; and

17                   c. Separated from a dwelling unit by at least 3 feet, eave to eave.

18                   9. Above-grade stormwater management features, such as bioretention planters  
19 and cisterns, are allowed in setbacks if:

20                   a. No feature, excluding piping, is more than:

21                                   1) Twelve feet tall if located in a portion of the rear setback that is  
22 not also a side setback; or

23                                   2) Six and a half feet tall, if located in other setbacks.

1                                   b. No feature greater than 4.5 feet tall is located within 10 feet of the front  
2 lot line, excluding piping, unless it is integrated into a bulkhead or retaining wall that is allowed  
3 in subsection 23.44.090.H.5;

4                                   c. No feature greater than 6 inches tall is located within 2.5 feet of the side  
5 lot line; and

6                                   d. The total storage capacity of all above-grade cisterns located in setbacks  
7 is no greater than 1,250 gallons.

8                                   10. Guardrails or handrails no more than 42 inches are allowed on unenclosed  
9 stairs, decks, access bridges, bulkheads, and retaining walls.

10                                  I. Other enclosed structures allowed in setbacks

11                                  1. Any accessory structure that is not a dwelling unit may be constructed in a side  
12 or rear setback that abuts the rear or side setback of another lot upon recording with the King  
13 County Recorder's Office an agreement to this effect between the owners of record of the  
14 abutting properties.

15                                  2. Enclosed structures that are not dwelling units are allowed in the rear setback  
16 provided that:

17                                   a. They are not located within 5 feet of a rear lot line that is not an alley lot  
18 line;

19                                   b. They are not more than 12 feet in height; and

20                                   c. They are separated from a dwelling unit by at least 3 feet, eave to eave.

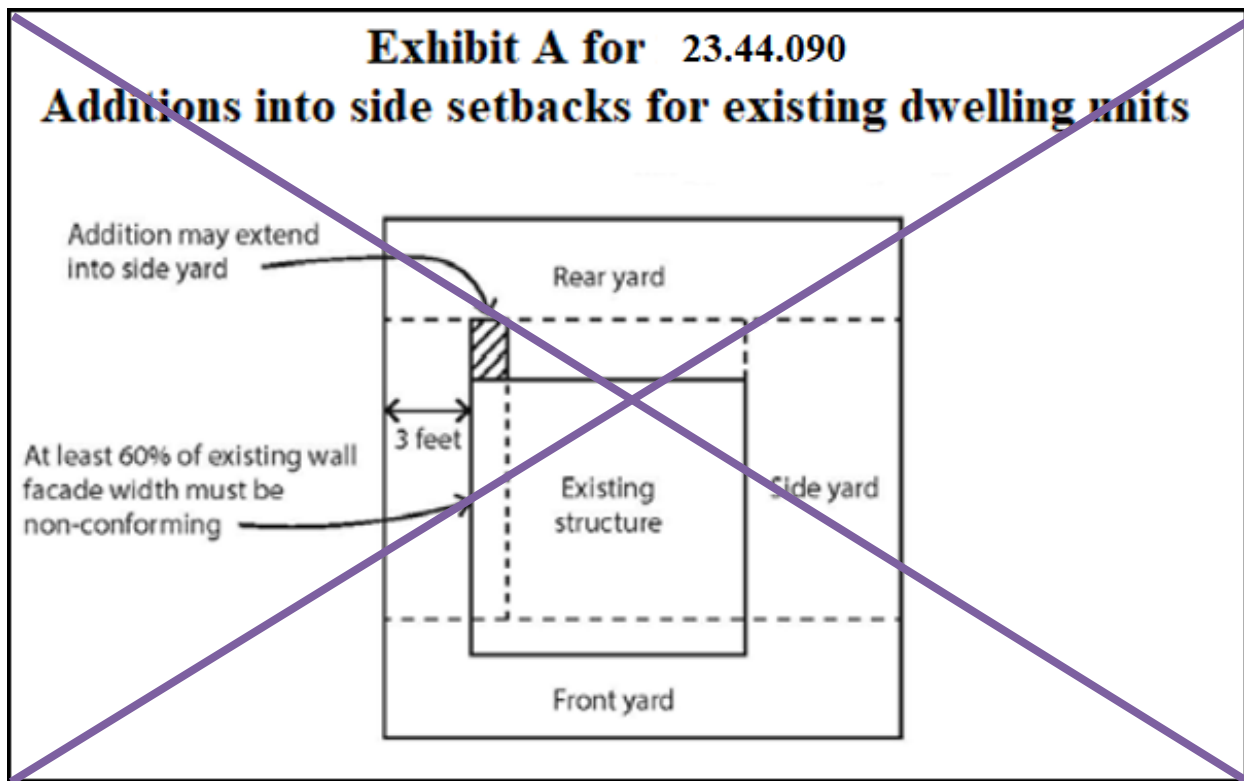
21                                  J. Certain additions. An addition to an existing dwelling unit may extend into a required  
22 side setback if:



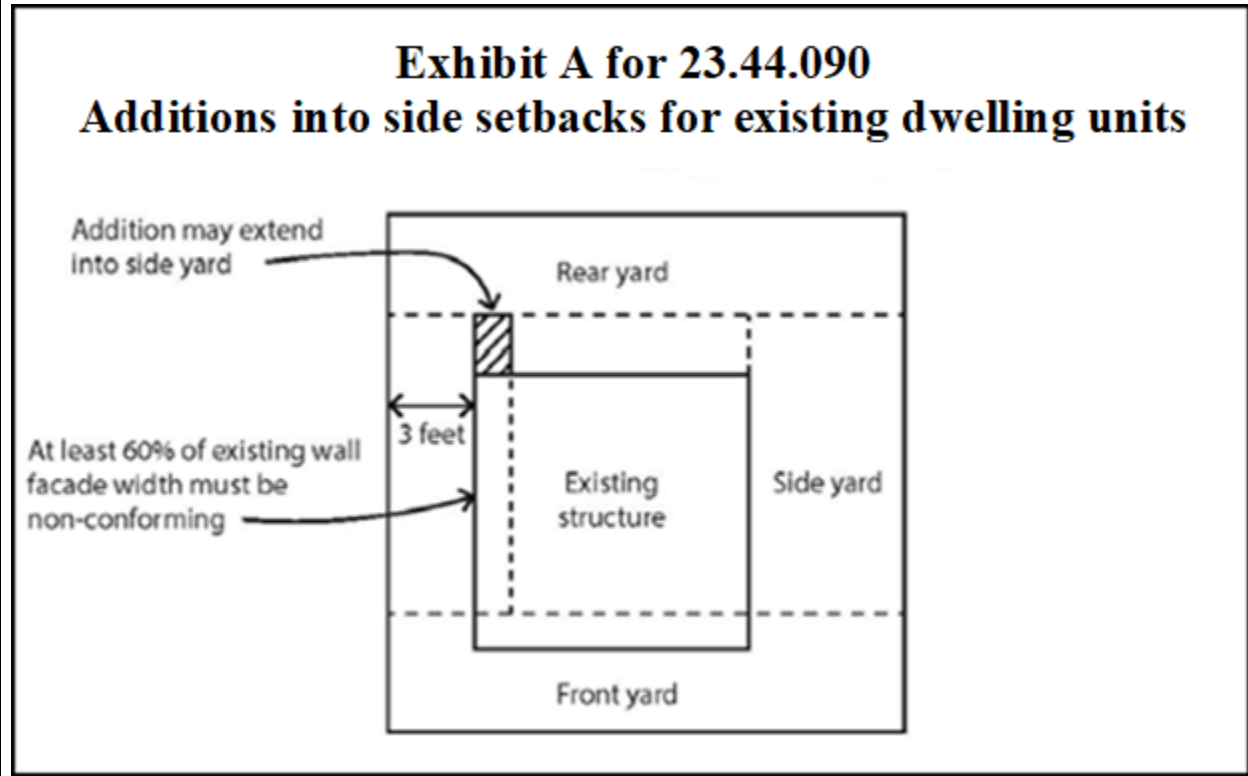
- 1                    1. The existing dwelling unit is already nonconforming with respect to that
- 2 setback;
- 3                    2. The portion of the dwelling unit that is presently nonconforming is at least 60
- 4 percent of the total width of the respective facade of the structure prior to the addition;
- 5                    3. The addition would not be located within 3 feet of a side lot line; and
- 6                    4. The addition would not be located any closer to the side lot line than the closest
- 7 part of the existing structure.

8 **Exhibit A for 23.44.090**

9 **Additions into side setbacks for existing dwelling units**



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K. A structure may be permitted to extend into front and rear setbacks as necessary to protect (~~Tier 1 and Tier 2~~) trees pursuant to Section 25.11.070.

**23.44.100 Separations between structures**

A. The minimum required separation between structures containing floor area is 5 feet except that if the structures are separated by a driveway or parking aisle, the minimum required separation between the structures is 2 feet greater than the required width of the driveway or parking aisle or 24 feet, whichever is less.

B. If structures containing floor area are separated by a driveway or parking aisle, projections that enclose floor area may extend a maximum of 3 feet into the required separation if they are at least 8 feet above finished grade.

C. Architectural features such as cornices, eaves, gutters, roofs, fireplaces, chimneys, and other forms of weather protection may project into required separations a maximum of 2 feet.

1 Garden windows, bay windows, covered porches and patios, balconies, and enclosed structures  
2 are not allowed in the required separation. Detached structures that are up to 10 feet in height and  
3 used exclusively for bike parking are allowed in required separations.

4 **23.44.110 Amenity area**

5 A. The amount of required amenity area for stacked dwelling units is equal to 25 percent  
6 of the lot area, unless every unit above the ground level has a balcony, in which case it is 20  
7 percent of the lot area. The amount of required amenity area for attached and detached units is 20  
8 percent of the lot area.

9 B. All dwelling units shall have access to either a common or private amenity area.

10 C. For attached and detached dwelling units, required ground-level amenity areas may be  
11 provided as either private or common space. For stacked dwelling units, at least 30 percent of the  
12 amenity area shall be provided as common space.

13 D. Amenity area shall not be enclosed within a structure.

14 E. Each amenity area shall be at least 120 square feet in area and have a minimum width  
15 and depth of 8 feet, except for balconies, which shall be at least 30 square feet in area and have a  
16 minimum width and depth of 4 feet.

17 F. Features in amenity areas

18 1. The following features are not allowed in amenity areas:

- 19 a. Vehicular parking areas, vehicular access easements, and driveways;  
20 b. Required bike parking;  
21 c. Solid waste and recyclable material storage area; and  
22 d. Enclosed structures.

1                   2. Pathways serving multiple dwelling units are not allowed in private amenity  
2 areas.

3                   3. Decks, porches, and steps; swimming pools, spas, and hot tubs; stormwater  
4 management features, including but not limited to bioretention planters and cisterns; play  
5 equipment; and similar features are allowed in amenity areas.

6                   4. Amenity areas may be covered by weather protection.

7                   5. Projections that do not provide floor area may extend into an amenity area if  
8 they meet the standards for projections into setbacks in subsection 23.44.090.E and if garden  
9 windows and other similar features are at least 8 feet above finished grade.

10                  6. Rooftop areas located within 8 feet of minor communication utilities and  
11 accessory communication devices do not qualify as amenity areas.

12                  G. Areas in environmentally critical areas and their buffers, including but not limited to  
13 steep slopes, may count toward amenity areas.

14                  H. No amenity area is required for:

15                    1. One new dwelling unit added to a dwelling unit existing as of January 1, 1982,  
16 or for one new dwelling unit added to a multifamily residential use existing as of October 10,  
17 2001; or

18                    2. Development that retains a Tier 2 tree, as defined in Section 25.11.130, or  
19 achieves a tree point score under Section 23.44.120, through planting or preserving medium and  
20 large trees that would result in a ((~~10~~)) ten percent canopy coverage for the site at tree maturity.

1 **23.44.120 Tree requirements**

2 A. Development containing one or more new dwelling units must plant or retain trees to  
 3 either achieve the number of tree points listed in Table A for 23.44.120 or provide at least one  
 4 new tree for every 2,500 square feet of lot area, whichever results in the greater number of trees.

<b>Table A for 23.44.120</b>	
<b>Number of tree points required</b>	
<b>Density (dwelling units per lot size)</b>	<b>Tree points required per lot area <sup>1</sup></b>
Less dense than 1 unit / 4,000 square feet	1 point / 500 square feet
1 unit / 4,000 square feet to 1 unit / 2,201 square feet	1 point / 600 square feet
1 unit / 2,200 square feet to 1 unit / 1,601 square feet	1 point / 675 square feet
1 unit / 1,600 square feet or denser	1 point / 750 square feet
Footnote to Table A for 23.44.120	
<sup>1</sup> For purposes of this Section 23.44.120, lot area shall not include submerged lands.	

5 B. Individual trees preserved during construction or planted as part of construction,  
 6 excluding street trees, count toward the tree score according to Table B for 23.44.120. Trees  
 7 required under Section 25.11.090 shall count toward this standard. All required trees shall meet  
 8 standards promulgated by the Director to provide for the long-term health and viability of  
 9 plantings. These standards may include but are not limited to tree selection, invasive species,  
 10 planting specification, soil and mulch amendment, and protection practices during construction.

<b>Table B for 23.44.120</b>			
<b>Tree points</b>			
<b>Type of tree</b>	<b>Tree species</b>	<b>Points for deciduous trees</b>	<b>Points for evergreen trees</b>
Trees planted as part of construction	Small	1 point	1.25 point
	Small/medium	2 points	2.5 points
	Medium/large	3 points	3.75 points
	Large	4 points	5 points

**Table B for 23.44.120**  
**Tree points**

Type of tree	Tree species	Points for deciduous trees	Points for evergreen trees
Trees preserved during construction	Small	0.4 point per inch of diameter	0.5 point per inch of diameter
	Small/medium	0.8 point per inch of diameter	1 point per inch of diameter
	Medium/large	1.2 point per inch of diameter	1.4 point per inch of diameter
	Large	1.6 point per inch of diameter	1.8 point per inch of diameter

1 C. Tree protection areas shall be designated in accordance with Section 25.11.060 for all  
 2 trees that are proposed to be preserved to receive points under subsection 23.44.120.B, regardless  
 3 of tree tier.

4 D. The owner of the subject lot is required to ensure that the trees planted remain healthy  
 5 for at least five years after inspection by the City, and the owner of the subject lot shall be  
 6 responsible for replacing any trees that do not remain healthy after inspection by the City.

7 E. Tree measurements

8 1. New trees planted to meet this requirement shall meet the following size  
 9 standards:

10 a. Deciduous trees with one trunk must be at least 1.5 inches in diameter,  
 11 measured 6 inches above the ground.

12 b. Multi-stemmed deciduous trees must have at least three stems and be at  
 13 least 6 feet tall.

14 c. Evergreen trees must be at least 4 feet tall.

1                   2. Existing trees shall be measured 4.5 feet above the ground.

2                   F. Tree location. New trees planted to meet this requirement shall not be planted:

- 3                   1. For small species trees, within 2 feet of a dwelling unit;
- 4                   2. For small/medium species trees, within 4 feet of a dwelling unit;
- 5                   3. For medium/large species trees, within 6 feet of a dwelling unit;
- 6                   4. For large species trees, within 8 feet of a dwelling unit; and
- 7                   5. For all trees, within 2 feet of a sidewalk located in the right-of-way.

8                   G. Street tree requirements

9                   1. Street trees are required for development that would add one or more principal  
10 dwelling units on a lot, except as provided in subsection 23.44.120.G.2 and Section 23.53.015.

11 Existing street trees shall be retained unless the Director of the Seattle Department of  
12 Transportation approves their removal. The Director, in consultation with the Director of the  
13 Seattle Department of Transportation, shall determine the number, type, and placement of  
14 additional street trees to be provided in order to:

- 15                   a. Improve public safety;
- 16                   b. Promote compatibility with existing street trees;
- 17                   c. Match trees to the available space in the planting strip;
- 18                   d. Maintain and expand the urban forest canopy;
- 19                   e. Encourage healthy growth through appropriate spacing;
- 20                   f. Protect utilities; and
- 21                   g. Allow access to the street, buildings, and lot.

22                   2. Exceptions to street tree requirements

1 a. If a lot borders an unopened right-of-way, the Director may reduce or  
2 waive the street tree requirement along that right-of-way as a Type I decision if, after  
3 consultation with the Director of the Seattle Department of Transportation, the Director  
4 determines that the right-of-way is unlikely to be opened or improved.

5 b. If it is not feasible to plant street trees in a right-of-way planting strip, a  
6 5-foot setback shall be planted with trees along the street lot line that abuts the required front  
7 setback, or landscaping other than trees shall be provided in the planting strip, subject to  
8 approval by the Director of the Seattle Department of Transportation. If a 5-foot setback or  
9 landscaped planting strip is not feasible, the Director may reduce or waive this requirement as a  
10 Type I decision.

11 **23.44.130 Structure width limits**

12 Structure width for each building containing residential uses in Neighborhood Residential zones  
13 may not exceed 90 feet. Measurement of structure width is provided in Section 23.86.014.

14 **23.44.140 Design standards**

15 A. Application of provisions.

16 1. The provisions of this Section 23.44.140 apply to development that includes the  
17 construction of new dwelling units, except for new dwelling units added within existing  
18 structures.

19 2. For the purposes of this Section 23.44.140, requirements for street-facing  
20 facades shall only apply to structures located within 40 feet of a street lot line or a vehicle access  
21 easement serving ten or more residential units. For structures located within 40 feet of a vehicle  
22 access easement serving ten or more residential units but not within 40 feet of a street lot line,  
23 the facade that faces the vehicle access easement shall be considered a street-facing facade for

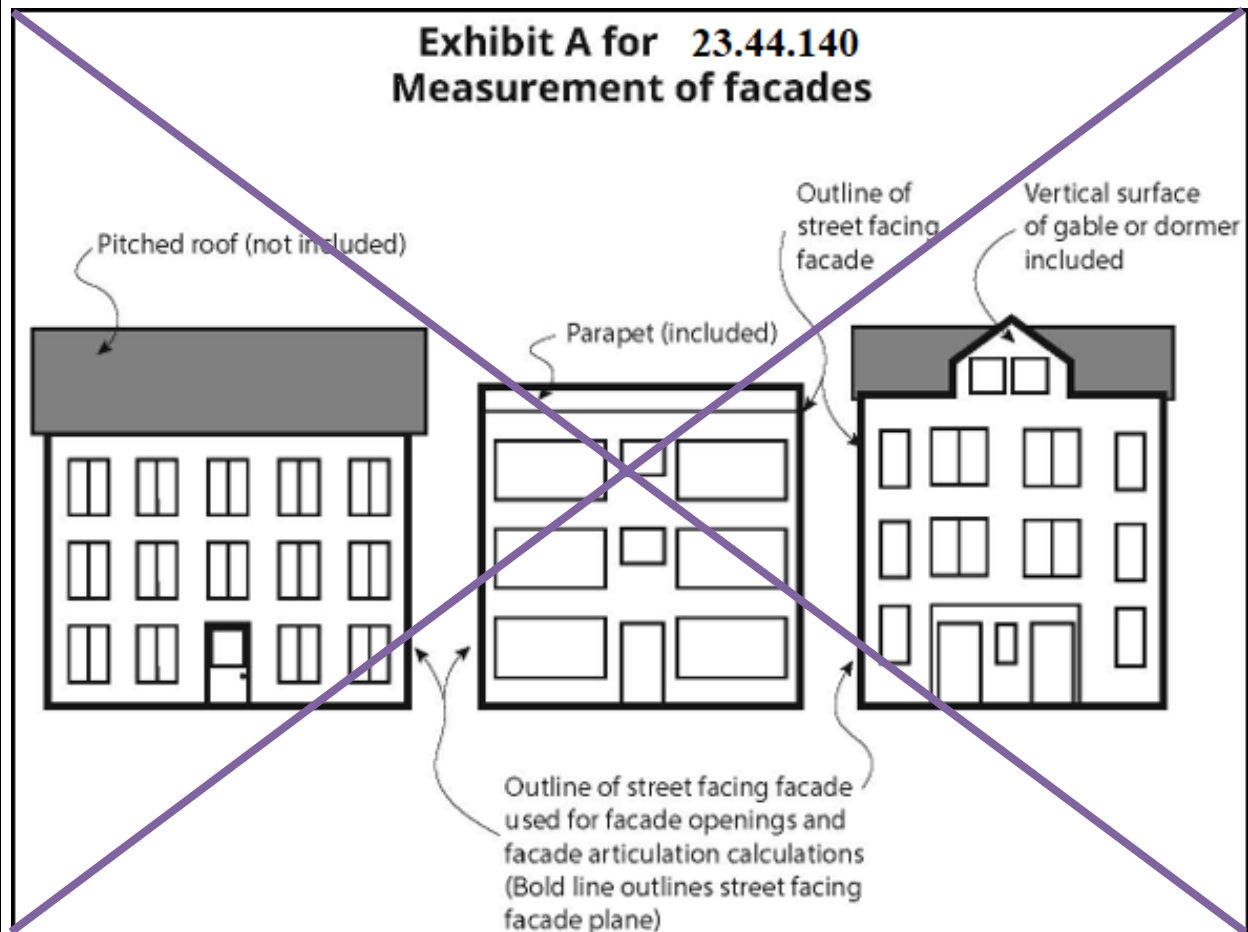


1 the purpose of this Section 23.44.140. If multiple facades face vehicle access easements, the  
2 applicant may decide which facade facing a vehicle access easement is considered the street-  
3 facing facade.

4 B. Measurement of street-facing facades. For the purposes of this Section 23.44.140, a  
5 street-facing facade includes all vertical surfaces enclosing interior space, including gables and  
6 dormers, as shown in Exhibit A for 23.44.140.

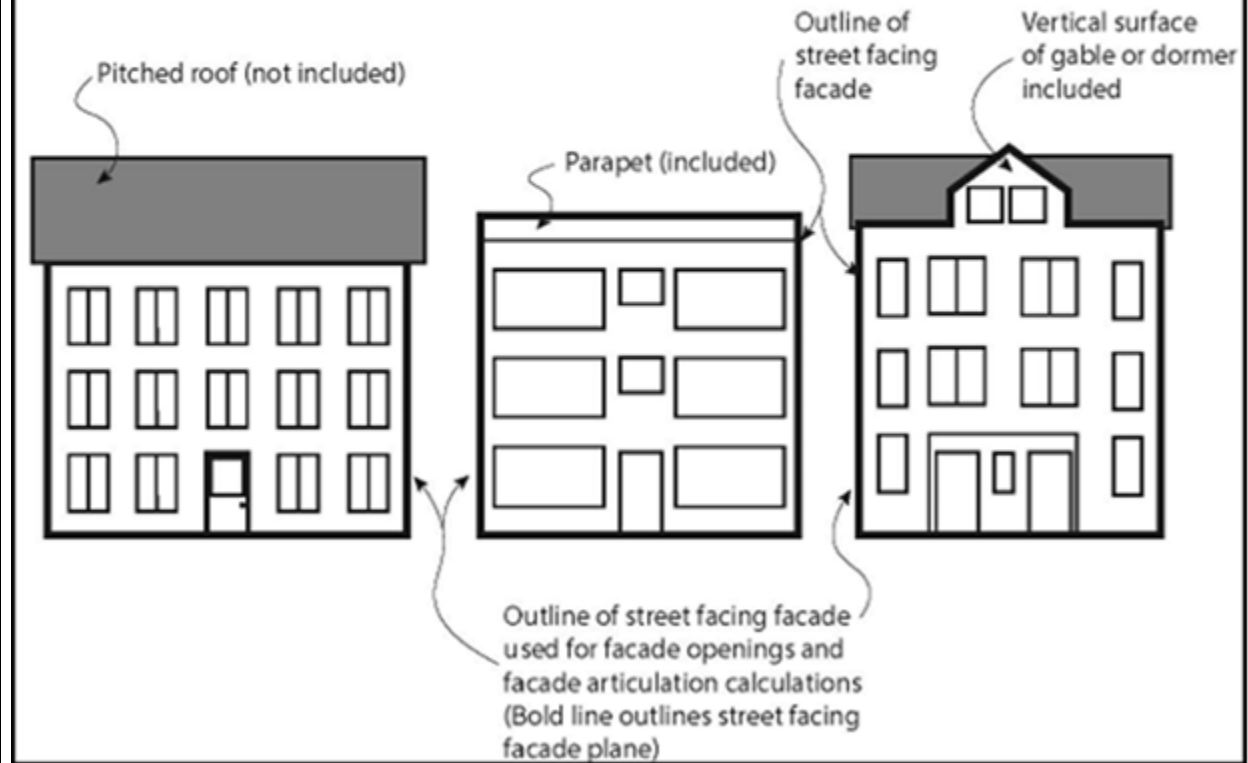
7 **Exhibit A for 23.44.140**

8 **Measurement of facades**



9

## Exhibit A for 23.44.140 Measurement of facades



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C. Pedestrian access. Each dwelling unit shall have pedestrian access at least 3 feet in width to the sidewalk or, if no sidewalk exists, the front lot line. This pedestrian access may be shared or private. This pedestrian access may cross any required setbacks or interior separation. This pedestrian access may be part of a driveway.

D. Entrances. Each structure with a street-facing facade shall have a pedestrian entry on that street-facing facade meeting the requirements of subsections 23.44.140.D.1 through 23.44.140.D.4. For attached and detached dwelling units, the pedestrian entry may be located on a wall perpendicular to the street-facing facade provided that the pedestrian entry abuts a covered porch or recessed entry that also abuts the street-facing facade.

1                   1. For stacked dwelling units, at least one pedestrian entry shall be required for  
2 the structure as a whole.

3                   2. For attached and detached dwelling units, each individual dwelling unit with a  
4 street-facing facade within 40 feet of the street lot line shall have at least one pedestrian entry on  
5 the street-facing facade.

6                   3. For structures or dwelling units with multiple street-facing facades, a pedestrian  
7 entry is required on only one of the street-facing facades.

8                   4. Required pedestrian entry on street-facing facades shall have weather  
9 protection, such as a covered porch, canopy, recessed entry, or similar feature, measuring at least  
10 3 feet by 3 feet in width and depth for attached and detached dwelling units and at least 6 feet in  
11 width and 4 feet in depth for stacked dwelling units.

12               E. Windows and doors. At least 20 percent of the area of each street-facing facade shall  
13 consist of windows and/or doors. If front and side facades are street-facing, the two facades shall  
14 be combined for the purpose of this calculation. Windows count toward the requirement for  
15 facade openings in this subsection 23.44.140.E only if they are transparent. Windows composed  
16 of garage doors and doors to utility and service areas do not count.

17 **23.44.150 Light and glare standards**

18               A. Exterior lighting shall be shielded and directed away from adjacent properties.

19               B. To prevent vehicle lights from affecting adjacent properties, driveways and parking  
20 areas for more than two vehicles shall be screened from abutting properties by a fence or wall  
21 between 5 feet and 6 feet in height, or a solid evergreen hedge or landscaped berm at least 5 feet  
22 in height. If the elevation of the lot line is different from the finished elevation of the driveway or  
23 parking surface, the difference in elevation may be measured as a portion of the required height

1 of the screen so long as the screen itself is a minimum of 3 feet in height. The Director may  
2 waive the requirement for the screening if it is not needed due to changes in topography,  
3 agreements to maintain an existing fence, or the nature and location of adjacent uses.

4 **23.44.160 Parking location and access**

5 A. Parking quantity. Off-street parking is required pursuant to Section 23.54.015.

6 B. Parking on same lot. Any required parking shall be located on the same lot as the  
7 principal use, except that parking accessory to a floating home, floating on-water residence,  
8 house barge, or vessel with a dwelling unit may be located on another lot if within 600 feet of the  
9 lot on which the floating home, floating on-water residence, house barge, or vessel with a  
10 dwelling unit is located.

11 C. Access to parking

12 1. Vehicular access to parking from an improved street, alley, or easement is  
13 required if parking is provided.

14 2. Access to parking is permitted from a street only if the Director determines that  
15 one of the following conditions exists:

16 a. There is no alley improved to the standards of subsection 23.53.030.B,  
17 and there is no unimproved alley in common usage that currently provides access to parking on  
18 the lot or to parking on adjacent lots in the same block;

19 b. Existing topography does not permit alley access;

20 c. At least 50 percent of alley frontage abuts property in a nonresidential  
21 zone;

22 d. Due to the relationship of the alley to the street system, use of the alley  
23 for parking access would create a significant safety hazard;

1 e. Parking access must be from the street in order to provide access to a  
2 parking space that complies with Chapter 11 of the Seattle Building Code; or

3 f. Providing alley access would require removal of a tree on private  
4 property that is a Tier 1 or Tier 2 tree and all other applicable criteria for tree protection in  
5 Chapter 25.11 are met.

6 D. Location of parking. Except as provided below, parking is not allowed within 20 feet  
7 of a front lot line or within 5 feet of a side street lot line:

8 1. If access to required parking passes through a required setback, automobiles,  
9 motorcycles, and similar vehicles may be parked on the open access located in a required  
10 setback.

11 2. If access is taken directly from an alley, surface parking may be located within  
12 20 feet of a street lot line if it is located within 28 feet of an alley lot line and is no closer than 7  
13 feet to any street lot line.

14 3. For lots at least 40 feet in width, up to two surface parking spaces are allowed  
15 within 20 feet of a street lot line provided:

16 a. Access to parking is allowed through the required setback abutting the  
17 street by subsection 23.44.160.C;

18 b. The parking spaces are located perpendicular to the street lot line from  
19 which they are accessed;

20 c. On corner lots, the parking spaces are not located within 20 feet of the  
21 street lot line parallel to the parking spaces;

22 d. No other parking spaces or driveways are located on the lot;

23 e. The parking spaces are not located within 10 feet of a street lot line; and

1 f. The combined width of the parking spaces shall not exceed 20 feet.

2 4. Lots with uphill setbacks abutting streets. Parking may be located in a required  
3 setback abutting a street provided:

4 a. Access to parking is allowed through the required setback abutting the  
5 street by subsection 23.44.160.C;

6 b. The existing grade of the lot slopes upward from the street lot line an  
7 average of at least 6 feet above sidewalk grade at a line that is 10 feet from the street lot line;

8 c. The parking area shall be at least an average of 6 feet below the existing  
9 grade prior to excavation and/or construction at a line that is 10 feet from the street lot line;

10 d. No other parking spaces or driveways are located on the lot;

11 e. If no garage is provided, the combined width of the parking spaces shall  
12 not exceed 20 feet. If a garage is provided, the width of a garage structure shall not exceed 24  
13 feet; and

14 f. The total width of parking spaces and garages is not more than 60  
15 percent of the width of the lot.

16 5. Lots with downhill setbacks abutting streets. Parking may be located in a  
17 required setback abutting a street if the following conditions are met:

18 a. Access to parking is allowed through the required setback abutting the  
19 street by subsection 23.44.160.C;

20 b. The existing grade slopes downward from the street lot line that the  
21 parking faces;

1 c. For parking located in a front setback, the lot has a vertical drop of at  
2 least 6 feet in the first 10 feet, measured along a line from the midpoint of the front lot line to the  
3 midpoint of the rear lot line;

4 d. Parking is not located in required side setbacks abutting a street;

5 e. No other parking spaces or driveways are located on the lot;

6 f. If no garage is provided, the combined width of the parking spaces shall  
7 not exceed 20 feet. If a garage is provided, the width of a garage structure shall not exceed 24  
8 feet; and

9 g. The total width of parking spaces and garages is not more than 60  
10 percent of the width of the lot.

11 E. No more than three vehicles may be parked outdoors per dwelling unit on a lot.

12 F. Trailers, boats, recreational vehicles, and similar equipment shall not be parked in  
13 required setbacks, unless fully enclosed in a structure otherwise allowed in a required setback by  
14 subsection 23.44.160.D.

15 G. The total combined horizontal width of all garage entrances that are located on front  
16 facades may not be more than 50 percent of the horizontal width of the street-level front facades  
17 or 10 feet, whichever is greater. No dwelling unit may have a garage entrance on both a front  
18 facade and a side facade.

19 H. Except as provided in subsections 23.44.160.D.4 and 23.44.160.D.5, garage entrances  
20 facing the street shall be set back at least 20 feet from the street lot line.

21 **23.44.170 Alternative standards for development of low-income housing and social housing**

22 A. Development of low-income housing or social housing that meets all of the following  
23 criteria may meet the alternative development standards in subsection 23.44.170.B:

1                   1. The restricted units are generally distributed throughout the development and  
2 have substantially the same functionality as unrestricted units, if any, in the development;

3                   2. To the extent practicable, the restricted units are comparable to unrestricted  
4 units, if any, in terms of square footage and number of bedrooms and bathrooms;

5                   3. The tenure (i.e., rental or ownership) of restricted units and unrestricted units, if  
6 any, is the same;

7                   4. For ownership housing, the restricted units are stewarded by a qualified non-  
8 profit organization, which for purposes of this subsection 23.44.170.A means a non-profit  
9 organization that the Office of Housing determines as experienced in the development and  
10 stewardship of permanently affordable homes, including:

11                   a. Pre-purchase verification of income and other requirements for eligible  
12 households, affordable sale price calculations for approval by the Office of Housing, and  
13 execution of legal restrictions on the property; and

14                   b. Post-purchase support for homeowners by facilitating resales,  
15 monitoring compliance with financial, owner occupancy, and other legal requirements, and clear  
16 communication of program guidelines and restrictions; and

17                   5. At such times as may be required by the Director of Housing but no less than  
18 annually, the property owner (for rental housing) or the qualified non-profit organization (for  
19 ownership housing) agree to file property reports with the Office of Housing, verified upon oath  
20 or affirmation, which shall contain such information as the Office of Housing may deem  
21 necessary to determine compliance with this subsection 23.44.170.A and the regulatory  
22 agreement, covenant, or other legal instrument.



1           B. Proposed development on a lot meeting the criteria in subsection 23.44.170.A may  
2 elect to meet the following development standards in lieu of the standards in subsections  
3 23.44.050.B (floor area), ~~23.44.060.B~~ 23.44.060.A (density), and 23.44.070.A (structure height),  
4 and Sections 23.44.080 (lot coverage) and 23.54.015 (parking):

5                   1. The maximum floor area ratio (FAR) limit is 2.0. The applicable FAR limit  
6 applies to the total chargeable floor area of all structures on the lot.

7                   2. The maximum density limit is one unit per 400 square feet.

8                   3. The maximum height limit is 42 feet.

9                   4. The maximum lot coverage is 60 percent.

10                  5. No minimum required parking.

11 **23.44.180 Institutions**

12           A. Institutions located in a Neighborhood Residential zone shall meet the development  
13 standards of this Section 23.44.180 and other sections of Chapter 23.44 except as provided in  
14 Section 23.44.030, Chapter 23.51B, Chapter 23.69, or Chapter 23.79. In the event of conflict  
15 between the standards in this Section 23.44.180 and other sections of Chapter 23.44, the  
16 standards in this Section 23.44.180 shall control.

17           B. Height limits

18                   1. The height limit for institutions shall be 32 feet, except as provided in  
19 subsection 23.44.180.B.2.

20                   2. For gymnasiums, auditoriums, and wood shops that are accessory to an  
21 institution, the maximum permitted height is 35 feet if all portions of the structure above the  
22 height limit of the zone are set back at least 20 feet from all lot lines. Pitched roofs on the  
23 auditorium, gymnasium, or wood shop with a slope of not less than 4:12 may extend 10 feet

1 above the 35-foot height limit. No portion of a shed roof on a gymnasium, auditorium, or wood  
2 shop is permitted to extend beyond 35 feet.

3 C. Landscaping

4 1. Landscaping that achieves a Green Factor score of 0.3 or greater, pursuant to  
5 Section 23.86.019, is required for any lot with:

6 a. Development, either a new structure or an addition to an existing  
7 structure, containing more than 4,000 new square feet of non-residential uses; or

8 b. Any parking lot containing more than 20 new parking spaces for  
9 automobiles.

10 2. All required trees shall meet standards promulgated by the Director to provide  
11 for the long-term health, viability, and coverage of plantings. These standards may include, but  
12 are not limited to, the type and size of plants, spacing of plants, depth, and quality of soil, access  
13 to light and air, and protection practices during construction.

14 D. Parking

15 1. Location of parking. Parking areas and facilities may be located anywhere on  
16 the lot except in the required front setback or side street setback.

17 2. Screening of surface parking areas. Surface parking areas for more than five  
18 vehicles shall be screened in accordance with the following requirements:

19 a. Screening shall be provided on each side of the parking area that abuts,  
20 or faces across a street, alley, or access easement, a lot in a residential zone.

21 b. Screening shall consist of a fence, solid evergreen hedge, or wall at least  
22 3 feet in height.

1 E. Odors. The venting of odors, vapors, smoke, cinders, dust, gas, and fumes shall be at  
2 least 10 feet above finished sidewalk grade and directed away to the extent possible from  
3 residential uses within 50 feet of the vent.

4 F. Light and glare

5 1. Exterior lighting for institutions shall be shielded or directed away from  
6 residential structures on adjacent lots.

7 2. Poles for freestanding exterior lighting are permitted up to a maximum height  
8 of 32 feet. Light poles for illumination of athletic fields on new and existing public school sites  
9 will be allowed to exceed 30 feet pursuant to Chapter 23.51B.

10 G. The Director may allow, as a Type I decision, higher fencing in a required setback  
11 when necessary for sports fields.

12 **23.44.190 Parks and open space**

13 A. The following accessory uses shall be permitted in public parks when within a  
14 structure or on a terrace abutting the structure, provided that when the use is within 100 feet of  
15 another lot in a residential zone the use is completely enclosed:

16 1. The sale and consumption of beer and wine during daylight hours;

17 2. The sale and consumption of alcoholic beverages under a Class H liquor license  
18 at municipal golf courses during established hours of operation.

19 B. The sale and consumption of beer and wine with meals served in a restaurant facility  
20 within the boundaries of Woodland Park shall be permitted. The use shall be permitted in only  
21 one facility located no closer than 100 feet from any lot in a residential zone and separated from  
22 other public activity areas and zoo buildings by at least 50 feet.

1 C. Storage structures and areas and other structures and activities customarily associated  
2 with parks and playgrounds are subject to the following development standards in addition to the  
3 general development standards for accessory uses:

4 1. Any active play area shall be located 30 feet or more from any lot in a  
5 Neighborhood Residential zone;

6 2. Garages and service or storage areas shall be located 100 feet or more from any  
7 other lot in a residential zone and obscured from view from each such lot.

8 Section 32. Section 23.45.502 of the Seattle Municipal Code, last amended by Ordinance  
9 125791, is amended as follows:

10 **23.45.502 Scope of provisions**

11 \* \* \*

12 D. Other regulations~~((;))~~ may apply to development proposals including but not limited to  
13 general use provisions (Chapter 23.42); transportation concurrency and transportation impact  
14 mitigation (Chapter 23.52); requirements for streets, alleys, and easements (Chapter 23.53);  
15 standards for access, off-street parking, ((quantity, access, and design)) and solid waste storage  
16 (Chapter 23.54); ~~((standards for solid waste storage (Chapter 23.54)))~~; ~~((signs))~~ sign regulations  
17 (Chapter 23.55); communication regulations (Chapter 23.57); ~~((and methods for measurements~~  
18 ~~(Chapter 23.86), may apply to development proposals))~~ shoreline regulations (Chapter 23.60A);  
19 and environmental protection and historic preservation (Title 25).

20 E. Congregate residences are subject to additional requirements as specified in Section  
21 23.42.049.

22 Section 33. Section 23.45.504 of the Seattle Municipal Code, last amended by Ordinance  
23 127098, is amended as follows:

1 **23.45.504 Permitted and prohibited uses**

2 A. All uses are permitted outright, prohibited, or permitted as a conditional use according  
 3 to Table A for 23.45.504 and this Section 23.45.504. Uses not referred to in Table A for  
 4 23.45.504 are prohibited, unless otherwise indicated in this Chapter 23.45 or Chapters 23.51A,  
 5 23.51B, or 23.57. Communication utilities and accessory communication devices, except as  
 6 exempted in Section 23.57.002, are subject to ~~((the regulations in))~~ this Chapter 23.45 and  
 7 ~~((additional regulations in))~~ Chapter 23.57. Public facilities are subject to ~~((the regulations in))~~  
 8 Section 23.51A.004.

9 B. All permitted uses are allowed as a principal use or as an accessory use, unless  
 10 otherwise indicated in this Chapter 23.45.

<b>Table A for 23.45.504 Permitted and prohibited uses</b>		
Uses	Permitted and prohibited uses by zone	
	LR1, LR2, and LR3	MR and HR
* * *		
C. Uses <u>not otherwise permitted</u> in existing or former public schools	<u>Permitted pursuant to procedures established in Chapter 23.78</u>	<u>Permitted pursuant to procedures established in Chapter 23.78</u>
<del>((C.1. Child care centers, preschools, public or private schools, educational and vocational training for the disabled, adult evening education classes, nonprofit libraries, community centers, community programs for the elderly, and similar uses in existing or former public schools</del>	P	P

**Table A for 23.45.504  
 Permitted and prohibited uses**

Uses	Permitted and prohibited uses by zone	
	LR1, LR2, and LR3	MR and HR
<del>C.2. Other non-school uses in existing or former public schools</del>	Permitted pursuant to procedures established in Chapter 23.78	Permitted pursuant to procedures established in Chapter 23.78))
* * *		
E. Parks and <del>((playgrounds including customary))</del> open space uses	P	P
F. Ground-floor commercial uses	RC/P <sup>4</sup>	RC/P <del>((4))</del> <sup>5</sup>
* * *		
<u>L. Heat recovery incinerators</u>	<u>CU</u>	<u>CU</u>
<u>M. Human service uses</u>	<u>P</u>	<u>P</u>
<del>((L-))</del> <u>N. All other uses</u>	X	X

Key to Table A for 23.45.504

P = Permitted outright

CU = Permitted as an administrative conditional use

RC = Permitted in areas zoned Residential Commercial (RC), and subject to the provisions of the RC zone, Chapter 23.46

X = Prohibited

Footnotes to Table A for 23.45.504

<sup>1</sup> Institutions meeting development standards including but not limited to the standards in Section 23.45.570 are permitted outright; all others are administrative conditional uses pursuant to Section 23.45.506. The provisions of this Chapter 23.45 shall apply to Major Institution uses as provided in Chapter 23.69.

<sup>2</sup> Prohibited in Station Area Overlay Districts (SAODs); otherwise, permitted as an administrative conditional use pursuant to Section 23.45.506 on surface parking existing as of January 1, 2017.

**Table A for 23.45.504  
 Permitted and prohibited uses**

Uses	Permitted and prohibited uses by zone	
	LR1, LR2, and LR3	MR and HR
<p><sup>3</sup> Prohibited in LR1 and LR2 zones, including LR1/RC and LR2/RC. Permitted outright in LR3, MR, HR, and LR3/RC zones, except prohibited in <del>((the))</del> a SAOD.</p> <p><sup>4</sup> <del>((Permitted in development that meets))</del> For lots located in a zone that does not include an RC designation, ground-floor commercial uses are allowed if they meet the requirements of Section 23.42.055 and Chapter 23.46 or the standards of subsection 23.45.504.D <del>((even if it is not located in a zone that includes an RC designation))</del>.</p> <p><sup>5</sup> <del>((Subject to subsection 23.45.504.E except in zones that include an RC designation.))</del> For lots located in a zone that does not include an RC designation, ground-floor commercial uses are allowed if they meet the standards of subsection 23.45.504.E and Section 23.45.532.</p> <p><sup>6</sup> Subject to subsections 23.45.504.G and 23.45.506.F.</p> <p><sup>7</sup> Subject to subsection 23.45.504.F.</p> <p><sup>8</sup> Prohibited in LR1 and LR2 zones. Permitted outright in all other multifamily zones as surface parking on surface parking lots existing as of January 1, 2017; permitted outright in garages; subject to Section 23.54.026.</p> <p><del>((P = Permitted outright            CU = Permitted as an Administrative Conditional Use            RC = Permitted in areas zoned Residential Commercial (RC), and subject to the provisions of the RC zone, Chapter 23.46            X = Prohibited))</del></p>		

1 C. Accessory uses. The following accessory uses are permitted in all multifamily zones,  
 2 subject to ~~((the standards in))~~ Section 23.45.545, if applicable:

- 3 1. Private garages and carports;
- 4 2. Private, permanent swimming pools, hot tubs, and other similar uses;
- 5 3. Solar collectors, including solar greenhouses;
- 6 4. ~~((Open wet moorage accessory to residential structures;))~~ Piers and floats,  
 7 provided they comply with Chapter 23.60A;
- 8 5. Uses accessory to parks and playgrounds, pursuant to Section 23.45.578;

1                   6. Bed and breakfasts in a dwelling unit that is at least five years old, provided  
2 they comply with subsection 23.45.504.I;

3                   7. Recycling collection stations;

4                   8. Urban farms with planting area not more than 4,000 square feet. Urban farms  
5 with greater than 4,000 square feet of planting area may be allowed as an administrative  
6 conditional use to any use permitted outright or as a conditional use. The Director may grant,  
7 condition, or deny a conditional use permit in accordance with subsection 23.42.051.B; and

8                   9. Accessory dwelling units provided they comply with Section 23.42.022.

9                   D. (~~Heat recovery incinerators may be permitted as accessory administrative conditional~~  
10 ~~uses, pursuant to Section 23.45.506.)~~ Ground-floor commercial use in Lowrise zones without an  
11 RC suffix are allowed if they comply with the following:

12                   ~~((1. The commercial use is located on a corner lot or on a lot that abuts both a~~  
13 ~~street and an alley.~~

14                   ~~2.))~~ 1. The commercial use is limited to the following:

15                   a. Food processing and craft work;

16                   b. General sales and services; and

17                   c. Restaurants.

18                   ~~((3.))~~ 2. The commercial uses do not occupy more than 2,500 square feet of gross  
19 floor area.

20                   ~~((4.))~~ 3. The commercial use is permitted only on or below the ground floor of a  
21 structure.

22                   ~~((5.))~~ 4. Vents for venting of odors, vapors, smoke, gas and fumes, and exterior  
23 heat exchangers and other similar devices (e.g., related to ventilation, air conditioning, or



1 refrigeration) shall be at least 10 feet above finished sidewalk grade and directed away to the  
2 extent possible from residential uses within 50 feet of the vent.

3 ~~((6))~~ 5. Drive-in businesses are prohibited as a principal or accessory use.

4 ~~((7))~~ 6. Outdoor sales of food or beverages must be located at least 50 feet from  
5 adjacent lots.

6 ~~((8))~~ 7. Outdoor service of food or beverages must be located at least 50 feet  
7 from adjacent lots.

8 ~~((9))~~ 8. Businesses may not be open between the hours of 10 p.m. and 6 a.m.

9 E. Ground-floor commercial use in Midrise and Highrise zones without an RC suffix are  
10 allowed if they comply with the following:

11 1. Drive-in businesses are prohibited~~((;))~~ as either a principal or accessory use.

12 2. ~~((The following uses are permitted as ground-floor commercial uses in MR and~~  
13 ~~HR zones pursuant to Section 23.45.532:))~~ The commercial use is limited to the following:

14 a. Business support services;

15 b. Food processing and craft work;

16 c. General sales and services;

17 d. Medical services;

18 e. Offices;

19 f. Restaurants; and

20 g. Live-work units with one of the uses permitted in this subsection

21 23.45.504.E as the permitted commercial use.

22 3. The ground-floor commercial uses meet the requirements of Section 23.45.532.

1 F. Existing cemeteries are permitted to continue in use. New cemeteries are prohibited  
2 and existing cemeteries are prohibited from expanding. For purposes of this Section 23.45.504, a  
3 change in a cemetery boundary is not considered an expansion in size and is permitted provided  
4 that:

- 5 1. The change does not increase the net land area occupied by the cemetery;
- 6 2. The land being added to the cemetery is contiguous to the existing cemetery  
7 and is not separated from the existing cemetery by a public street or alley whether or not  
8 improved; and
- 9 3. The use of the land being added to the cemetery will not result in the loss of  
10 housing.

11 G. Except as provided in subsections 23.45.504.G.1 and 23.45.504.G.2 (~~below~~),  
12 medical service uses other than permitted ground-floor commercial uses are prohibited.

- 13 1. Medical service uses in HR zones may be permitted as administrative  
14 conditional uses pursuant to subsection 23.45.506.F.
- 15 2. Medical service uses meeting the development standards for institutions are  
16 permitted outright on property conveyed by a deed from the City that, at the time of conveyance,  
17 restricted the property's use to a health care or health-related facility.

18 H. Fences and free-standing walls of utility services uses shall be set back from the street  
19 lot line by an average of 7 feet and be no less than 5 feet from the street lot line at any point.  
20 Landscaping shall be provided between the fence or wall and the street lot line. The Director  
21 may reduce this setback after finding that the reduced setback will not significantly increase  
22 project impacts, including but not limited to noise, odor, and the scale of the structure in relation  
23 to nearby buildings. Acceptable methods to reduce fence or wall impacts include changes in the

1 height, design, or construction of the fence or wall, including the use of materials, architectural  
2 detailing, artwork, vegetated trellises, decorative fencing, or similar features to provide visual  
3 interest facing the street lot line. Fences and walls may obstruct or allow views to the interior of  
4 a site. Where site dimensions and conditions allow, applicants are encouraged to provide both a  
5 landscaped setback between the fence or wall and the right-of-way, and a fence or wall that  
6 provides visual interest facing the street lot line, through the height, design, or construction of the  
7 fence or wall, including the use of materials, architectural detailing, artwork, vegetated trellises,  
8 decorative fencing, or similar features.

9 I. Bed and breakfast uses. A bed and breakfast use may be operated in a principal  
10 dwelling unit or an accessory dwelling unit under the following conditions:

11 1. The bed and breakfast use has a valid business license tax certificate issued by  
12 the Department of Finance and Administrative Services;

13 2. The bed and breakfast use is operated by the primary resident of the dwelling  
14 unit where the bed and breakfast is located or the resident operator; and

15 3. There is no evidence of a bed and breakfast use visible from the exterior of the  
16 dwelling unit other than a sign permitted by subsection 23.55.022.D.1.

17 Section 34. Section 23.45.508 of the Seattle Municipal Code, last amended by Ordinance  
18 127098, is amended as follows:

19 **23.45.508 General provisions**

20 A. Except for structures related to an urban farm, a structure occupied by a permitted use  
21 other than a residential use may be partially or wholly converted to a residential use even if the  
22 structure does not conform to the development standards for residential uses in multifamily  
23 zones.

1           B. (~~Off street parking shall be provided pursuant to Section 23.54.015, and as permitted~~  
2 ~~by provisions of Sections 23.45.504 and 23.45.506, if applicable.~~

3           ~~C.))~~ Expansions of nonconforming converted structures and conversions of structures  
4 occupied by nonconforming uses are regulated by Sections 23.42.108 and 23.42.110.

5           ~~((D. Methods for measurements are provided in Chapter 23.86. Requirements for streets,~~  
6 ~~alleys, and easements are provided in Chapter 23.53. Standards for parking and access and~~  
7 ~~design are provided in Chapter 23.54. Standards for solid waste and recyclable materials storage~~  
8 ~~space are provided in Section 23.54.040. Standards for signs are provided in Chapter 23.55.~~

9           ~~E.))~~ C. Assisted living facilities, congregate residences, nursing homes, and structures  
10 containing ground floor commercial uses as allowed by Chapter 23.46 in RC zones shall meet  
11 the development standards for ~~((apartments))~~ stacked dwelling units unless otherwise specified.

12           ~~((F. Single family dwelling units. In LR zones, single family dwelling units shall meet~~  
13 ~~the development standards for townhouse developments, except as otherwise provided. In MR~~  
14 ~~and HR zones, single family dwelling units shall meet the development standards of the zone.~~

15           ~~G. Proposed uses in all multifamily zones are subject to the transportation concurrency~~  
16 ~~level of service standards prescribed in Chapter 23.52.~~

17           ~~H.))~~ D. Lots with no street frontage. For purposes of structure width, depth, and setbacks,  
18 multifamily zoned lots that have no street frontage are subject to the following:

19                   1. For lots that have only one alley lot line, the alley lot line shall be treated as a  
20 front lot line.

21                   2. For lots that have more than one alley lot line, the Director shall determine  
22 which alley lot line shall be treated as the front lot line.

1                   3. For lots that have no alley lot lines, the applicant may choose the front lot line  
2 provided that the selected front lot line length is at least 50 percent of the width of the lot.

3                   ~~((F.))~~ E. Any other provision of the Seattle Municipal Code notwithstanding, an applicant  
4 is not entitled to a permit for any use or development on a lot in an LR zone that would be  
5 inconsistent with any term, condition, or restriction contained either in any recorded agreement  
6 that is in effect as to that lot and was made in connection with a rezone of the lot to LDT, L1, L2,  
7 L3, or L4, or in any City Council decision or ordinance related to a rezone of the lot to LDT, L1,  
8 L2, L3, or L4 conditioned on a recorded agreement prior to April 19, 2011.

9                   ~~((F.))~~ F. If more than one category of residential use is located on a lot, and if different  
10 development standards apply to the different categories of use, then each category's percentage  
11 of the total limit imposed by the development standard shall be calculated based on each  
12 category's percentage of total structure footprint area, as follows:

13                   1. Calculate the footprint, in square feet, for each category of residential use. For  
14 purposes of this calculation, "footprint" is defined as the horizontal area enclosed by the exterior  
15 walls of the structure.

16                   2. Calculate the total square feet of footprint of all categories of residential uses  
17 on the lot.

18                   3. Divide the square footage of the footprint for each category of residential  
19 structure in subsection ~~((23.45.508.J.1))~~ 23.45.508.F.1 by the total square feet of footprints of all  
20 residential uses in subsection ~~((23.45.508.J.2))~~ 23.45.508.F.2.

21                   4. Multiply the percentage calculated in subsection ~~((23.45.508.J.3))~~  
22 23.45.508.F.3 for each housing category by the area of the lot. The result is the area of the lot  
23 devoted to each housing category.

1                   5. The total limit for each category of residential use is the applicable limit for  
2 that use multiplied by the percentage calculated in subsection ~~((23.45.508.J.4))~~ 23.45.508.F.4.

3                   ~~((K))~~ G. Unless otherwise specified, the development standards of each zone shall be  
4 applied in that zone, and may not be used in any other zone, except that if both zones have the  
5 same development standards, the development standard shall be applied to the lot as a whole. If a  
6 lot or development site includes more than one zoning designation and a development standard is  
7 based on lot area, the lot area used in applying the development standard shall be the portion of  
8 the contiguous area with the corresponding zoning designation.

9                   Section 35. Section 23.45.510 of the Seattle Municipal Code, last amended by Ordinance  
10 127099, is amended as follows:

11 **23.45.510 Floor area**

12                   A. Gross floor area. In multifamily zones, gross floor area includes exterior corridors,  
13 breezeways, and stairways that provide building circulation and access to dwelling units or  
14 sleeping rooms. Balconies, patios, and decks that are associated with a single dwelling unit or  
15 sleeping room and that are not used for common circulation ~~((, and ground-level walking paths,))~~  
16 are not considered gross floor area.

17                   B. Floor area ratio (FAR) limits in LR and MR zones. FAR limits apply in LR and MR  
18 zones as shown in Table A for 23.45.510~~((-))~~, provided that if the LR zone designation includes  
19 an incentive zoning suffix, then gross floor area may exceed the base FAR as identified in the  
20 suffix designation, up to the limits shown in Table A for 23.45.510, if the applicant complies  
21 with Chapter 23.58A, Incentive Provisions. The applicable FAR limit applies to the total  
22 chargeable floor area of all structures on the lot.

**Table A for 23.45.510  
 FAR limits in LR and MR zones**

Zone	Zones with an MHA suffix	Zones without an MHA suffix
LR1	1.3, <u>except 1.5 for stacked dwelling units</u>	1.0
LR2	1.4, <u>except 1.6 for stacked dwelling units</u> <sup>1</sup>	1.1
LR3 outside urban centers and urban villages	1.8	1.2, except 1.3 for ((apartments)) <u>stacked dwelling units</u>
LR3 inside urban centers and urban villages	2.3	1.2, except 1.5 for ((apartments)) <u>stacked dwelling units</u>
MR	4.5	3.2

Footnote to Table A for 23.45.510

<sup>1</sup> Except that the FAR is ~~((1.6))~~ 1.8 for ((apartments)) stacked dwelling units that provide one or more outdoor amenity areas meeting the requirements of Section 23.45.522 and the following provisions are met:

1. The total amount of ~~((;))~~ outdoor amenity area is equal to at least 35 percent of the lot area;
2. No part of such amenity area has a width or depth of less than 20 feet; and
3. The outdoor amenity area is located at ground level or within 4 feet of finished grade.

\* \* \*

D. The following floor area is exempt from FAR limits:

1. All stories, or portions of stories, that are underground.
2. The floor area in a Landmark structure subject to controls and incentives imposed by a designating ordinance, if the owner of the Landmark has executed and recorded an agreement acceptable in form and content to the Landmarks Preservation Board, providing for the restoration and maintenance of the historically significant features of the structure, except

1 that this exemption does not apply to a lot from which a transfer of development potential (TDP)  
2 has been made under Chapter 23.58A, and does not apply for purposes of determining TDP  
3 available for transfer under Chapter 23.58A.

4 3. The floor area in structures built prior to January 1, 1982, as ~~((single family))~~  
5 detached dwelling units that will remain in residential use, regardless of the number of dwelling  
6 units within the existing structure, provided that:

7 a. ~~((All residential structures in LR zones, except as provided in~~  
8 ~~subsection 23.45.510.D.4.b;))~~ No other principal structure is located between the existing  
9 residential structure and the street lot line along at least one street frontage. If the existing  
10 residential structure is moved on the lot, the floor area of the existing residential structure  
11 remains exempt if it continues to meet this subsection 23.45.510.D.3.a; and

12 b. ~~((Single family, cottage housing, rowhouse, and townhouse~~  
13 ~~developments in LR zones, provided that all parking is located at the rear of the structure or is~~  
14 ~~enclosed in structures with garage entrances located on the rear facade; and))~~ The exemption is  
15 limited to the gross floor area that existed on January 1, 1982 and does not include any additions  
16 to floor area made to the residential structure after January 1, 1982.

17 4. Portions of a story that extend no more than 4 feet above existing or finished  
18 grade, whichever is lower, excluding access, (see Exhibit A for 23.45.510), in the following  
19 circumstances:

20 a. ~~((All residential structures))~~ Stacked dwelling units in LR zones  
21 ~~((except as provided in subsection 23.45.510.D.4.b));~~

22 b. ~~((Single family, cottage housing, rowhouse, and townhouse~~  
23 ~~developments))~~ Attached and detached dwelling units in LR zones, provided that all parking is

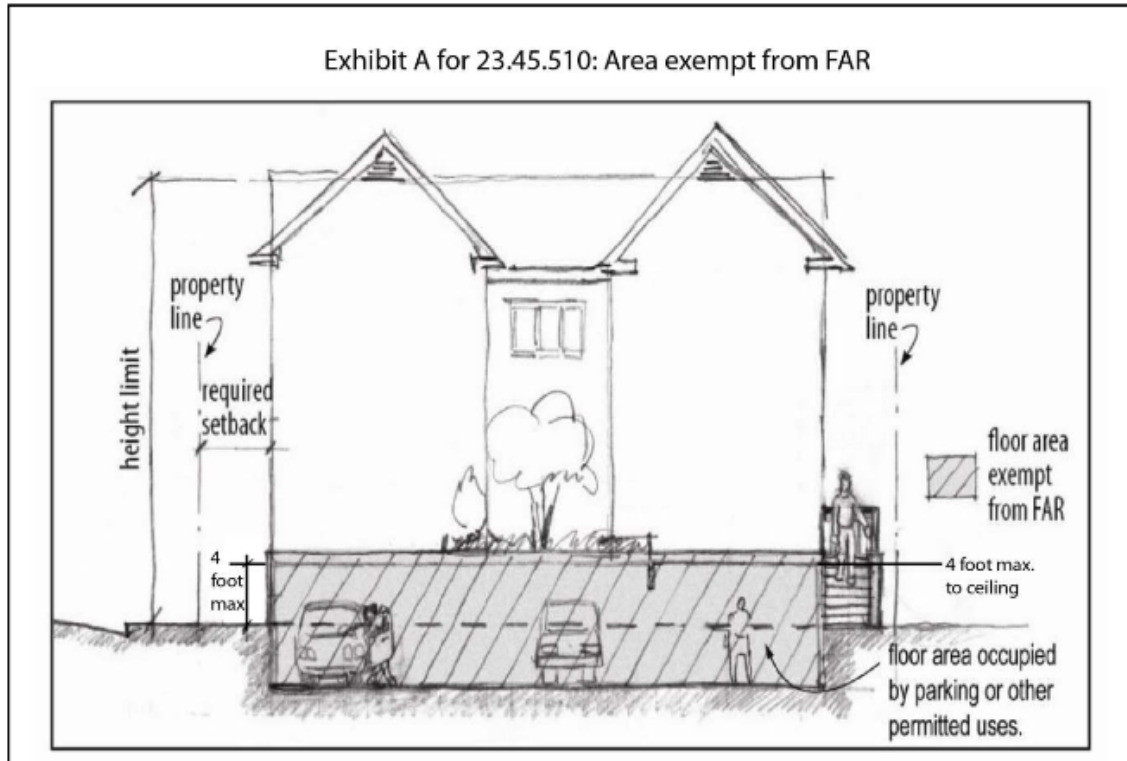


1 located at the rear of the structure or is enclosed in structures with garage entrances located on  
2 the rear facade; and

3 c. All ~~((multifamily structures))~~ dwelling units in MR and HR zones.

4 **Exhibit A for 23.45.510**

5 **Area exempt from FAR**



6  
7 5. For ~~((rowhouse and townhouse developments and apartments))~~ attached and  
8 stacked dwelling units, floor area within a story, or portion of a story, that is partially above  
9 grade if all of the following conditions are met:

10 a. The story, or portion of the story, that is partially above grade is used  
11 for parking or other accessory uses and has no additional stories above;

12 b. The average height of the exterior walls enclosing the floor area does  
13 not exceed one story, measured from existing or finished grade, whichever is lower;

1 c. The roof area above the exempt floor area is predominantly flat, is used  
2 as amenity area, and meets the standards for amenity area at ground level in Section 23.45.522;  
3 and

4 d. At least 25 percent of the perimeter of the amenity area on the roof  
5 above the floor area is not enclosed by the walls of the structure.

6 6. Enclosed common amenity area in HR zones.

7 7. As an allowance for mechanical equipment, in any structure more than 85 feet  
8 in height, 3.5 percent of the gross floor area that is not otherwise exempt under this subsection  
9 23.45.510.D.

10 8. In HR zones, ground floor commercial uses meeting the requirements of  
11 Section 23.45.532, if the street level of the structure containing the commercial uses has a  
12 minimum floor-to-floor height of 13 feet and a minimum depth of 15 feet.

13 9. The floor area of required bicycle parking for small efficiency dwelling units or  
14 congregate residence sleeping rooms, if the bicycle parking is located within the structure  
15 containing the small efficiency dwelling units or congregate residence sleeping rooms. Floor area  
16 of bicycle parking that is provided beyond the required bicycle parking is not exempt from FAR  
17 limits.

18 10. Common walls separating individual (~~rowhouse and townhouse~~) attached  
19 dwelling units.

20 11. In the Northgate Urban Center, up to 15,000 square feet of floor area in  
21 residential use in a structure built prior to 1990 that is located on a split-zoned lot of at least  
22 40,000 square feet in size.

23 12. In MR and HR zones, all gross floor area in child care centers.

1                   13. In low-income housing, all gross floor area for accessory human service uses.

2                   E. If TDP is transferred from a lot pursuant to Section 23.58A.042, the amount of non-  
3 exempt floor area that may be permitted is ~~((an))~~ a FAR of 7, plus any net amount of TDP  
4 previously transferred to the lot, minus the sum of the existing non-exempt floor area on the lot  
5 and the amount of TDP transferred.

6                   Section 36. Section 23.45.512 of the Seattle Municipal Code, last amended by Ordinance  
7 127211, is repealed:

8 ~~((23.45.512 Density limits and family size unit requirements—LR zones~~

9                   A. ~~Density limits~~

10                   1. ~~Except according to subsection 23.45.512.A.4, the following developments~~  
11 ~~must meet the density limits described in this subsection 23.45.512.A:~~

12                   a. ~~In LR1 zones, rowhouse development on interior lots and all townhouse~~  
13 ~~development; and~~

14                   b. ~~All development in Lowrise zones that do not have a mandatory~~  
15 ~~housing affordability suffix.~~

16                   2. ~~Development described in subsection 23.45.512.A.1 shall not exceed a density~~  
17 ~~of one dwelling unit per 1,150 square feet of lot area, except that apartments in LR3 zones that~~  
18 ~~do not have a mandatory housing affordability suffix shall not exceed a density limit of one~~  
19 ~~dwelling unit per 800 square feet.~~

20                   3. ~~When density calculations result in a fraction of a unit, any fraction up to and~~  
21 ~~including 0.85 constitutes zero additional units, and any fraction over 0.85 constitutes one~~  
22 ~~additional unit.~~

1                   ~~4. Low income housing shall have a maximum density of one dwelling unit per~~  
2 ~~400 square feet of lot area.~~

3                   ~~B. Family sized unit requirements in LR1 zones~~

4                   ~~1. Apartment developments in LR1 zones with four or more units shall provide at~~  
5 ~~least one unit with two or more bedrooms and a minimum net unit area of 850 square feet for~~  
6 ~~every four units in the structure.~~

7                   ~~2. One unit with three or more bedrooms and a minimum net unit area of 1,050~~  
8 ~~square feet may be provided in place of any two units required to include two bedrooms and a~~  
9 ~~minimum net unit area of 850 square feet.~~

10                  ~~C. Nursing homes, congregate housing, assisted living facilities, and accessory dwelling~~  
11 ~~units that meet the standards of Section 23.45.545 are exempt from the density limit set in~~  
12 ~~subsection 23.45.512.A and the requirements in subsection 23.45.512.B.~~

13                  ~~D. Dwelling unit(s) located in structures built prior to January 1, 1982, as single family~~  
14 ~~dwelling units that will remain in residential use are exempt from density limits.~~

15                  ~~E. If dedication of right of way is required, permitted density shall be calculated before~~  
16 ~~the dedication is made.~~

17                  ~~F. Adding units to existing structures~~

18                  ~~1. One additional dwelling unit may be added to an existing residential structure~~  
19 ~~regardless of the density restrictions in subsection 23.45.512.A and the requirements in~~  
20 ~~subsection 23.45.512.B. An additional unit is allowed only if the proposed additional unit is to be~~  
21 ~~located entirely within an existing structure, and no additional floor area to accommodate the~~  
22 ~~new unit is proposed to be added to the existing structure.~~

2. For the purposes of this subsection 23.45.512.F, "existing residential structures" are those that were established under permit as of October 31, 2001, or for which a permit has been granted and the permit has not expired as of October 31, 2001.)

Section 37. Section 23.45.514 of the Seattle Municipal Code, last amended by Ordinance 127211, is amended as follows:

**23.45.514 Structure height**

A. Subject to the additions and exceptions allowed as set forth in this Section 23.45.514, the height limits for structures in LR zones are as shown on Table A for 23.45.514.

<b>Table A for 23.45.514 Structure height for LR zones (in feet)</b>				
<b><del>((Housing)) Dwelling unit type</del></b>	<b>LR1</b>	<b>LR2</b>	<b>LR3 outside urban centers, urban villages, and Station Area Overlay Districts</b>	<b>LR3 in urban centers, urban villages, and Station Area Overlay Districts</b>
<del>((Cottage housing developments</del>	22	22	22	22
<del>Rowhouse and townhouse developments)) Attached and detached dwelling units</del>	<del>((30)) 32</del>	40 <sup>1</sup>	40 <sup>1</sup>	50 <sup>1</sup>
<del>((Apartments)) Stacked dwelling units</del>	<del>((30)) 32</del>	40 <sup>1</sup>	40 <sup>1</sup>	50 <sup>2</sup>

Footnotes for Table A for 23.45.514  
<sup>1</sup> Except that the height limit is ~~((30)) 32~~ feet in zones without a mandatory housing affordability suffix.  
<sup>2</sup> Except that the height limit is 40 feet in zones without a mandatory housing affordability suffix.

\* \* \*

1 C. The height limit for accessory structures other than accessory dwelling units that are  
2 located in required setbacks or separations is 12 feet, except as follows:

3 1. Garages and carports are limited to 12 feet in height as measured on the facade  
4 containing the vehicle entrance. Open rails may extend an additional 3 feet above the roof of the  
5 garage or carport if any portion of the roof is within 4 feet of existing grade. The ridge of a  
6 pitched roof on a garage located in a required setback may extend up to 3 feet above the 12-foot  
7 height limit. All parts of the roof above the height limit shall be pitched at a rate of not less than  
8 4:12. No portion of a shed roof is permitted to extend beyond the 12-foot height limit.

9 2. ~~((The height limit for an accessory dwelling unit is provided in subsection~~  
10 ~~23.42.022.D.~~

11 3.)) Freestanding flagpoles and religious symbols for religious institutions are  
12 exempt from height controls((;)) except as regulated in Chapter 23.64, provided they are no  
13 closer to any lot line than 50 percent of their height above existing grade.

14 \* \* \*

15 F. For ~~((apartments in LR2 zones, and for all residential uses in LR3))~~ stacked dwelling  
16 units in LR zones, the applicable height limit is increased 4 feet above the height shown on Table  
17 A for 23.45.514 for a structure that includes a story that is partially below-grade, provided that:

18 1. This height exception does not apply to portions of lots that are within 50 feet  
19 of a ~~((neighborhood residential))~~ Neighborhood Residential zone boundary line, unless the lot in  
20 the LR zone is separated from a ~~((neighborhood residential))~~ Neighborhood Residential zoned lot  
21 by a street;



**((Table A for 23.45.518  
 Required setbacks in LR zones measured in feet**

All LR zones	Category of residential use			
Front	7 average; 5 minimum	5 minimum	7 average; 5 minimum	5 minimum
Rear	0 with alley; 7 if no alley	0 with alley; With no alley: 7 average; 5 minimum	7 average; 5 minimum	10 minimum with alley; 15 minimum if no alley
Side setback for facades 40 feet or less in length <sup>1</sup>	5	0 where abutting another rowhouse development <sup>2</sup> ; otherwise 3.5; except that on side lot lines that abut a neighborhood residential zone, the setback is 5	5	5
Side setback for facades greater than 40 feet in length <sup>3</sup>	5 minimum	0 where abutting another rowhouse development <sup>2</sup> ; otherwise 3.5; except that on side lot lines that abut a neighborhood residential zone, the setback is 7 average; 5 minimum	7 average; 5 minimum	7 average; 5 minimum

Footnotes to Table A for 23.45.518

<sup>1</sup>—Additions to existing nonconforming structures built prior to April 11, 2011, shall be set back a sufficient distance so that the addition complies with setback standards. For any portion of a structure built before April 11, 2011, the average setback applies only to a new addition built after that date. If an addition is to a side wall extended vertically, the existing side wall line may be continued by the addition, provided that the average setback of 7 feet or the 5-foot minimum setback is met.

<sup>2</sup>—If the side facades of rowhouse developments on abutting lots are not joined, then a 3.5-foot setback is required, except the side setback may be reduced to zero if the abutting lot contains a rowhouse development and an easement is provided along the shared lot line of the abutting



**((Table A for 23.45.518  
 Required setbacks in LR zones measured in feet**

All LR zones	Category of residential use
--------------	-----------------------------

lot sufficient to leave a 3.5-foot separation between the principal structures of the abutting rowhouse developments.  
<sup>2</sup>—Portions of structures that qualify for the FAR exemption in subsection 23.45.510.D.5 are not considered part of the facade length for the purposes of determining the side setback requirement.))

1

**Table A for 23.45.518  
Required setbacks in LR zones**

<u>Front</u>	<u>7 feet average, 5 feet minimum</u> <sup>1</sup>
<u>Rear</u>	<u>If rear lot line abuts an alley, 0 feet</u> <u>Otherwise, 7 feet average, 5 feet minimum</u>
<u>Side</u>	<u>5 feet</u>

<sup>1</sup> For lots abutting landmark public right-of-way on Queen Anne Boulevard, front setbacks shall be 20 feet or the average of the front setbacks of the structures on abutting lots, whichever is less, except that if the natural gradient or slope (as measured from the front line of the lot for a distance of 60 feet or the full depth of the lot, whichever is less) is in excess of 35 percent, the required front setback depth shall be the lesser of: 20 feet less one foot for each one percent of gradient or slope in excess of 35 percent; or the average of the front setbacks on the abutting lots.

2

2. Upper-level setbacks in LR2 and LR3 zones

3

a. An upper-level setback of 12 feet from the front lot line is required for

4

all portions of a structure above the following height:

5

1) Forty-four feet for zones with a height limit of 40 feet; and

6

2) Fifty-four feet for zones with a height limit of 50 feet.



1                   1. ~~In LR and MR zones, the minimum required separation between principal~~  
2 ~~structures at any two points on different interior facades is 10 feet, except for cottage housing~~  
3 ~~developments, and principal structures separated by a driveway or parking aisle.~~

4                   2. ~~In LR and MR zones, if principal structures are separated by a driveway or~~  
5 ~~parking aisle, the minimum required separation between the principal structures is 2 feet greater~~  
6 ~~than the required width of the driveway or parking aisle, provided that the separation is not~~  
7 ~~required to be any greater than 24 feet. If principal structures are separated by a driveway or~~  
8 ~~parking aisle, projections that enclose floor area may extend a maximum of 3 feet into the~~  
9 ~~required separation if they are at least 8 feet above finished grade.~~

10                   3. ~~Cottage housing developments in LR and MR zones:~~

11                   a. ~~The minimum required separation between principal structures at any~~  
12 ~~two points on different interior facades is 6 feet, unless there is a principal entrance on an interior~~  
13 ~~facade, in which case the minimum separation required from that facade is 10 feet.~~

14                   b. ~~Facades of principal structures shall be separated from facades of~~  
15 ~~accessory structures by a minimum of 3 feet.~~

16                   G.) Front and rear setbacks ~~((and all separations))~~ on lots containing certain  
17 environmentally critical areas or buffers may be reduced pursuant to Sections 25.09.280 and  
18 25.09.300.

19                   ((H.)) G. Projections permitted in required setbacks ~~((and separations))~~

20                   1. ~~((Cornices))~~ Architectural features such as cornices, eaves, gutters, roofs,  
21 fireplaces, chimneys, and other ~~((forms of weather protection))~~ similar features may project into  
22 required setbacks ~~((and separations))~~ a maximum of 4 feet if they are no closer than 3 feet to any  
23 lot line.

1                   2. Garden windows and other similar features that do not provide floor area may  
2 project a maximum of 18 inches into required setbacks (~~(and separations)~~) if they:

- 3                   a. Are a minimum of 30 inches above the finished floor;  
4                   b. Are no more than 6 feet in height and 8 feet wide; and  
5                   c. Combined with bay windows and other similar features with floor area,  
6 make up no more than 30 percent of the area of the facade.

7                   3. Bay windows and other similar features that provide floor area may project a  
8 maximum of 2 feet into required setbacks (~~(and separations)~~) if they:

- 9                   a. Are no closer than 5 feet to any lot line;  
10                  b. Are no more than 10 feet in width; and  
11                  c. Combined with garden windows and other (~~(features)~~) projections  
12 included in subsection (~~(23.45.518.H.2)~~) 23.45.518.G.2, make up no more than 30 percent of the  
13 area of the facade.

14                  4. Unenclosed decks up to 18 inches above existing or finished grade, whichever  
15 is lower, may project into required setbacks (~~(or separations)~~).

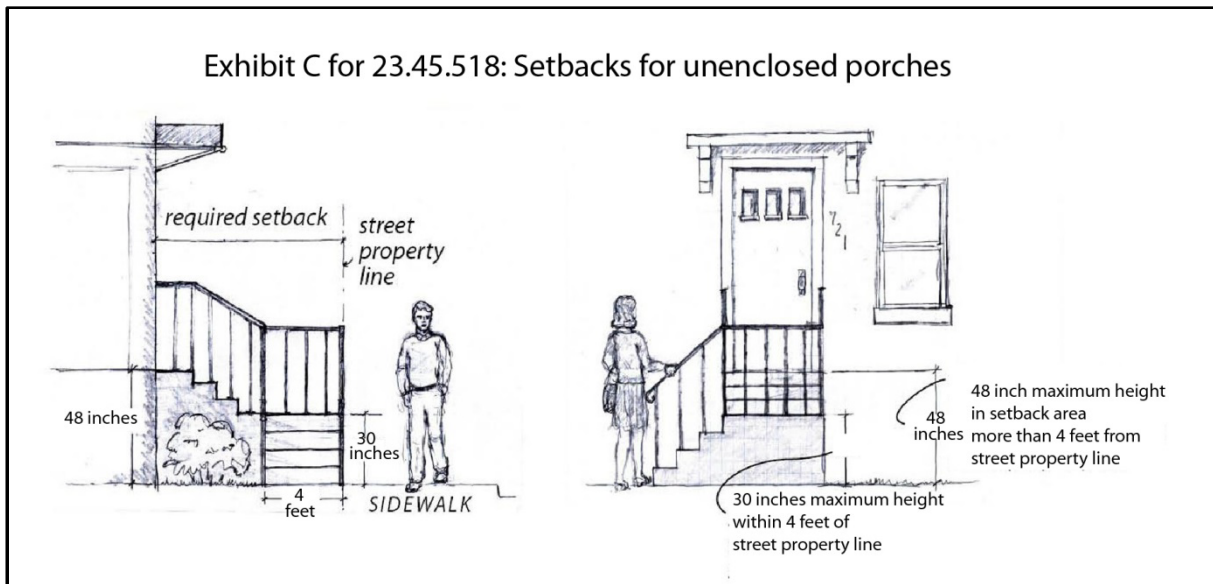
16                  5. Unenclosed porches or steps  
17                  a. Unenclosed porches or steps no higher than 4 feet above existing grade,  
18 or the grade at the street lot line closest to the porch, whichever is lower, may extend to within 4  
19 feet of a street lot line, except that portions of entry stairs or stoops not more than 2.5 feet in  
20 height from existing or finished grade, whichever is lower, (~~(excluding guard rails or hand~~  
21 ~~rails,)) may extend to a street lot line. See Exhibit C for 23.45.518.~~

1                                    b. Unenclosed porches or steps no higher than 4 feet above existing grade  
2 may project into the required rear setback (~~(or required separation)~~) between structures a  
3 maximum of 4 feet provided they are a minimum of 5 feet from a rear lot line.

4                                    c. Unenclosed porches or steps permitted in required setbacks (~~and~~  
5 ~~separations~~) shall be limited to a combined maximum width of 20 feet.

6 **Exhibit C for 23.45.518**

7 **Setbacks for unenclosed porches**



8  
9                                    d. Permitted porches or steps may be covered, provided that no portions of  
10 the cover-structure, including any supports, are closer than 3 feet to any lot line.

11                                    6. Fireplaces and chimneys may project up to 18 inches into required setbacks (~~(or~~  
12 ~~separations)~~).

13                                    7. Unenclosed decks and balconies may project a maximum of 4 feet into required  
14 setbacks if each one is:

15                                    a. No closer than 5 feet to any lot line;

16                                    b. No more than 20 feet wide; and

1 c. Separated from other decks and balconies on the same facade of the  
2 structure by a distance equal to at least 1/2 the width of the projection.

3 8. Mechanical equipment. Heat pumps and similar mechanical equipment, not  
4 including incinerators, are permitted in required setbacks if they comply with the requirements of  
5 Chapter 25.08. Any heat pump or similar equipment shall not be located within 3 feet of any lot  
6 line. Charging devices for electric cars are considered mechanical equipment and are permitted  
7 in required setbacks if not located within 3 feet of any lot line.

8 ~~((F.))~~ H. Structures in required setbacks (~~((or separations))~~), except upper-level setbacks

9 1. Detached garages, carports, or other accessory structures that are not accessory  
10 dwelling units are allowed in (~~((required separations and))~~) required rear or side setbacks, subject  
11 to the following requirements:

12 a. Any accessory structure located between a principal structure and a side  
13 lot line shall provide the setback required for the principal structure;

14 b. Any portion of an accessory structure located more than 25 feet from a  
15 rear lot line shall be set back at least 5 feet from the side lot line;

16 c. Accessory structures shall be set back at least 7 feet from any lot line  
17 that abuts a street; and

18 d. Accessory structures shall be separated by at least 3 feet from all  
19 principal structures, including the eaves, gutters, and other projecting features of the principal  
20 structure.

21 2. Ramps or other devices necessary for access for the disabled and elderly that  
22 meet the Seattle Residential Code(~~((, Chapter 3,))~~) or Seattle Building Code(~~((, Chapter 11,~~  
23 ~~Accessibility,))~~) are allowed in any required setback (~~((or separation))~~).

1                   3. Uncovered, unenclosed pedestrian bridges, necessary for access and 5 feet or  
2 less in width, are allowed in any required setback (~~(or separation)~~).

3                   4. Underground structures are allowed in any required setback (~~(or separation)~~).

4                   5. Solar collectors are allowed in any required setback (~~(or separation)~~), pursuant  
5 to the provisions of Section 23.45.545.

6                   6. Freestanding signs, bike racks, and similar unenclosed structures that are 6 feet  
7 or less in height above existing or finished grade, whichever is lower, are allowed in any required  
8 setback (~~(or separation)~~), provided that signs meet the provisions of Chapter 23.55(~~(Signs)~~).

9                   7. Fences

10                   a. Fences no greater than 6 feet in height are allowed in any required  
11 setback (~~(or separation)~~), except that fences in the required front setback extended to side lot  
12 lines or in street side setbacks extended to the front and rear lot lines may not exceed 4 feet in  
13 height. Fences located on top of a bulkhead or retaining wall are also limited to 4 feet. If a fence  
14 is placed on top of a new bulkhead or retaining wall used to raise grade, the maximum combined  
15 height is limited to 9.5 feet.

16                   b. Up to 2 feet of additional height for architectural features such as arbors  
17 or trellises on the top of a fence is allowed(~~(7)~~) if the architectural features are predominately  
18 open.

19                   c. Fence height may be averaged along sloping grades for each 6-foot-long  
20 segment of the fence, but in no case may any portion of the fence exceed 8 feet in height when  
21 the height allowed by subsection (~~(23.45.518.I.7.a)~~) 23.45.518.H.7.a is 6 feet, or 6 feet in height  
22 when the height allowed by subsection (~~(23.45.518.I.7.a)~~) 23.45.518.H.7.a is 4 feet.

23                   8. Bulkheads and retaining walls

1 a. Bulkheads and retaining walls used to raise grade are allowed in any  
2 required setback if they are limited to 6 feet in height, measured above existing grade. ((A  
3 guardrail no higher than 42 inches may be placed on top of a bulkhead or retaining wall existing  
4 as of January 3, 1997.))

5 b. Bulkheads and retaining walls used to protect a cut into existing grade  
6 may not exceed the minimum height necessary to support the cut or 6 feet measured from the  
7 finished grade on the low side, whichever is greater. ((If the bulkhead is measured from the low  
8 side and it exceeds 6 feet, an open guardrail of no more than 42 inches meeting Seattle  
9 Residential Code or Seattle Building Code requirements may be placed on top of the bulkhead or  
10 retaining wall.)) Any fence shall be set back a minimum of 3 feet from such a bulkhead or  
11 retaining wall.

12 ((9. Arbors are allowed in any required setback or separation under the following  
13 conditions:

14 a. In each required setback or separation, an arbor may be erected with no  
15 more than a 40 square foot footprint, measured on a horizontal roof plane inclusive of eaves, to a  
16 maximum height of 8 feet. At least 50 percent of both the sides and the roof of the arbor shall be  
17 open, or, if latticework is used, there shall be a minimum opening of 2 inches between  
18 crosspieces.

19 b. In each required setback abutting a street, an arbor over a private  
20 pedestrian walkway with no more than a 30 square foot footprint, measured on the horizontal  
21 roof plane and inclusive of eaves, may be erected to a maximum height of 8 feet. At least 50  
22 percent of the sides of the arbor shall be open, or, if latticework is used, there shall be a  
23 minimum opening of 2 inches between crosspieces.



1                   ~~10. Above-grade green stormwater infrastructure (GSI) features are allowed in~~  
2 ~~any required setback or separation if:~~

3                   ~~a. Each above-grade GSI feature is no more than 4.5 feet tall, excluding~~  
4 ~~pipng;~~

5                   ~~b. Each above-grade GSI feature is no more than 4 feet wide; and~~

6                   ~~c. The total storage capacity of all above-grade GSI features is no greater~~  
7 ~~than 600 gallons.~~

8                   ~~11. Above-grade GSI features larger than what is allowed in subsection~~  
9 ~~23.45.518.I.10 are allowed in any required setback or separation if:~~

10                   ~~a. Above-grade GSI features do not exceed ten percent coverage of any~~  
11 ~~one setback or separation area;~~

12                   ~~b. No portion of an above-grade GSI feature is located closer than 2.5 feet~~  
13 ~~from a side lot line; and~~

14                   ~~c. No portion of an above-grade GSI feature projects more than 5 feet into~~  
15 ~~a front or rear setback area.))~~

16                   9. Guardrails or handrails that are no more than 42 inches in height are allowed on  
17 unenclosed stairs, decks, access bridges, bulkheads, and retaining walls.

18                   10. Above-grade stormwater management features, such as bioretention planters  
19 and cisterns, are allowed in setbacks if:

20                   a. No feature, excluding piping, is more than:

21                                 1) Twelve feet tall if located in a portion of the rear setback that is  
22 not also a side setback; or

23                                 2) Six and one half feet tall, if located in other setbacks;

1                                    b. No feature greater than 4.5 feet tall is located within 10 feet of the front  
2 lot line, excluding piping, unless it is integrated into a bulkhead or retaining wall that is allowed  
3 in subsection 23.45.518.H.8;

4                                    c. No feature greater than 6 inches tall is located within 2.5 feet of the side  
5 lot line; and

6                                    d. The total storage capacity of all above-grade cisterns is no greater than  
7 1,250 gallons.

8                                    ~~((12.))~~ 11. Mechanical equipment. Heat pumps and similar mechanical  
9 equipment, not including incinerators, are allowed in any required setback if they comply with  
10 the requirements of Chapter 25.08. No heat pump or similar equipment shall be located within 3  
11 feet of any lot line. Charging devices for electric cars are considered mechanical equipment and  
12 are allowed in any required setbacks if not located within 3 feet of any lot line.

13                                    ~~((13.))~~ 12. Detached, unenclosed structures accessory to ((townhouses)) attached  
14 or detached dwelling units that are up to 8 feet in height and used exclusively for bike parking  
15 are allowed in any required setback ~~((or separation))~~.

16                                    ~~((14. Detached structures accessory to townhouses that are up to 10 feet in height~~  
17 ~~and used exclusively for bike parking are allowed in required separations.))~~

18                                    13. Private, permanent swimming pools, hot tubs, and other similar uses are  
19 permitted in any required setback, provided that:

20                                    a. No part of any swimming pools, hot tubs, and other similar uses projects  
21 more than 18 inches above existing grade in a required front setback; and

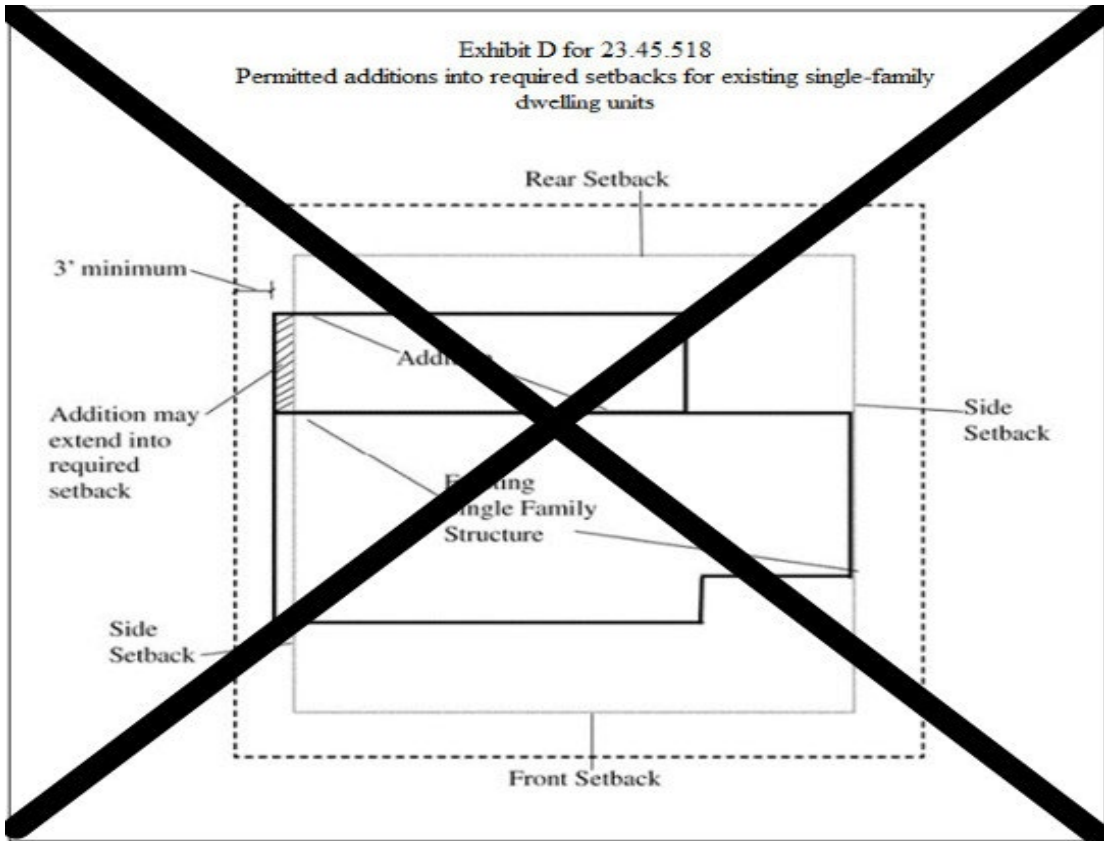
22                                    b. No swimming pool is placed closer than 5 feet to any front or side lot  
23 line.

1           ~~((1.))~~ I. Exceptions for existing ~~((single-family))~~ structures. ~~((1.))~~ In all multifamily  
2 zones, certain additions to a ~~((single-family dwelling unit))~~ residential structure may extend into  
3 a required side setback if the structure is already nonconforming with respect to that setback, and  
4 if the presently nonconforming section is at least 60 percent of the total width of the respective  
5 facade of the structure prior to the addition. The line formed by the nonconforming wall of the  
6 structure shall be the limit to which any additions may be built, which may extend up to the  
7 height limit and may include basement additions (Exhibit D for 23.45.518), provided that  
8 additions shall be at least 3 feet from the side lot line.

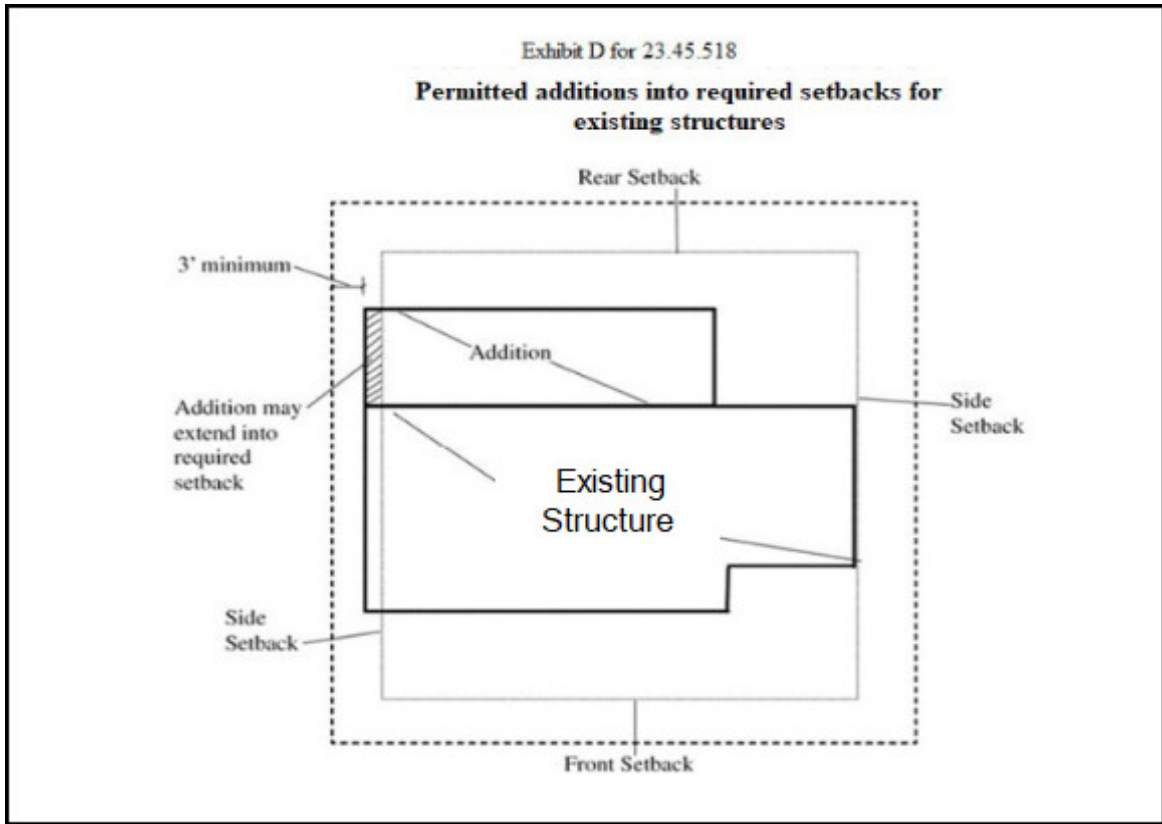
9           ~~((2. An existing single-family dwelling unit in a LR zone may be converted to a  
10 multifamily use without conforming to setback standards for apartments in subsection  
11 23.45.518.A, provided that the building envelope is not changed. For the purposes of this  
12 subsection 23.45.518.J.2, "existing single-family dwelling unit" is one that was established under  
13 permit as of October 31, 2001, or for which a permit has been granted and the permit has not  
14 expired on October 31, 2001.))~~

15 **Exhibit D for 23.45.518**

16 **Permitted additions into required setbacks for existing ~~((single-family dwelling units))~~**  
17 **structures**



1



2

1 Section 39. A new Section 23.45.519 is added to the Seattle Municipal Code as follows:

2 **23.45.519 Separations between structures**

3 A. In LR and MR zones, the minimum required separation between structures containing  
4 floor area is 5 feet except that, if the structures are separated by a driveway or parking aisle, the  
5 minimum required separation between structures containing floor area is 2 feet greater than the  
6 required width of the driveway or parking aisle or 24 feet, whichever is less. If the structures are  
7 separated by a driveway or parking aisle, projections that enclose floor area may extend a  
8 maximum of 3 feet into the required separation if they are at least 8 feet above finished grade.

9 B. Architectural features such as cornices, eaves, gutters, roofs, fireplaces, chimneys, and  
10 other forms of weather protection may project into required separations a maximum of 2 feet.  
11 Unenclosed structures allowed in side setbacks are allowed in the minimum separation. Garden  
12 windows, bay windows, covered porches and patios, balconies, and enclosed structures are not  
13 allowed in the required separation. Detached structures that are up to 10 feet in height and used  
14 exclusively for bike parking are allowed in required separations.

15 Section 40. Section 23.45.522 of the Seattle Municipal Code, last amended by Ordinance  
16 126157, is amended as follows:

17 **23.45.522 Amenity area**

18 A. Amount of amenity area (~~((required for rowhouse and townhouse developments and~~  
19 ~~apartments in LR zones))~~)

20 1. The ~~((required))~~ amount of required amenity area (~~((for rowhouse and~~  
21 ~~townhouse developments and apartments))~~) in LR zones is equal to ~~((25))~~ 20 percent of the lot  
22 area.

1                   ~~((2. A minimum of 50 percent of the required amenity area shall be provided at~~  
2 ~~ground level, except that amenity area provided on the roof of a structure that meets the~~  
3 ~~provisions of subsection 23.45.510.D.5 may be counted as amenity area provided at ground~~  
4 ~~level.~~

5                   ~~3. For rowhouse and townhouse developments, amenity area required at ground~~  
6 ~~level may be provided as either private or common space.~~

7                   ~~4. For apartments, amenity area required at ground level shall be provided as~~  
8 ~~common space.~~

9                   ~~B. Amenity area requirements for cottage housing developments in all multi-family zones~~

10                   ~~1. A minimum of 300 square feet of amenity area is required for each cottage.~~

11                   ~~2. A minimum of 150 square feet of amenity area is required for each carriage~~  
12 ~~house.~~

13                   ~~3. The required quantity shall be allocated as follows:~~

14                   ~~a. Half of the amenity area required for each cottage, and all of the~~  
15 ~~amenity area required for each carriage house, shall be provided as common amenity area; and~~

16                   ~~b. Half of the amenity area required for each cottage shall be provided as~~  
17 ~~private amenity area for that unit.~~

18                   ~~4. The required common amenity area may be divided into no more than two~~  
19 ~~separate areas and shall:~~

20                   ~~a. have cottages or carriage houses abutting on at least two sides;~~

21                   ~~b. be in a location central to the cottage housing development; and~~

22                   ~~c. have no horizontal dimension of less than 10 feet.~~

1                   ~~5. Carriage houses shall have stairs that provide access to the common amenity~~  
2 ~~area.~~

3                   ~~C. Amount of amenity area required in MR and HR zones.)~~ 2. The ~~((required))~~ amount  
4 of required amenity area in MR and HR zones is equal to ~~((5))~~ five percent of the total gross  
5 floor area of a residential structure. ~~((in residential use, except that cottage housing developments~~  
6 ~~shall meet the standards in subsection 23.45.522.B.~~

7                   ~~D. General requirements. Required amenity areas shall meet the following conditions:~~

8                   ~~1. All units))~~ B. Attached and detached dwelling units shall have access to either a  
9 common or private amenity area. Stacked dwelling units shall have access to a common amenity  
10 area.

11                   ~~((2.))~~ C. Enclosed amenity areas

12                   ~~((a. In LR zones, an amenity area shall not be enclosed within a structure.~~

13                   ~~b. In MR and HR zones, except for cottage housing, no))~~ 1. No more than 50  
14 percent of the amenity area may be enclosed, and this enclosed area shall be provided as  
15 common amenity area.

16                   2. Enclosed amenity areas must be provided in a room used exclusively for this  
17 purpose or in an area on the ground floor that can be accessed directly from the building lobby or  
18 an outdoor amenity space and does not include any of the following:

19                   a. Internal circulation hallways between outside doors and elevators or  
20 stairs;

21                   b. Mailrooms;

22                   c. Bike parking;

23                   d. Solid waste and recyclable materials storage; and

1                                    e. Laundry facilities.

2                                    ~~((3. Projections into amenity areas. Structural projections that do not provide floor~~  
3 ~~area, such as garden windows, may extend up to 2 feet into an amenity area if they are at least 8~~  
4 ~~feet above finished grade.))~~

5                                    D. Amenity area size

6                                    ~~((4.)) 1. Private amenity areas. ((a. There is no minimum dimension for private~~  
7 ~~amenity areas, except that if a private amenity area is located between the structure and a side lot~~  
8 ~~line that is not a side street lot line, the minimum horizontal dimension shall be measured from~~  
9 ~~the side lot line and is required to be a minimum of 10 feet.~~

10                                    ~~b. An unenclosed porch that is a minimum of 60 square feet in size and that faces~~  
11 ~~a street or a common amenity area may be counted as part of the private amenity area for the~~  
12 ~~rowhouse, townhouse, or cottage to which it is attached.)) Each private amenity area shall be at~~  
13 ~~least 60 square feet in area and have a minimum width and depth of 6 feet, except for balconies,~~  
14 ~~which shall be at least 30 square feet in area and have a minimum width and depth of 4 feet.~~

15                                    ~~((5.)) 2. Common amenity areas. ((for rowhouse and townhouse developments~~  
16 ~~and apartments shall meet the following conditions: a. No)) Each common amenity area shall be~~  
17 ~~((less than)) at least 250 square feet ((in area, and common amenity areas shall)) and have a~~  
18 ~~minimum ((horizontal dimension)) width and depth of 10 feet.~~

19                                    ~~((b. Common amenity areas shall be improved as follows:~~

20                                    1) ~~At least 50 percent of a common amenity area provided at~~  
21 ~~ground level shall be landscaped with grass, ground cover, bushes, bioretention facilities, and/or~~  
22 ~~trees.~~





1                   3. Decks, porches, and steps; swimming pools, spas, and hot tubs; stormwater  
2 management features, including but not limited to bioretention planters and cisterns; play  
3 equipment; and similar features are allowed in amenity areas.

4                   4. Amenity areas may be covered by weather protection.

5                   5. Projections that do not provide floor area may extend into an amenity area if  
6 they meet the standards for projections into setbacks in subsection 23.45.518.G and if garden  
7 windows and other similar features are at least 8 feet above finished grade.

8                   6. Rooftop areas located within 8 feet of minor communication utilities and  
9 accessory communication devices do not qualify as amenity areas.

10                  F. Common amenity areas shall be improved as follows:

11                   1. At least 35 percent of a common amenity area provided at ground level shall be  
12 landscaped with grass, ground cover, bushes, bioretention facilities, and/or trees.

13                   2. Elements that enhance the usability and livability of the space for residents,  
14 such as seating, outdoor lighting, weather protection, art, or other similar features, shall be  
15 provided.

16                  G. Areas in environmentally critical areas and their buffers, including but not limited to  
17 steep slopes, may count toward amenity areas. No amenity area enhancement elements shall be  
18 placed in the environmentally critical areas and their buffers non disturbance area.

19                  ~~((E.))~~ H. No amenity area is required for ~~((a))~~ one dwelling unit added to a ~~((single-~~  
20 ~~family dwelling unit))~~ residential structure existing as of January 1, 1982, ~~((or for one new~~  
21 ~~dwelling unit added to a multifamily residential use existing as of October 10, 2001))~~ provided  
22 that no dwelling units have been added since that date.

1 Section 41. Section 23.45.527 of the Seattle Municipal Code, last amended by Ordinance  
 2 126509, is amended as follows:

3 **23.45.527 Structure width ((and façade length) limits in LR zones**

4 ((A.)) Structure width ((in LR zones)) for buildings containing residential uses may not exceed  
 5 ((the width indicated on Table A for 23.45.527)) 90 feet in LR1 and LR2 zones and 150 feet in  
 6 LR3 zones.

7 **((Table A for 23.45.527: Maximum Structure Width in LR zones in feet**

Zone	Width in feet by Category of Residential Use		
	Cottage Housing and Rowhouse Developments	Townhouse Developments	Apartments
LR1	No limit	60	45
LR2	No limit	90	90
LR3 outside Urban Villages, Urban Centers or Station Area Overlay Districts	No limit	120	120
LR3 inside Urban Villages, Urban Centers or Station Area Overlay Districts	No limit	150	150

8 **B. Maximum façade length in Lowrise zones.**

9 1. The maximum combined length of all portions of façades within 15 feet of a lot  
 10 line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the  
 11 length of that lot line, except as specified in subsection 23.45.527.B.2.

12 2. For a rowhouse development on a lot that abuts the side lot line of a lot in a  
 13 neighborhood residential zone, the maximum combined length of all portions of façades within  
 14 15 feet of the abutting side lot line is 40 feet.))

1 Section 42. Section 23.45.529 of the Seattle Municipal Code, last amended by Ordinance  
2 127099, is amended as follows:

3 **23.45.529 Design standards**

4 ~~((A. Intent. The intent of the design standards in this Section 23.45.529 is to:~~

5 ~~1. Enhance street facing and side facades to provide visual interest, promote new~~  
6 ~~development that contributes to an attractive streetscape, and avoid the appearance of blank walls~~  
7 ~~along a street or adjacent residential property;~~

8 ~~2. Foster a sense of community by integrating new pedestrian-oriented~~  
9 ~~multifamily development with the neighborhood street environment and promoting designs that~~  
10 ~~allow easy surveillance of the street by area residents;~~

11 ~~3. Promote livability in multifamily areas by providing a sense of openness and~~  
12 ~~access to light and air; and~~

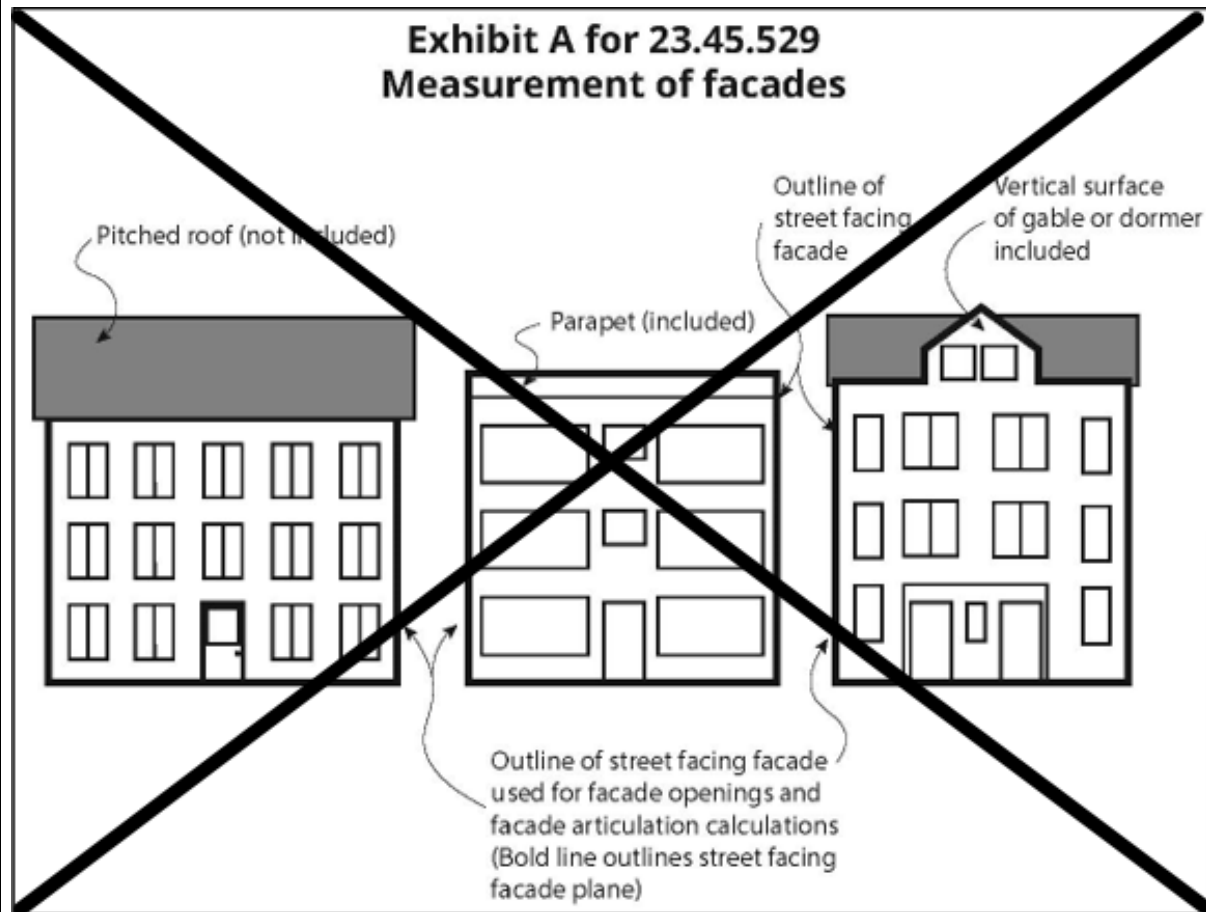
13 ~~4. Encourage the compatibility of a variety of housing types with the scale and~~  
14 ~~character of neighborhoods where new multifamily development occurs.~~

15 ~~B. Application of provisions. The provisions of this Section 23.45.529 apply to all~~  
16 ~~residential uses that do not undergo any type of design review pursuant to Chapter 23.41, except~~  
17 ~~single family dwelling units.~~

18 ~~C. Treatment of street facing facades. For the purposes of this subsection 23.45.529.C, a~~  
19 ~~street facing facade includes all vertical surfaces enclosing interior space, including gables and~~  
20 ~~dormers, as shown in Exhibit A for 23.45.529.~~

1 ~~Exhibit A for 23.45.529~~

2 ~~Measurement of facades~~



3 ~~1. Facade openings~~

4 ~~a. At least 20 percent of the area of each street-facing facade shall consist~~  
5 ~~of windows and/or doors, except as provided in subsection 23.45.529.C.1.b. If a front and side~~  
6 ~~facade are street-facing, the two facades may be combined for the purpose of this calculation.~~

7 ~~b. For any rowhouse or townhouse dwelling unit that has both a front and~~  
8 ~~a side facade that are street-facing, the percentage of the side street-facing facade required to~~  
9 ~~consist of windows and/or doors is reduced to ten percent for the portion of the facade associated~~  
10 ~~with that dwelling unit. This reduction to ten percent is not allowed if the facades are combined~~

1 ~~for the purpose of this standard pursuant to subsection 23.45.529.C.1.a or if any of the exceptions~~  
2 ~~in subsection 23.45.529.C.3 are applied.~~

3 ~~c. Windows count toward the requirement for facade openings in this~~  
4 ~~subsection 23.45.529.C.1 only if they are transparent. Windows composed of glass blocks or~~  
5 ~~opaque glass, garage doors, and doors to utility and service areas do not count.~~

6 ~~2. Facade articulation~~

7 ~~a. If a street-facing facade or portion of a street-facing facade is not~~  
8 ~~vertical, the Director shall determine whether the facade is substantially vertical and required to~~  
9 ~~comply with this subsection 23.45.529.C.~~

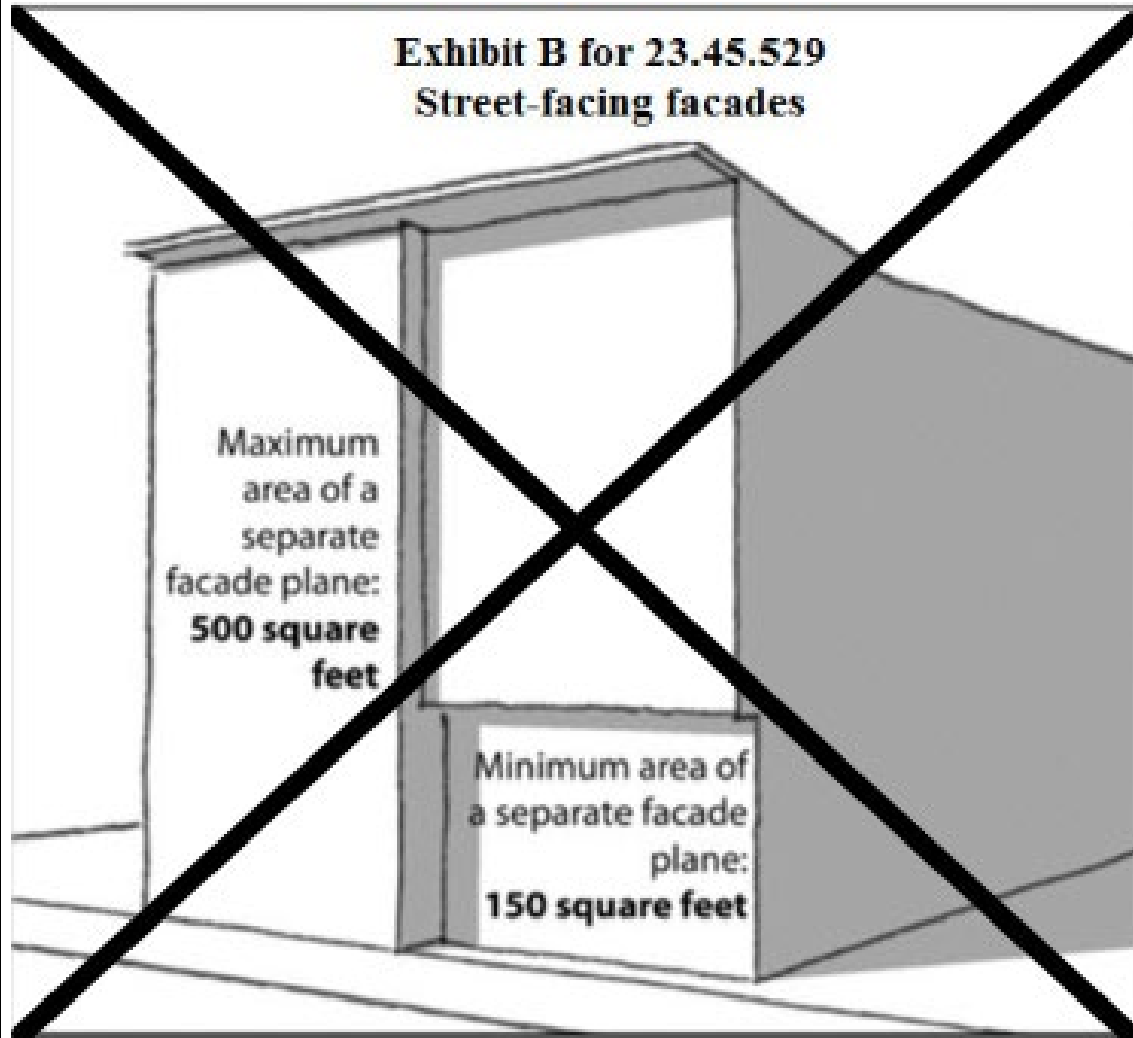
10 ~~b. If the street-facing facade of a structure exceeds 750 square feet in area,~~  
11 ~~division of the facade into separate facade planes is required (see Exhibit B for 23.45.529).~~

12 ~~c. In order to be considered a separate facade plane for the purposes of this~~  
13 ~~subsection 23.45.529.C.2, a portion of the street-facing facade shall have a minimum area of 150~~  
14 ~~square feet and a maximum area of 500 square feet, and shall project or be recessed from~~  
15 ~~abutting facade planes by a minimum depth of 18 inches.~~

16 ~~d. Trim that is a minimum of 0.75 inches deep and 3.5 inches wide is~~  
17 ~~required to mark roof lines, porches, windows, and doors on all street-facing facades.~~

1 **Exhibit B for 23.45.529**

2 **Street-facing facades**



4 3. The Director may allow exceptions to the facade opening requirements in  
5 subsection 23.45.529.C.1 and the facade articulation requirements in subsection 23.45.529.C.2, if  
6 the Director determines that the street-facing facade will meet the intent of subsection  
7 23.45.529.A.1 for all housing types, and, as applicable, the intent of subsections 23.45.529.E.2,  
8 23.45.529.F.3, and 23.45.529.G.4 for cottage housing developments, rowhouse developments,  
9 and townhouse developments, respectively, through one or more of the following street-facing  
10 facade treatments:

1                           a. ~~Variations in building materials and/or color, or both, that reflect the~~  
2 ~~stacking of stories or reinforce the articulation of the facade;~~

3                           b. ~~Incorporation of architectural features that add interest and dimension to~~  
4 ~~the facade, such as porches, bay windows, chimneys, pilasters, columns, cornices, and/or~~  
5 ~~balconies;~~

6                           e. ~~Special landscaping elements provided to meet Green Factor~~  
7 ~~requirements pursuant to Section 23.45.524, such as trellises, that accommodate vegetated walls~~  
8 ~~covering a minimum of 25 percent of the facade surface;~~

9                           d. ~~Special fenestration treatment, including an increase in the percentage~~  
10 ~~of windows and doors to at least 25 percent of the street-facing facade(s).~~

11                   D. ~~Treatment of side facades that are not street-facing. For the purposes of this subsection~~  
12 ~~23.45.529.D, a side facade that is not street-facing includes all vertical surfaces enclosing interior~~  
13 ~~space, including gables and dormers, as shown in Exhibit A for 23.45.529, if located within 10~~  
14 ~~feet of a side lot line.~~

15                           1. ~~If the side facade of a structure that is not street-facing exceeds 1,000 square~~  
16 ~~feet in area, one of the following must be met:~~

17                           a. ~~A portion of the side facade with a minimum area of 250 square feet and~~  
18 ~~a maximum area of 750 square feet shall project or be recessed from abutting facade planes by a~~  
19 ~~minimum depth of 18 inches; or~~

20                           b. ~~The side facade shall include vertical or horizontal variations in~~  
21 ~~building materials or color, covering a minimum of 25 percent of the facade surface.~~

22                           2. ~~Structures shall be designed to maintain the privacy of dwelling units by~~  
23 ~~minimizing placement of proposed windows where they would directly align with windows on~~



1 ~~the side facade of a structure on an abutting lot located within 20 feet of the side property line or~~  
2 ~~by use of fencing, screening, landscaping, or translucent windows to create privacy between~~  
3 ~~buildings.~~

4 ~~E. Design standards for cottage housing developments~~

5 ~~1. Pedestrian entry. Each cottage with a street facing facade that is located within~~  
6 ~~10 feet of the street lot line shall have a visually prominent pedestrian entry through the use of~~  
7 ~~covered stoops, porches, or other architectural entry features. For cottages on corner lots that~~  
8 ~~have more than one street facing facade within 10 feet of the street lot line, a visually prominent~~  
9 ~~pedestrian entry is required on only one of the street facing facades. Access to these entrances~~  
10 ~~may be through a required private amenity area that abuts the street.~~

11 ~~2. Architectural expression. Cottage housing developments shall include~~  
12 ~~architectural details that reduce the visual scale of the units. Each cottage shall employ one or~~  
13 ~~more of the following design techniques to reduce visual scale of the units:~~

14 ~~a. Attached covered porch;~~

15 ~~b. Roofline features such as dormers or clerestories;~~

16 ~~c. Bay windows;~~

17 ~~d. Variation in siding texture and materials; and~~

18 ~~e. Other appropriate architectural techniques demonstrated by the~~  
19 ~~applicant to reduce the visual scale of cottages.~~

20 ~~F. Design standards for rowhouse developments~~

21 ~~1. Pedestrian entry. Each rowhouse unit shall have a pedestrian entry on the~~  
22 ~~street facing facade that is designed to be visually prominent through the use of covered stoops,~~

1 ~~porches, or other architectural entry features. For rowhouse units on corner lots, a visually~~  
2 ~~prominent pedestrian entry is required on only one of the street-facing facades.~~

3 ~~2. Front setback. Design elements to provide a transition between the street and~~  
4 ~~the rowhouse units, such as landscaping, trees, fences, or other similar features, are required in~~  
5 ~~the front setback.~~

6 ~~3. Architectural expression. The street-facing facade of a rowhouse unit shall~~  
7 ~~provide architectural detail or composition to visually identify each individual rowhouse unit as~~  
8 ~~seen from the street. Design elements such as trim or molding, modulation, massing, color and~~  
9 ~~material variation, or other similar features may be used to achieve visual identification of~~  
10 ~~individual units. Rooftop features, such as dormers or clerestories, or roofline variation may be~~  
11 ~~used to visually identify individual rowhouse units.~~

12 ~~G. Design standards for townhouse developments~~

13 ~~1. Building orientation. Townhouse developments shall maximize the orientation~~  
14 ~~of individual units to the street by complying with one of the following conditions:~~

15 ~~a. When multiple buildings are located on a lot, at least 50 percent of the~~  
16 ~~townhouse units shall be located so that there is no intervening principal structure between the~~  
17 ~~unit and the street, unless the intervening principal structure was established under permit as of~~  
18 ~~October 31, 2001, or was granted a permit on October 31, 2001, and the permit has not expired;~~  
19 ~~or~~

20 ~~b. All townhouse units without a street-facing facade shall have direct~~  
21 ~~access to a common amenity area meeting the requirements of Section 23.45.522 that either~~  
22 ~~abuts the street or is visible and accessible from the street by a clear pedestrian pathway.~~

1                   2. ~~Pedestrian pathway. A clear pedestrian pathway from the street to the entrance~~  
2 ~~of each townhouse unit shall be provided. The pedestrian pathway may be part of a driveway,~~  
3 ~~provided that the pathway is differentiated from the driveway by pavement color, texture, or~~  
4 ~~similar technique. Signage identifying townhouse unit addresses and the directions to the unit~~  
5 ~~entrance(s) from the street shall be provided.~~

6                   3. ~~Pedestrian entry. Each townhouse unit with a street-facing facade shall have a~~  
7 ~~pedestrian entry on the street-facing facade that is designed to be a visually prominent feature~~  
8 ~~through the use of covered stoops, porches, or other architectural entry features. For townhouse~~  
9 ~~units on corner lots, a visually prominent pedestrian entry is required on only one of the street-~~  
10 ~~facing facades.~~

11                   4. ~~Architectural expression. Architectural detail or composition shall be provided~~  
12 ~~to visually identify each individual townhouse unit, as seen from the public street. Design~~  
13 ~~elements such as trim or molding, modulation, massing, color and material variation, or other~~  
14 ~~similar features may be used to achieve visual identification of individual units. Rooftop~~  
15 ~~features, such as dormers or clerestories, or roofline variation may be used to visually identify~~  
16 ~~individual townhouse units.~~

17                   H. ~~Building entry orientation standards for apartments~~

18                   1. ~~For each apartment structure, a principal shared pedestrian entrance is required~~  
19 ~~that faces either a street or a common amenity area, such as a landscaped courtyard, that abuts~~  
20 ~~and has direct access to the street. Additional pedestrian entrances to individual units are~~  
21 ~~permitted.~~

1                   ~~2. If more than one apartment structure is located on a lot, each apartment~~  
2 ~~structure separated from the street by another principal structure shall have a principal entrance~~  
3 ~~that is accessible from a common amenity area with access to the street.~~

4                   ~~3. The shared entrance of each apartment structure shall have a pedestrian entry~~  
5 ~~that is designed to be visually prominent, through the use of covered stoops, overhead weather~~  
6 ~~protection, a recessed entry, or other architectural entry features.))~~

7                   A. Application of provisions

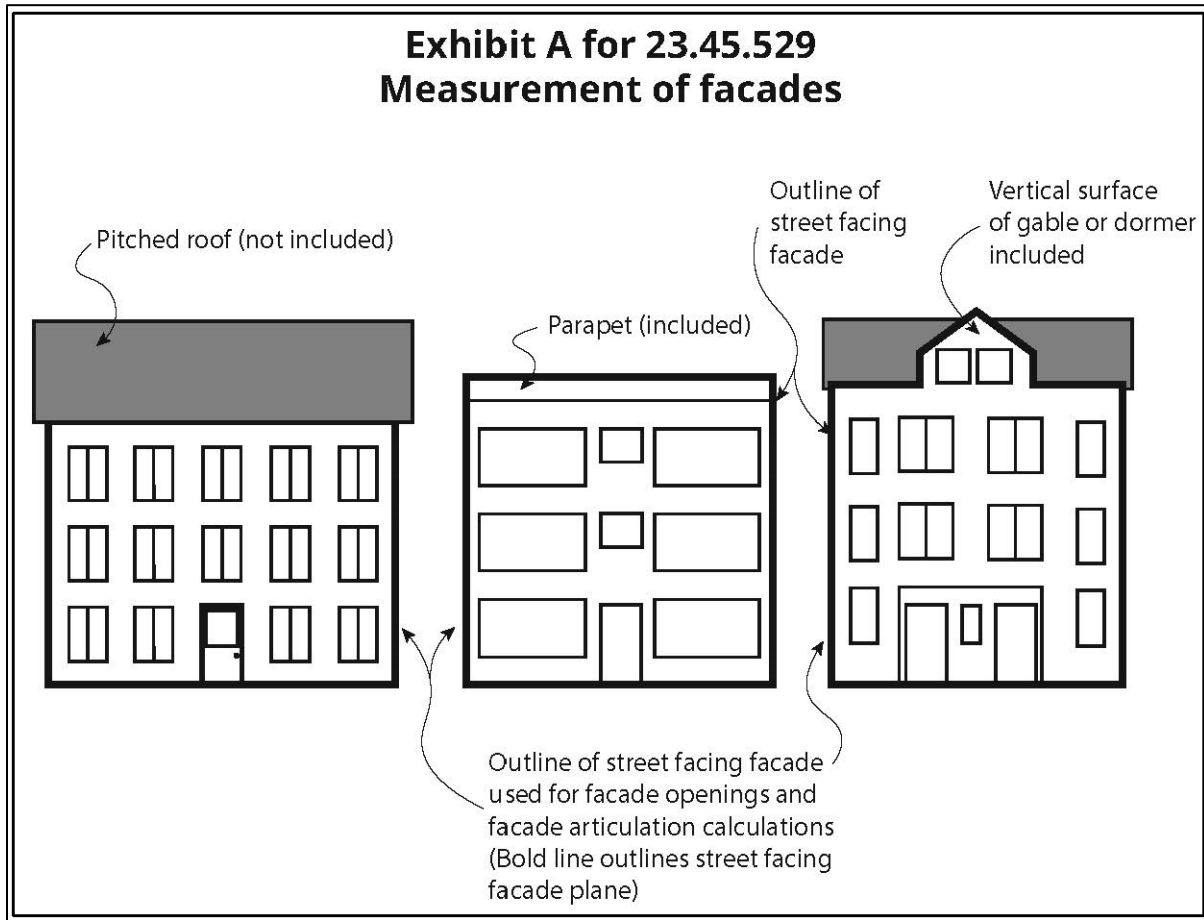
8                   1. The provisions of this Section 23.45.529 apply to development that includes the  
9 construction of new dwelling units, except for new dwelling units added within existing  
10 structures.

11                   2. For the purposes of this Section 23.45.529, requirements for street-facing  
12 facades shall only apply to structures located within 40 feet of a street lot line or a vehicle access  
13 easement serving ten or more residential units. For structures located within 40 feet of a vehicle  
14 access easement serving ten or more residential units but not within 40 feet of street lot line, the  
15 facade that faces the vehicle access easement shall be considered a street-facing facade for the  
16 purpose of this Section 23.45.529. If multiple facades face vehicle access easements, the  
17 applicant may decide which facade facing a vehicle access easement is considered the street-  
18 facing facade.

19                   B. Measurement of street-facing facades. For the purposes of this Section 23.45.529, a  
20 street-facing facade includes all vertical surfaces enclosing interior space, including gables and  
21 dormers, as shown in Exhibit A for 23.45.529.

1 **Exhibit A for 23.45.529**

2 **Measurement of facades**



3

4 C. Pedestrian access. Each dwelling unit shall have pedestrian access at least 3 feet in

5 width to the sidewalk or, if no sidewalk exists, the front lot line. This pedestrian access may be

6 shared or private. This pedestrian access may cross any required setbacks or interior separation.

7 This pedestrian access may be part of a driveway.

8 D. Entrances. Each structure with a street-facing facade shall have a pedestrian entry on

9 that street-facing facade meeting the requirements of subsections 23.44.140.D.1 through

10 23.44.140.D.4. For attached and detached dwelling units, the pedestrian entry may be located on

11 a wall perpendicular to the street-facing facade provided that the pedestrian entry abuts a covered

12 porch or recessed entry that also abuts the street-facing facade.

1                   1. For stacked dwelling units, at least one pedestrian entry shall be required for  
2 the structure as a whole.

3                   2. For attached and detached dwelling units, each individual dwelling unit with a  
4 street-facing facade within 40 feet of the street lot line shall have at least one pedestrian entry on  
5 the street-facing facade.

6                   3. For structures or dwelling units with multiple street-facing facades, a pedestrian  
7 entry is required on only one of the street-facing facades.

8                   4. Required pedestrian entry on street-facing facades shall have weather  
9 protection, such as a covered porch, canopy, recessed entry, or similar feature, measuring at least  
10 3 feet by 3 feet in width and depth for attached and detached dwelling units and at least 6 feet in  
11 width and 4 feet in depth for stacked dwelling units.

12                   E. Windows and doors. At least 20 percent of the area of each street-facing facade shall  
13 consist of windows and/or doors. If front and side facades are street-facing, the two facades shall  
14 be combined for the purpose of this calculation. Windows count toward the requirement for  
15 facade openings in this subsection 23.45.529.E only if they are transparent. Windows composed  
16 of garage doors and doors to utility and service areas do not count. For the purpose of this  
17 Section 23.45.529, a window shall include the glass pane, window frame, and internal  
18 components such as sashes, mullions, grilles, muntins, and stiles.

19                   Section 43. Section 23.45.531 of the Seattle Municipal Code, enacted by Ordinance  
20 123495, is repealed:

21 ~~((23.45.531 Development standards for cottage housing developments and carriage house~~  
22 ~~structures~~

23                   A. Size limit for dwelling units.







1                   1. ~~No part of any swimming pools, hot tubs and other similar uses shall project~~  
2 ~~more than 18 inches above existing grade in a required front setback; and~~

3                   2. ~~No swimming pool shall be placed closer than 5 feet to any front or side lot~~  
4 ~~line.~~

5                   ~~B. Solar greenhouses, greenhouses and solariums~~

6                   1. ~~Solar greenhouses, greenhouses and solariums, in each case that are attached to~~  
7 ~~and integrated with the principal structure and no more than 12 feet in height are permitted in a~~  
8 ~~required rear setback, subject to subsection 23.45.545.B.3, and may extend a maximum of 6 feet~~  
9 ~~into required front and side setbacks, subject to subsection 23.45.545.B.2.~~

10                  2. ~~An attached solar greenhouse, greenhouse or solarium, in a required setback,~~  
11 ~~shall be no closer than 3 feet from side lot lines and 8 feet from front lot lines.~~

12                  3. ~~A solar greenhouse, greenhouse or solarium allowed pursuant to subsection~~  
13 ~~23.45.545.B.1 shall not be closer than 5 feet to the rear lot line, except that it may abut an alley if~~  
14 ~~it is no taller than 10 feet along the rear lot line, is of no greater average height than 12 feet for a~~  
15 ~~depth of 15 feet from the rear lot line, and is no wider than 50 percent of lot width for a depth of~~  
16 ~~15 feet from the rear lot line.~~

17                  ~~C. Solar~~) General standards for solar collectors

18                  1. Solar collectors are permitted in required setbacks, subject to the following:

19                   a. Detached solar collectors are permitted in required rear setbacks, no  
20 closer than 5 feet to any other principal or accessory structure.

21                   b. Detached solar collectors are permitted in required side setbacks, no  
22 closer than 5 feet to any other principal or accessory structure, and no closer than 3 feet to the  
23 side lot line.

1                   2. Sunshades that provide shade for solar collectors that meet minimum written  
2 energy conservation standards administered by the Director may project into southern front or  
3 rear setbacks. Those that begin at 8 feet or more above finished grade may be no closer than 3  
4 feet from the lot line. Sunshades that are between finished grade and 8 feet above finished grade  
5 may be no closer than 5 feet to the lot line.

6                   3. Solar collectors on roofs. Solar collectors that are located on a roof are  
7 permitted as follows:

8                                 a. In LR zones up to 4 feet above the maximum height limit or 4 feet  
9 above the height of stair or elevator penthouse(s), whichever is higher; and

10                                b. In MR and HR zones up to 10 feet above the maximum height limit or  
11 10 feet above the height of stair or elevator penthouse(s), whichever is higher.

12                                c. If the solar collectors would cause an existing structure to become  
13 nonconforming, or increase an existing nonconformity, the Director may permit the solar  
14 collectors as a special exception pursuant to Chapter 23.76. Solar collectors may be permitted  
15 under this subsection (~~((23.45.545.C.3.e))~~ 23.45.545.A.3.c even if the structure exceeds the height  
16 limits established in this subsection (~~((23.45.545.C.3))~~ 23.45.545.A.3, if the following conditions  
17 are met:

18   1) There is no feasible alternative solution to placing the  
19 collector(s) on the roof; and

20   2) The collector(s) are located so as to minimize view blockage  
21 from surrounding properties and the shading of property to the north, while still providing  
22 adequate solar access for the solar collectors.

23                   (~~D.~~ [Reserved.])

1           ~~E. Nonconforming solar collectors.~~) B. Special exceptions. The Director may permit the  
2 installation of solar collectors that meet minimum energy standards and that increase an existing  
3 nonconformity as a special exception pursuant to Chapter 23.76. Such an installation may be  
4 permitted even if it exceeds the height limits established in this Section 23.45.545 and Section  
5 23.45.514 when the following conditions are met:

6                   1. There is no feasible alternative solution to placing the collector(s) on the roof;  
7 and

8                   2. Such collector(s) are located so as to minimize view blockage from  
9 surrounding properties and the shading of property to the north, while still providing adequate  
10 solar access for the solar collectors.

11           ~~((F. Open wet moorage facilities for residential uses are permitted as an accessory use~~  
12 ~~pursuant to Chapter 23.60A, Shoreline District, if only one slip per residential unit is provided.~~

13           ~~G. Bed and breakfast uses. A bed and breakfast use may be operated under the following~~  
14 ~~conditions:~~

15                   1. ~~The bed and breakfast use has a valid business license tax certificate issued by~~  
16 ~~the Department of Finance and Administrative Services;~~

17                   2. ~~All operators of bed and breakfast uses who use a short term rental platform for~~  
18 ~~listing the bed and breakfast shall have a valid short term rental operator's license issued by the~~  
19 ~~Department of Finance and Administrative Services.~~

20                   3. ~~The bed and breakfast use shall be operated by the primary resident of the~~  
21 ~~dwelling unit where the bed and breakfast is located or the resident operator;~~

22                   4. ~~There shall be no evidence of a bed and breakfast use visible from the exterior~~  
23 ~~of the dwelling unit other than a sign permitted by subsection 23.55.022.D.1; and~~

1                   5. ~~A bed and breakfast use may be located in a dwelling unit or an accessory~~  
2 ~~dwelling unit.~~

3                   H. ~~Heat recovery incinerators, located on the same lot as the principal use, may be~~  
4 ~~permitted by the Director as accessory administrative conditional uses, pursuant to Section~~  
5 ~~23.45.506.~~

6                   I. ~~Accessory dwelling units are allowed pursuant to Section 23.42.022.~~

7                   J. ~~Urban farms are subject to the standards in Section 23.42.051 and the conditional use~~  
8 ~~requirement in subsection 23.45.504.C.8.)~~

9                   Section 46. Section 23.45.550 of the Seattle Municipal Code, last amended by Ordinance  
10 126855, is amended as follows:

11 **23.45.550 Alternative ((Standards)) standards for development of ((affordable)) low-income**  
12 **units ((on property owned or controlled by a religious organization))**

13 ~~((In lieu of meeting development standards contained in subsections 23.45.510.B and~~  
14 ~~23.45.510.C (floor area), subsections 23.45.512.A and 23.45.512.B (density), and subsections~~  
15 ~~23.45.514.A and 23.45.514.B (height), a proposed development that meets the requirements of~~  
16 ~~Section 23.42.055 may elect to meet the alternative development standards in this Section~~  
17 ~~23.45.550.))~~

18                   A. Development on a lot that meets the requirements of Section 23.42.055 may elect to  
19 meet the development standards in subsections 23.45.550.B and 23.45.550.C in lieu of the  
20 standards in subsection 23.45.510.C (floor area) and subsections 23.45.514.A and 23.45.514.B  
21 (height).

22                   ((A.)) B. Floor area

1 1. Development permitted pursuant to Section 23.42.055 is subject to the FAR  
 2 limits as shown in Table A for 23.45.550.

<b>Table A for 23.45.550 FAR limits for development permitted pursuant to Section 23.42.055</b>		
<b>Zone</b>	<b>Base FAR</b>	<b>Maximum additional exempt FAR <sup>1</sup></b>
LR1	1.5 <sup>2</sup>	0.3
LR2	<del>((1.8))</del> <u>2.0</u>	0.3
LR3 outside urban centers and urban villages	2.5	0.5
LR3 inside urban centers and urban villages	3.25	0.5
MR	5.0	0.5
HR	16	1.0

Footnotes to Table A for 23.45.550  
<sup>1</sup> Gross floor area for uses listed in subsection 23.45.550.B.2 are exempt from FAR calculations up to this amount.  
<sup>2</sup> Except that lots in LR1 zones that have previously been zoned RSL have a base FAR of 2.7.

3 2. In addition to the FAR exemptions in subsection 23.45.510.D, an additional  
 4 FAR exemption up to the total amount specified in Table A for 23.45.550 is allowed for any  
 5 combination of the following floor area:

- 6 a. Floor area in units with two or more bedrooms and a minimum net unit  
 7 area of 850 square feet;
- 8 b. Floor area of a religious facility; ~~((and))~~
- 9 c. Floor area in a structure designated as a Landmark pursuant to Chapter  
 10 25.12; and/or

1 d. Any floor area in a development located within ~~((1/4 mile (1,320 feet)~~  
2 ~~of a transit stop or station served by a frequent transit route as defined in subsection~~  
3 ~~23.54.015.B.4))~~ a frequent transit service area.

4 3. Split-zoned lots

5 a. On lots located in two or more zones, the FAR limit for the entire lot  
6 shall be the highest FAR limit of all zones in which the lot is located, provided that:

7 1) At least 65 percent of the total lot area is in the zone with the  
8 highest FAR limit;

9 2) No portion of the lot is located in a ~~((neighborhood residential))~~  
10 Neighborhood Residential zone; and

11 3) A minimum setback of 10 feet applies for any lot line that abuts  
12 a lot in a ~~((neighborhood residential))~~ Neighborhood Residential zone.

13 b. For the purposes of this subsection ~~((23.45.550.A.3))~~ 23.45.550.B.3, the  
14 calculation of the percentage of a lot or lots located in two or more zones may include lots that  
15 abut and are in the same ownership at the time of the permit application.

16 ~~((B-))~~ C. Maximum height

17 1. Development permitted pursuant to Section 23.42.055 is subject to the height  
18 limits as shown in Table B for 23.45.550.

<b>Table B for 23.45.550</b>	
<b>Structure height for development permitted pursuant to Section 23.42.055</b>	
<b>Zone</b>	<b>Height limit (in feet)</b>
LR1	<del>((40))</del> <u>50</u>
LR2	50

**Table B for 23.45.550  
Structure height for development permitted pursuant to Section 23.42.055**

<b>Zone</b>	<b>Height limit (in feet)</b>
LR3 outside urban centers and urban villages	55
LR3 inside urban centers and urban villages	65
MR	95
HR	480

2. Split-zoned lots

a. On lots located in two or more zones, the height limit for the entire lot shall be the highest height limit of all zones in which the lot is located, provided that:

1) At least 65 percent of the total lot area is in the zone with the highest height limit;

2) No portion of the lot is located in a ~~((neighborhood residential))~~ Neighborhood Residential zone; and

3) A minimum setback of 10 feet applies for any lot line that abuts a lot in a ~~((neighborhood residential))~~ Neighborhood Residential zone.

b. For the purposes of this subsection ~~((23.45.550.B.2))~~ 23.45.550.C.2, the calculation of the percentage of a lot or lots located in two or more zones may include lots that abut and are in the same ownership at the time of the permit application.

~~((C. Density limits. Development permitted pursuant to this Section 23.45.550 is not subject to the standards of subsections 23.45.512.A and 23.45.512.B.))~~

Section 47. A new Section 23.45.560 is added to the Seattle Municipal Code, as follows:

**23.45.560 Alternative standards for certain development**

1           A. Development that meets all of the following criteria may elect to meet the  
2 development standards in subsections 23.45.560.B and 23.45.560.C in lieu of the standards in  
3 subsections 23.45.510.B and 23.45.510.C (floor area) and subsections 23.45.514.A and  
4 23.45.514.B (height):

5                   1. At least 25 percent of the dwelling units in the development are restricted units  
6 subject to a regulatory agreement, covenant, or other legal instrument enforceable by The City of  
7 Seattle that, for a period of 50 years, ensures that the units are affordable to and reserved solely  
8 for:

9                           a. ~~((in))~~ In the case of rental units, households with annual incomes no  
10 higher than 60 percent of median income; or

11                           b. ~~((in))~~ In the case of ownership units, households with annual incomes  
12 no higher than 80 percent of median income.

13                   2. The restricted units are generally distributed throughout the development and  
14 have substantially the same functionality as unrestricted units, if any, in the development;

15                   3. To the extent practicable, the restricted units are comparable to unrestricted  
16 units, if any, in terms of square footage and number of bedrooms and bathrooms;

17                   4. The tenure (i.e., rental or ownership) of restricted units and unrestricted units, if  
18 any, is the same;

19                   5. If the development containing the restricted units is demolished or converted to  
20 a nonresidential use prior to the end of the 50-year affordability period, the Director shall require  
21 the owner to make a payment in lieu of continuing affordability;

22                   6. For ownership housing, the restricted units are stewarded by a qualified non-  
23 profit organization, which for purposes of this subsection 23.45.560.A means a non-profit



1 organization that the Office of Housing determines as experienced in the development and  
2 stewardship of permanently affordable homes, including:

3 a. Pre-purchase verification of income and other requirements for eligible  
4 households, affordable sale price calculations for approval by the Office of Housing, and  
5 execution of legal restrictions on the property; and

6 b. Post-purchase support for homeowners by facilitating resales,  
7 monitoring compliance with financial, owner occupancy, and other legal requirements, and clear  
8 communication of program guidelines and restrictions.

9 7. At such times as may be required by the Director of Housing but no less than  
10 annually, the property owner (for rental housing) or the qualified non-profit organization (for  
11 ownership housing) agree to file property reports with the Office of Housing, verified upon oath  
12 or affirmation, which shall contain such information as the Office of Housing may deem  
13 necessary to determine compliance with this subsection 23.45.560.A and the regulatory  
14 agreement, covenant, or other legal instrument; and

15 8. In zones that have a mandatory housing affordability suffix, the restricted units  
16 shall count towards any obligation to provide MHA-R units according to subsection  
17 23.58C.050.A, provided that subsections 23.58C.050.B through 23.58C.050.E, except for  
18 subsection 23.58C.050.C.8, shall apply to any dwelling units so counted and shall govern over  
19 any conflicting requirements of this subsection 23.45.560.A.

20 B. Floor area

21 1. Development permitted pursuant to this Section 23.45.560 is subject to the  
22 FAR limits as shown in Table A for 23.45.560.

**Table A for (~~23.45.550~~) 23.45.560  
 FAR limits for development permitted pursuant to Section 23.45.560**

<b>Zone</b>	<b>Base FAR</b>	<b>Maximum additional exempt FAR <sup>1</sup></b>
LR1	2.0 <sup>2</sup>	0.5
LR2	2.0	0.5
LR3 outside <u>regional centers and</u> urban centers ( <del>(and urban villages)</del> )	3.0	0.5
LR3 inside <u>regional centers and</u> urban centers ( <del>(and urban villages)</del> )	3.5	0.5
MR	5.0	0.5
HR	16	1.0

Footnotes to Table A for 23.45.560

<sup>1</sup> Gross floor area for uses listed in subsection 23.45.560.B.2 are exempt from FAR calculations up to this amount.

<sup>2</sup> Except that lots in LR1 zones that have previously been zoned RSL have a base FAR of 2.7.

- 1                    2. In addition to the FAR exemptions in subsection 23.45.510.D, an additional  
 2 FAR exemption up to the total amount specified in Table A for 23.45.560 is allowed for any  
 3 combination of the following floor area:
- 4                    a. Floor area in units with two or more bedrooms and a minimum net unit  
 5 area of 850 square feet;
  - 6                    b. Floor area of a religious facility;
  - 7                    c. Floor area in a structure designated as a Landmark pursuant to Chapter  
 8 25.12; and/or
  - 9                    d. Any floor area in a development located within a frequent transit  
 10 service area.
- 11                    C. Maximum height
- 12                    1. Development permitted pursuant to this Section 23.44.560 is subject to the  
 13 height limits as shown in Table B for 23.45.560.

<b>Table B for 23.45.560</b>	
<b>Structure height for development permitted pursuant to Section ((<del>23.44.560</del>))</b>	
<b><u>23.45.560</u></b>	
<b>Zone</b>	<b>Height limit (in feet)</b>
LR1	55
LR2	55
LR3 (( <del>outside urban centers and urban villages</del> ))	65
(( <del>LR3 inside urban centers and urban villages</del> ))	<del>65</del> )
MR	95
HR	480

1 Section 48. Table A for Section 23.47A.004 of the Seattle Municipal Code, which section  
 2 was last amended by Ordinance ~~127099-127228~~, is amended as follows:

3 **23.47A.004 Permitted and prohibited uses**

4 \* \* \*

<b>Table A for 23.47A.004</b>					
<b>Uses in ((<del>Commercial</del>)) <u>commercial zones</u></b>					
	<b>Permitted and prohibited uses by zone <sup>1</sup></b>				
<b>Uses</b>	<b>NC1</b>	<b>NC2</b>	<b>NC3</b>	<b>C1</b>	<b>C2</b>
* * *					
<b>E. ((<del>INSTITUTIONS</del>)) <u>HUMAN SERVICE AND INSTITUTIONAL USES</u></b>					
E.1. (( <del>Institutions</del> )) <u>Human service and institutional uses not listed below</u>	10	25	P	P	P
E.2. Major institutions subject to the provisions of Chapter 23.69	P	P	P	P	P
E.3. Religious facilities	P	P	P	P	P
E.4. Schools, elementary or secondary	P	P	P	P	P
E.5. Child care centers	P	P	P	P	P

**Table A for 23.47A.004  
 Uses in ~~((Commercial))~~ commercial zones**

		Permitted and prohibited uses by zone <sup>1</sup>				
Uses		NC1	NC2	NC3	C1	C2
* * *						
I. PUBLIC FACILITIES						
I.1. Jails						
	I.1.a. Youth <del>((Service Centers))</del> <u>service centers</u>	X	X	P <sup>13</sup>	X	X
	I.1.b. All other jails	X	X	X	X	X
	I.2. Work-release centers	CCU-10	CCU-25	CCU	CCU	CCU
J. RESIDENTIAL USES <sup>14</sup>		<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>CU</u> <sup>15</sup>
	<del>((J.1. Residential uses not listed below</del>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>CU</u> <sup>15</sup>
	J.2. Caretaker's quarters	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>
	J.3. Congregate residence	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>CU</u> <sup>15</sup>
	J.4. Low income housing	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u> )
* * *						

~~((KEY))~~ Key to Table A for 23.47A.004

A = Permitted as an accessory use only  
 CU = Administrative ~~((Conditional Use))~~ conditional use (business establishment limited to the multiple of 1,000 square feet of any number following a hyphen, pursuant to Section 23.47A.010)  
 CCU = Council ~~((Conditional Use))~~ conditional use (business establishment limited to the multiple of 1,000 square feet of any number following a hyphen, pursuant to Section 23.47A.010)  
 P = Permitted  
 S = Permitted in shoreline areas only

**Table A for 23.47A.004  
 Uses in ((Commercial)) commercial zones**

Uses	Permitted and prohibited uses by zone <sup>1</sup>				
	NC1	NC2	NC3	C1	C2
<p>X = Prohibited            CU-25 = Conditionally permitted; use is limited to 25,000 square feet, pursuant to Section 23.47A.010            10 = Permitted, business establishments limited to 10,000 square feet, pursuant to Section 23.47A.010            20 = Permitted, business establishments limited to 20,000 square feet, pursuant to Section 23.47A.010            25 = Permitted, business establishments limited to 25,000 square feet, pursuant to Section 23.47A.010            35 = Permitted, business establishments limited to 35,000 square feet, pursuant to Section 23.47A.010            40 = Permitted, business establishments limited to 40,000 square feet, pursuant to Section 23.47A.010            50 = Permitted, business establishments limited to 50,000 square feet, pursuant to Section 23.47A.010</p>					

Footnotes to Table A for 23.47A.004

<sup>1</sup> In pedestrian-designated zones, a portion of the street-level street-facing facade of a structure along a designated principal pedestrian street may be limited to certain uses as provided in subsection 23.47A.005.D. In pedestrian-designated zones, drive-in lanes are prohibited (Section 23.47A.028).

<sup>2</sup> In addition to the provisions in this Chapter 23.47A, uses that entail major cannabis activity are subject to the requirements of Section 23.42.058.

<sup>3</sup> For commercial uses with drive-in lanes, see Section 23.47A.028.

<sup>4</sup> Subject to subsection 23.47A.004.H.

<sup>5</sup> Permitted at Seattle Center.

<sup>6</sup> Bed and breakfasts in existing structures are permitted outright with no maximum size limit.

<sup>7</sup> Medical services over 10,000 square feet within 2,500 feet of a medical Major Institution Overlay boundary require conditional use approval, unless they are included in a Major Institution Master Plan or dedicated to veterinary services.

<sup>8</sup> Medical service uses that are located in an urban center or urban village, which are in operation at such location before August 1, 2015, and that routinely provide medical services on a reduced fee basis to individuals or families having incomes at or below 200 percent of the poverty guidelines updated periodically in the Federal Register by the U.S. Department of Health and Human Services under the authority of 42 USC 9902(2), are limited to 20,000 square feet. This provision does not apply to medical service uses that are subject to a Major Institution Master Plan.

**Table A for 23.47A.004  
 Uses in ~~((Commercial))~~ commercial zones**

Uses	Permitted and prohibited uses by zone <sup>1</sup>				
	NC1	NC2	NC3	C1	C2
<p><sup>9</sup> Office uses in C1 and C2 zones are permitted up to the greater of 1 FAR or 35,000 square feet as provided in subsection 23.47A.010.D. Office uses in C1 and C2 zones are permitted outright with no maximum size limit if they meet the standards identified in subsection 23.47A.010.D.</p> <p><sup>10</sup> Gas stations and other businesses with drive-in lanes are not permitted in pedestrian-designated zones (Section 23.47A.028). Elsewhere in NC zones, establishing a gas station may require a demonstration regarding impacts under Section 23.47A.028.</p> <p><sup>11</sup> Grocery stores meeting the conditions of subsection 23.47A.010.E are permitted up to 23,000 square feet in size.</p> <p><sup>12</sup> Subject to subsection 23.47A.004.G.</p> <p><sup>13</sup> Permitted pursuant to subsection 23.47A.004.D.7.</p> <p><sup>14</sup> Residential uses may be limited to 20 percent of a street-level street-facing facade pursuant to subsection 23.47A.005.C.</p> <p><sup>15</sup> Residential uses are conditional uses in C2 zones <del>((under))</del> <u>subject to</u> subsection 23.47A.006.A.3, except <u>that low-income housing is allowed outright or</u> as otherwise provided <del>((above in Table A for 23.47A.004 or))</del> in subsection 23.47A.006.A.3.</p> <p><sup>16</sup> Permitted at Seattle Center; see Section 23.47A.011.</p> <p><sup>17</sup> Flexible-use parking is subject to Section 23.54.026. In pedestrian-designated zones, surface parking is prohibited adjacent to principal pedestrian streets pursuant to subsection 23.47A.032.B.2.</p> <p><sup>18</sup> Permitted as surface parking only on surface parking lots existing as of January 1, 2017. In pedestrian-designated zones, surface parking is prohibited adjacent to principal pedestrian streets pursuant to subsection 23.47A.032.B.2.</p> <p><sup>19</sup> Permitted outright, except prohibited in <del>((the))</del> <u>a</u> SAOD.</p> <p><sup>20</sup> See Chapter 23.57, Communications regulations, for regulation of communication utilities.</p> <p><sup>21</sup> A recycling use that is located on the same development site as a solid waste transfer station may be permitted by administrative conditional use, subject to the requirements of subsection 23.47A.006.A.7.</p>					

1 Section 49. Section 23.47A.009 of the Seattle Municipal Code, last amended by  
 2 Ordinance 126862, is amended as follows:

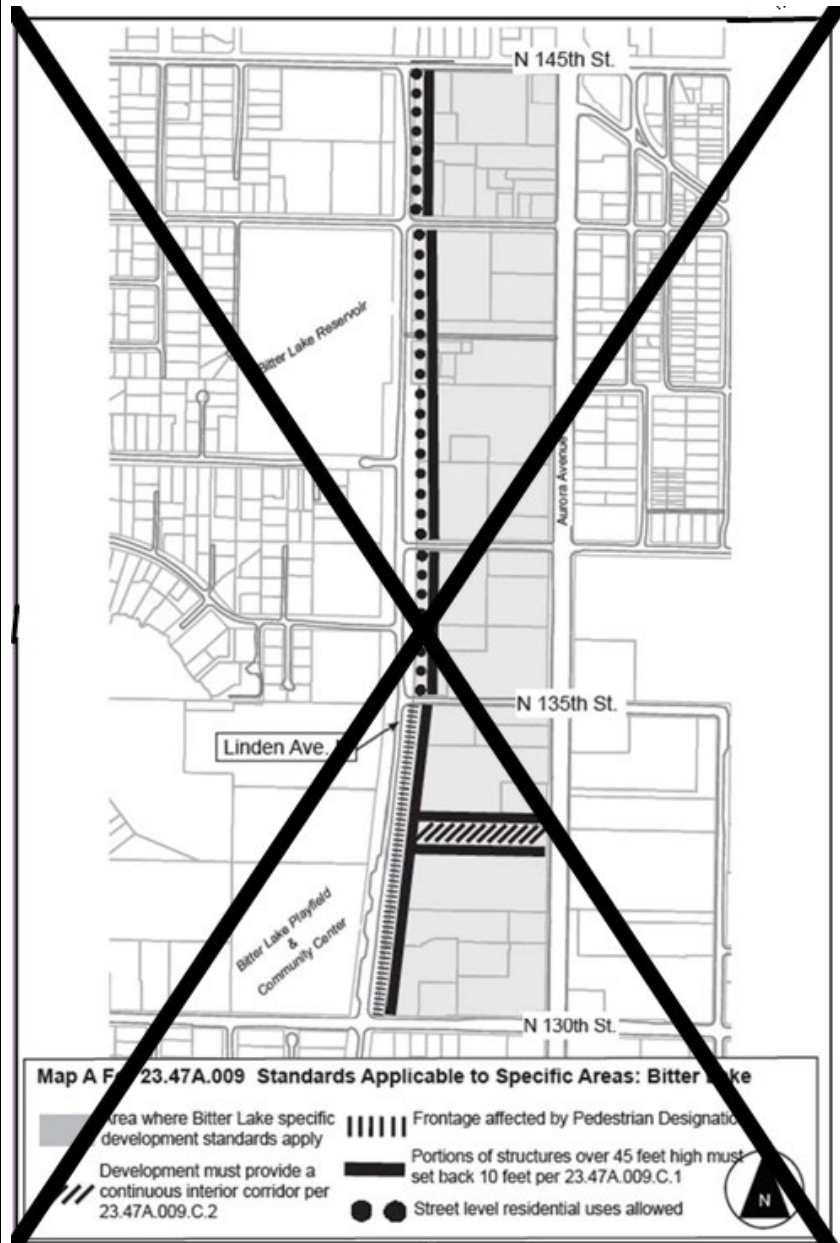
3 **23.47A.009 Standards applicable to specific areas**

4 \* \* \*

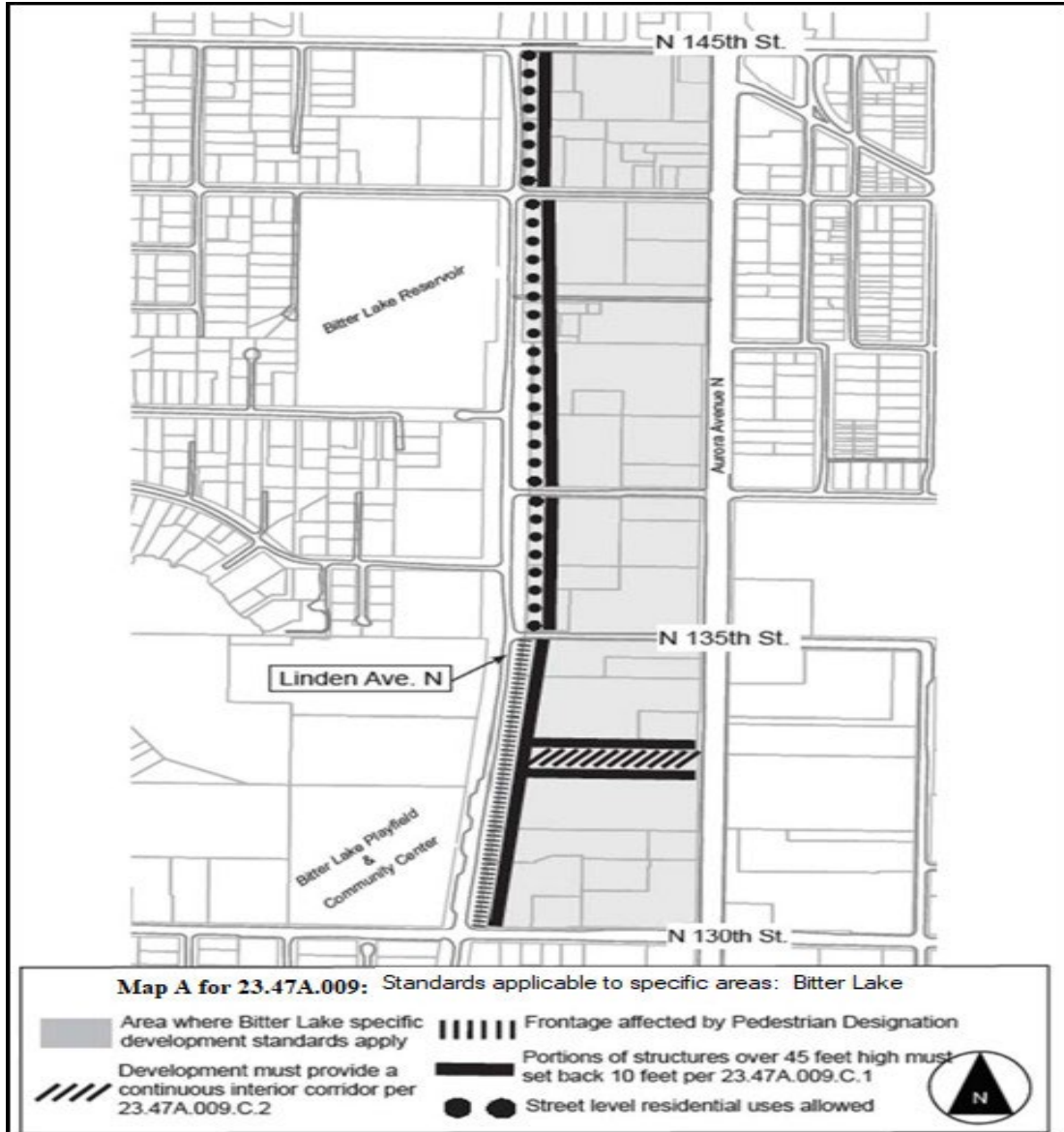
1 C. Bitter Lake Village Hub Urban Village. Development on lots designated on Map A for  
2 23.47A.009 shall meet the following requirements:

3 **Map A for 23.47A.009**

4 **Standards ((Applicable)) applicable to ((Specific Areas)) specific areas: Bitter Lake**



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1. Upper-level setback requirement. The following standards apply to development on lots abutting the east side of Linden ((Ave)) Avenue North or along both sides of the corridor required in subsection 23.47A.009.C.2.

a. Any portion of a structure greater than 45 feet in height, measured from the finished grade along the street property line that abuts Linden Avenue North or along the access corridor required in subsection 23.47A.009.C.2, measured from the finished grade along the edge of the access corridor, shall set back an average of 10 feet from the lot line abutting



1 Linden Avenue North or from the edge of the access corridor as measured according to Section  
2 23.86.012. The maximum depth of a setback that can be used for calculating the average setback  
3 is 20 feet.

4 b. Structures permitted in required setbacks are subject to subsection  
5 23.47A.014.G.

6 2. Corridor requirement. An access corridor shall be provided on lots over 8 acres  
7 that abut Linden Avenue North and Aurora Avenue North, to connect Linden Avenue North and  
8 Aurora Avenue North. The location of the proposed corridor shall be clearly shown on the site  
9 plan that is submitted with the permit application.

10 a. The corridor shall have a minimum width of 40 feet and a maximum  
11 width of 60 feet.

12 b. The point at which the corridor intersects Linden Avenue North and  
13 Aurora Avenue North shall be at least 335 feet south of the south boundary of the North 135th  
14 Street right-of-way, and 700 feet north of the north boundary of the North 130th Street right-of-  
15 way, as illustrated by example in Map A for 23.47A.009.

16 c. The corridor shall include a minimum of one walkway, at least 6 feet  
17 wide, extending between Linden Avenue North and Aurora Avenue North. If vehicle access is  
18 provided within the corridor, the corridor shall include walkways at least 6 feet wide along both  
19 sides of the vehicle access.

20 d. Landscaping shall be provided along the corridor. If vehicle access is  
21 provided within the corridor, trees shall be provided between the walkways and vehicle travel  
22 lanes. The Director will determine the number, type, and placement of trees to be provided in  
23 order to:

- 1 1) Match trees to the available space;
- 2 2) Complement existing or planned street trees on abutting streets;

3 and

- 4 3) Encourage healthy growth through appropriate spacing.

5 e. Pedestrian-scaled lighting shall be provided along the corridor.

6 f. The corridor shall not include any features or structures except the  
7 following:

8 1) Vehicle access, not more than one lane in each direction and  
9 meeting the standards of Section 23.54.030.

10 2) Parking meeting the standards of Section 23.54.030 is allowed  
11 along vehicle access lanes within the corridor. Such parking is in addition to the maximum  
12 number of spaces allowed under subsection 23.54.015.C.2. The requirements of subsection  
13 23.47A.032.A do not apply to access to parking from the corridor.

14 3) Overhead horizontal building projections of an architectural or  
15 decorative character such as cornices, eaves, sills, and gutter, provided that they project no more  
16 than 18 inches from the structure facade.

17 4) Ramps or other devices that provide access for the disabled and  
18 elderly and that meet the standards of the Seattle Building Code are permitted.

19 5) Stairs or ramps to accommodate changes in grade.

20 6) Underground structures.

21 7) Unenclosed porches or steps for residential units no higher than  
22 4 feet above the finished grade of the corridor are permitted to project no more than 4 feet into  
23 the corridor.



1 **Map B for 23.47A.009**

2 **Roosevelt**



3

4

1. Setback requirements

5

a. The following setbacks are required from the listed street property lines:

6

1) Northeast 66th Street. An average ground-level setback of 10

7

feet along the length of the street property line and a minimum upper-level setback of 4 feet. The

8

minimum upper-level setback shall be provided in addition to the required ground-level setback

1 at all points along the length of the street property line at 45 feet of height and above, as  
2 measured from average finished grade.

3                                   2) Brooklyn Avenue Northeast. An average ground-level setback  
4 of 5 feet along the length of the street property line and a minimum upper-level setback of 4 feet.  
5 The minimum upper-level setback shall be provided in addition to the required ground-level  
6 setback at all points along the length of the street property line at 45 feet of height and above, as  
7 measured from average finished grade.

8                                   3) 14th Avenue Northeast. An average ground-level setback of 15  
9 feet and a minimum ground-level setback of 5 feet along the length of the street property line and  
10 a minimum upper-level setback of 3 feet. The minimum upper-level setback shall be provided in  
11 addition to the required ground-level setback at all points along the length of the street property  
12 line at 45 feet of height and above, as measured from average finished grade.

13                                   4) 15th Avenue Northeast. A minimum ground-level setback of 5  
14 feet along the length of the street property line and an average upper-level setback of 7 feet. The  
15 average upper-level setback shall be provided in addition to the required ground-level setback at  
16 all points along the length of the street property line at 45 feet of height and above, as measured  
17 from average finished grade.

18                                   5) Northeast 65th Street and 12th Avenue Northeast. An average  
19 ground-level setback of 8 feet shall be provided, and the setback may include pedestrian access  
20 and circulation.

21                                   b. Structures permitted in required setbacks are subject to subsection  
22 23.47A.014.G, except that:





1                   2. All portions of a story that extend no more than 4 feet above existing or  
2 finished grade, whichever is lower, excluding access;

3                   3. Gross floor area of a transit station, including all floor area open to the general  
4 public during normal hours of station operation but excluding retail or service establishments to  
5 which public access is limited to customers or clients, even where such establishments are  
6 primarily intended to serve transit riders;

7                   4. On a lot containing a peat settlement-prone environmentally critical area,  
8 above-grade parking within or covered by a structure or portion of a structure, if the Director  
9 finds that locating a story of parking below grade is infeasible due to physical site conditions  
10 such as a high water table, if either:

11                   a. The above-grade parking extends no more than 6 feet above existing or  
12 finished grade and no more than 3 feet above the highest existing or finished grade along the  
13 structure footprint, whichever is lower, as measured to the finished floor level or roof above,  
14 pursuant to subsection 23.47A.012.A.3; or

15                   b. All of the following conditions are met:

16                                   1) No above-grade parking is exempted by subsection  
17 23.47A.013.B.4.a;

18                                   2) The parking is accessory to a residential use on the lot;

19                                   3) Total parking on the lot does not exceed one space for each  
20 residential dwelling unit plus the number of spaces required for ~~((non-residential))~~ nonresidential  
21 uses; and

22                                   4) The amount of gross floor area exempted by this subsection  
23 23.47A.013.B.4.b does not exceed 25 percent of the area of the lot in zones with a height limit



1 less than 65 feet, or 50 percent of the area of the lot in zones with a height limit 65 feet or  
2 greater; (~~and~~)

3 5. Rooftop greenhouse areas meeting the standards of subsections  
4 23.47A.012.C.4, 23.47A.012.C.5, and 23.47A.012.C.6;

5 6. Bicycle commuter shower facilities required by subsection (~~(23.54.015.K.8)~~)  
6 23.54.037.H;

7 7. The floor area of required bicycle parking for small efficiency dwelling units or  
8 congregate residence sleeping rooms, if the bicycle parking is located within the structure  
9 containing the small efficiency dwelling units or congregate residence sleeping rooms. Floor area  
10 of bicycle parking that is provided beyond the required bicycle parking is not exempt from FAR  
11 limits;

12 8. All gross floor area in child care centers; and

13 9. In low-income housing, all gross floor area for accessory human service uses.

14 \* \* \*

15 Section 51. Section 23.47A.032 of the Seattle Municipal Code, last amended by  
16 Ordinance 125558, is amended as follows:

17 **23.47A.032 Parking location and access**

18 A. Access to parking

19 1. NC zones. The following rules apply in NC zones, except as provided under  
20 subsections 23.47A.032.A.2 and 23.47A.032.D:

21 a. Access to parking shall be from the alley if the lot abuts an alley  
22 improved to the standards of subsection 23.53.030.C, or if the Director determines that alley

1 access is feasible and desirable to mitigate parking access impacts. If alley access is infeasible,  
2 the Director may allow street access.

3                   b. If access is not provided from an alley and the lot abuts only one street,  
4 access is permitted from the street, and limited to one two-way curb cut.

5                   c. If access is not provided from an alley and the lot abuts two or more  
6 streets, access is permitted across one of the side street lot lines pursuant to subsection  
7 23.47A.032.C, and curb cuts are permitted pursuant to ~~((subsection 23.54.030.F.2.a.1))~~ Section  
8 23.54.031.

9                   d. For each permitted curb cut, street-facing facades may contain one  
10 garage door, not to exceed the maximum width allowed for curb cuts.

11                   2. In addition to the provisions governing NC zones in subsection  
12 23.47A.032.A.1, the following rules apply in pedestrian-designated zones, except as may be  
13 permitted under subsection 23.47A.032.D:

14                   a. If access is not provided from an alley and the lot abuts two or more  
15 streets, access to parking shall be from a street that is not a principal pedestrian street.

16                   b. If access is not provided from an alley and the lot abuts only a principal  
17 pedestrian street or streets, access is permitted from the principal pedestrian street, and limited to  
18 one two-way curb cut.

19                   3. In C1 and C2 zones, access to off-street parking may be from a street, alley, or  
20 both when the lot abuts an alley. However, structures in C zones with residential uses, structures  
21 in C zones with pedestrian designations, and structures in C zones across the street from  
22 residential zones shall meet the requirements for parking access for NC zones as provided in  
23 subsection 23.47A.032.A.1. If two or more structures are located on a single site, then a single

1 curb cut shall be provided according to the standards in ~~((Sections))~~ subsections  
2 23.47A.032.A.1~~((;))~~ and 23.47A.032.A.2~~((;))~~ and ~~((23.54.030.F.2))~~ Section 23.54.031.

3 4. In the event of conflict between the standards for curb cuts in this subsection  
4 23.47A.032.A and the provisions of ~~((subsection 23.54.030.F))~~ Section 23.54.031, the standards  
5 in ~~((subsection 23.54.030.F))~~ Section 23.54.031 shall control.

6 \* \* \*

7 Section 52. Section 23.48.020 of the Seattle Municipal Code, last amended by Ordinance  
8 127198, is amended as follows:

9 **23.48.020 Floor area ratio (FAR)**

10 \* \* \*

11 B. Floor area exempt from FAR calculations. The following floor area is exempt from  
12 maximum FAR calculations:

- 13 1. All underground stories or portions of stories.
- 14 2. Portions of a story that extend no more than 4 feet above existing or finished  
15 grade, whichever is lower, excluding access.
- 16 3. As an allowance for mechanical equipment, in any structure 65 feet in height or  
17 more, 3.5 percent of the total chargeable gross floor area in a structure is exempt from FAR  
18 calculations. Calculation of the allowance includes the remaining gross floor area after all  
19 exempt space allowed in this subsection 23.48.020.B has been deducted. Mechanical equipment  
20 located on the roof of a structure, whether enclosed or not, is not included as part of the  
21 calculation of total gross floor area.
- 22 4. All gross floor area for solar collectors and wind-driven power generators.

5. Bicycle commuter shower facilities required by ~~((subsection 23.54.015.K.8))~~

Section 23.54.037.

6. The floor area of required bicycle parking for small efficiency dwelling units or congregate residence sleeping rooms, if the bicycle parking is located within the structure containing the small efficiency dwelling units or congregate residence sleeping rooms. Floor area of bicycle parking that is provided beyond the required bicycle parking is not exempt from FAR limits.

7. Child care centers.

8. In low-income housing, all gross floor area for accessory human service uses.

9. Other uses permitted by interim street activation provisions in Section 23.42.041.

\* \* \*

Section 53. Section 23.49.019 of the Seattle Municipal Code, last amended by Ordinance 125815, is amended as follows:

**23.49.019 Parking quantity, location, and access requirements, and screening and landscaping of parking areas**

The regulations in this Section 23.49.019 do not apply to the Pike Market Mixed zones.

A. Parking quantity requirements

1. No parking, either long-term or short-term, is required for uses on lots in ~~((Downtown))~~ downtown zones, except as follows:

a. In the International District Mixed and International District Residential zones, parking requirements for restaurants, motion picture theaters, and other entertainment uses are as prescribed by Section 23.66.342.



1 C. Maximum parking limits

2 1. Except as provided in subsections 23.49.019.C.2 and 23.66.342.B, parking for  
3 ~~((non-residential))~~ nonresidential uses is limited to a maximum of one parking space per 1,000  
4 square feet.

5 2. In the area east of Interstate 5, parking for general sales and service uses and  
6 for eating and drinking establishments is limited to a maximum of two parking spaces per 1,000  
7 square feet.

8 D. Ridesharing and transit incentive program requirements. The following requirements  
9 apply to all new structures containing more than 10,000 square feet of new ~~((non-residential))~~  
10 nonresidential use, and to structures where more than 10,000 square feet of ~~((non-residential))~~  
11 nonresidential use is proposed to be added.

12 1. The building owner shall establish and maintain a transportation coordinator  
13 position for the proposed structure and designate a person to fill this position, or the building  
14 owner may contract with an area-wide transportation coordinator acceptable to the Department.  
15 The transportation coordinator shall devise and implement alternative means for employee  
16 commuting. The transportation coordinator shall be trained by the Seattle Department of  
17 Transportation or by an alternative organization with ridesharing experience, and shall work with  
18 the Seattle Department of Transportation and building tenants. The coordinator shall disseminate  
19 ridesharing information to building occupants to encourage use of public transit, carpools,  
20 vanpools, and flextime; administer the in-house ridesharing program; and aid in evaluation and  
21 monitoring of the ridesharing program by the Seattle Department of Transportation. The  
22 transportation coordinator in addition shall survey all employees of building tenants once a year  
23 to determine commute mode percentages.



1 Director of the Seattle Department of Transportation. Unless the Director otherwise determines  
2 under subsection 23.49.019.H.1.c, access is allowed only from a right-of-way in the category,  
3 determined by the classifications shown on Map 1B and Map 1F of the Downtown Overlay  
4 Maps or another map identified in a note to Map 1F, that is most preferred among the categories  
5 of rights-of-way abutting the lot, according to the ranking set forth below, from most to least  
6 preferred (a portion of a street that is included in more than one category is considered as  
7 belonging only to the least preferred of the categories in which it is included):

- 8 1) Access street;
- 9 2) Class II pedestrian street/Minor arterial;
- 10 3) Class II pedestrian street/Principal arterial;
- 11 4) Class I pedestrian street/Minor arterial;
- 12 5) Class I pedestrian street/Principal arterial;
- 13 6) Principal transit street;
- 14 7) Designated green street.

15 c. The Director may allow or require access from a right-of-way other than  
16 one indicated by subsection 23.49.019.H.1.a or 23.49.019.H.1.b if, after consulting with the  
17 Director of the Seattle Department of Transportation on whether and to what extent alternative  
18 locations of access would enhance pedestrian safety and comfort, facilitate transit operations,  
19 facilitate the movement of vehicles, minimize the on-street queuing of vehicles, enhance  
20 vehicular safety, or minimize hazards, and, for hotel use, improve passenger loading safety or  
21 increase visibility of vehicular access for guests arriving by car, the Director finds that an  
22 exception to the general policy is warranted. The Director may approve an exception for hotel  
23 use and impose conditions to minimize any adverse impacts to the pedestrian environment or



1 street operations, including but not limited to allowing one-way driveways that are less than the  
2 minimum width otherwise required. Curb cut controls on designated green streets shall be  
3 evaluated on a case-by-case basis, but generally access from green streets is not allowed if access  
4 from any other right-of-way is possible.

5 d. If a street or alley vacation is proposed, the Director shall consult with  
6 the Seattle Design Commission on how the location and extent of proposed curb cuts affects or  
7 impacts the public realm and how those impacts have been reduced.

8 2. Curb cut width and number. The width and number of ~~((curb cuts))~~ curb cuts  
9 shall comply with Section ~~((23.54.030, Parking space standards))~~ 23.54.031.

10 I. Screening and landscaping of surface parking areas

11 1. Screening. Surface parking areas for more than five vehicles shall be screened  
12 in accordance with the following requirements:

13 a. Screening is required along each street lot line.

14 b. Screening shall consist of:

15 1) A view-obscuring fence or wall at least 3 feet in height; or

16 2) A landscaped area with vegetation at least 3 feet in height.

17 Landscaped areas may include bioretention facilities or landscaped berms, provided that the top  
18 of the vegetation is at least 3 feet above the grade abutting the facility or berm.

19 c. A landscaped strip on the street side of the fence or wall shall be  
20 provided if a fence or wall is used for screening. The strip shall be an average of 3 feet from the  
21 property line, but at no point less than 1.5 feet wide. Each landscaped strip shall be planted with  
22 sufficient shrubs, grass, and/or evergreen groundcover so that the entire strip, excluding

1 driveways, will be covered in three years. Each landscaped strip may be a bioretention facility, at  
2 grade, or a raised berm.

3 d. Sight triangles shall be provided in accordance with Section  
4 ~~((23.54.030, Parking space standards))~~ 23.54.032.

5 2. Landscaping. Surface parking areas for 20 or more vehicles, except temporary  
6 surface parking areas, shall be landscaped according to the following requirements:

7 a. The amount of landscaped area required is shown on Table B for  
8 23.49.019:

<b>Table B for 23.49.019 Required landscaping for surface parking areas with 20 or more parking spaces</b>	
<b>Total number of parking spaces</b>	<b>Minimum required landscaped area</b>
20 to 50	18 square feet per parking space
51 to 99	25 square feet per parking space
100 or more spaces	35 square feet per parking space

9 b. The minimum size of a required landscaped area is 100 square feet.

10 Berms provided to meet the screening standards in subsection 23.49.019.I.1 may be counted as  
11 part of a landscaped area. No part of a landscaped area shall be less than 4 feet in any dimension  
12 except those dimensions reduced by turning radii or angles of parking spaces.

13 c. The landscaped area may include bioretention facilities.

14 d. No parking stall shall be more than 60 feet from a required landscaped  
15 area.

16 e. One tree per every five parking spaces is required.

1 f. Each tree shall be at least 3 feet from any curb of a landscaped area or  
2 edge of the parking area.

3 g. Permanent curbs or structural barriers shall protect landscaped areas.

4 h. Sufficient hardy evergreen groundcover shall be planted to cover each  
5 landscaped area completely within three years. Trees shall be selected from the Seattle  
6 Department of Transportation's list for parking area planting.

7 J. Transportation management programs

8 1. When a development is proposed that is expected to generate 50 or more  
9 employees single-occupant vehicle (SOV) trips in any one p.m. hour, the applicant shall prepare  
10 and implement a Transportation Management Program (TMP) consistent with requirements for  
11 TMPs in any applicable Director's Rule.

12 a. For purposes of measuring attainment of SOV goals contained in the  
13 TMP, the proportion of SOV trips shall be calculated for the p.m. hour in which an applicant  
14 expects the largest number of vehicle trips to be made by employees at the site (the p.m. peak  
15 hour of the generator). The proportion of SOV trips shall be calculated by dividing the total  
16 number of employees using an SOV to make a trip during the expected peak hour by the total  
17 number of employee person trips during the expected peak hour.

18 b. Compliance with this ~~((section))~~ Section 23.49.019 does not supplant  
19 the responsibility of any employer to comply with Seattle's Commute Trip Reduction (CTR)  
20 Ordinance.

21 2. An applicant who proposes multifamily development that is expected to  
22 generate 50 or more vehicle trips in any one p.m. hour or demand for 25 or more vehicles  
23 parking on the street overnight shall prepare and implement a TMP. The TMP shall be consistent



1                   b. All gross floor area used for accessory parking, except as provided in  
2 subsection 23.50.028.D;

3                   c. All gross floor area located on the rooftop of a structure and used for  
4 any of the following: mechanical equipment, stair and elevator penthouses, and communication  
5 equipment and antennas;

6                   d. All gross floor area used for covered rooftop recreational space of a  
7 building existing as of December 31, 1998, in an IG1 or IG2 zone, if complying with subsection  
8 23.50.012.D; and

9                   e. Bicycle commuter shower facilities required by subsection  
10 ~~((23.54.015.K.8))~~ 23.54.037.H.

11                   2. In addition to areas exempt from FAR calculations in subsection 23.50.028.B.1,  
12 within IG1 and IG2 zones, the gross floor area of rooftop recreational space accessory to office  
13 use meeting the standards of subsection 23.50.012.D is exempt from FAR calculations.

14                   Section 55. Section 23.51A.002 of the Seattle Municipal Code, last amended by  
15 Ordinance ~~126685~~ 127228, is amended as follows:

16 **23.51A.002 Public facilities in ~~((neighborhood residential))~~ Neighborhood Residential zones**

17                   A. Except as provided in subsections 23.51A.002.B, 23.51A.002.D, ~~and~~ 23.51A.002.F,  
18 and 23.51.A.002.G, uses in public facilities that are most similar to uses permitted outright or  
19 permitted as an administrative conditional use under Chapter 23.44 are also permitted outright or  
20 as an administrative conditional use, subject to the same use regulations, development standards  
21 and administrative conditional use criteria that govern the similar use. The ~~((City))~~ Council may  
22 waive or modify applicable development standards or administrative conditional use criteria  
23 according to the provisions of Chapter 23.76, Subchapter III, ~~((Council Land Use Decisions,))~~

1 with public projects considered as Type IV quasi-judicial decisions and City facilities considered  
2 as Type V legislative decisions.

3 B. Permitted ~~((Uses))~~ uses in ~~((Public Facilities Requiring))~~ public facilities requiring  
4 City Council ~~((Approval))~~ approval. The following uses in public facilities in ~~((neighborhood~~  
5 ~~residential))~~ Neighborhood Residential zones may be permitted by the City Council, according to  
6 the provisions of Chapter 23.76~~((, Procedures for Master Use Permits and Council Land Use~~  
7 ~~Decisions))~~:

- 8 1. Police precinct station;
- 9 2. Fire station;
- 10 3. Public boat moorage;
- 11 4. Utility services use; and
- 12 5. Other similar use.

13 The proponent of any such use shall demonstrate the existence of a public necessity for  
14 the public facility use in a ~~((neighborhood residential))~~ Neighborhood Residential zone. The  
15 public facility use shall be developed according to the development standards for institutions  
16 (Section ~~((23.44.022))~~ 23.44.180), unless the City Council makes a determination to waive or  
17 modify applicable development standards according to the provisions of Chapter 23.76,  
18 Subchapter III, ~~((Council Land Use Decisions,))~~ with public projects considered as Type IV  
19 quasi-judicial decisions and City facilities considered as Type V legislative decisions.

20 \* \* \*

21 D. Sewage treatment plants. The expansion or reconfiguration (which term shall include  
22 reconstruction, redevelopment, relocation on the site, or intensification of treatment capacity) of  
23 existing sewage treatment plants in ~~((neighborhood residential))~~ Neighborhood Residential zones

1 may be permitted if there is no feasible alternative location in a zone where the use is permitted  
2 and the conditions imposed under subsections 23.51A.002.D.3 and 23.51A.002.D.4 are met.

3 1. Applicable procedures. Except as provided in subsection 23.51A.002.C.2.a, the  
4 decision on an application for the expansion or reconfiguration of a sewage treatment plant is a  
5 Type IV Council land use decision. If an application for an early determination of feasibility is  
6 required to be filed pursuant to subsection 23.51A.002.D.2, the early determination of feasibility  
7 will also be a Council land use decision subject to Sections 23.76.038 through 23.76.056.

8 2. Need for feasible alternative determination. The proponent shall demonstrate  
9 that there is no feasible alternative location in a zone where establishment of the use is permitted.

10 a. The Council's decision as to the feasibility of alternative location(s)  
11 shall be based upon a full consideration of the environmental, social, and economic impacts on  
12 the community, and the intent to preserve and to protect the physical character of neighborhood  
13 residential areas, and to protect neighborhood residential areas from intrusions of (~~non-single-~~  
14 ~~family~~) nonresidential uses.

15 b. The determination of feasibility may be the subject of a separate  
16 application for a Council land use decision prior to submission of an application for a project-  
17 specific approval if the Director determines that the expansion or reconfiguration proposal is  
18 complex, involves the phasing of programmatic and project-specific decisions, or affects more  
19 than one site in a (~~neighborhood residential~~) Neighborhood Residential zone.

20 c. Application for an early determination of feasibility shall include:

21 1) The scope and intent of the proposed project in the  
22 (~~neighborhood residential~~) Neighborhood Residential zone and appropriate alternative(s) in  
23 zones where establishment of the use is permitted, identified by the applicant or the Director;





1                                   d. Measures to minimize potential odor emission and airborne pollutants  
2 including methane shall meet standards of and be consistent with best available technology as  
3 determined in consultation with the Puget Sound Clean Air Agency (PSCAA), and shall be  
4 incorporated into the design and operation of the facility.

5                                   e. Methods of storing and transporting chlorine and other hazardous and  
6 potentially hazardous chemicals shall be determined in consultation with the Seattle Fire  
7 Department and incorporated into the design and operation of the facility.

8                                   f. Vehicular access suitable for trucks is available or provided from the  
9 plant to a designated arterial improved to City standards.

10                                  g. The bulk of facilities shall be compatible with the surrounding  
11 community. Public facilities that do not meet bulk requirements may be located in  
12 ~~((neighborhood residential))~~ Neighborhood Residential zones if there is a public necessity for  
13 their location there.

14                                  h. Landscaping and screening, separation from less intensive zones, noise,  
15 light and glare controls, and other measures to ensure the compatibility of the use with the  
16 surrounding area and to mitigate adverse impacts shall be incorporated into the design and  
17 operation of the facility.

18                                  i. No residential structures, including those modified for nonresidential  
19 use, are demolished for facility expansion unless a need has been demonstrated for the services  
20 of the institution or facility in the surrounding community.

21                                  4. Substantial ~~((Conformance))~~ conformance. If the application for a project-  
22 specific proposal is submitted after an early determination that location of the sewage treatment  
23 plant is not feasible in a zone where establishment of the use is permitted, the proposed project

1 must be in substantial conformance with the feasibility determination. Substantial conformance  
2 shall include, but not be limited to, a determination that:

3 a. There is no net substantial increase in the environmental impacts of the  
4 project-specific proposal as compared to the impacts of the proposal as approved in the  
5 feasibility determination.

6 b. Conditions included in the feasibility determination are met.

7 E. Prohibited ~~((Uses))~~ uses. ~~((The))~~ Unless determined to be an essential public facility  
8 under Chapter 23.80, the following public facilities are prohibited in ~~((neighborhood residential))~~

9 Neighborhood Residential zones:

- 10 1. Jails;
- 11 2. Metro operating bases;
- 12 3. Park and ride lots;
- 13 4. Establishment of new sewage treatment plants;
- 14 5. Solid waste transfer stations;
- 15 6. Animal control shelters;
- 16 7. Post Office distribution centers; and
- 17 8. Work-release centers.

18 F. Essential ~~((Public Facilities))~~ public facilities except for light rail transit facilities.  
19 ~~((Permitted essential))~~ Essential public facilities, except for light rail transit facilities, shall also  
20 be reviewed according to the provisions of Chapter 23.80~~((, Essential Public Facilities))~~.

21 \* \* \*

22 Section 56. Section 23.51B.002 of the Seattle Municipal Code, last amended by  
23 Ordinance 126685, is amended as follows:

1 **23.51B.002 Public schools in residential zones**

2 \* \* \*

3 C. Lot (~~Coverage~~) coverage in Neighborhood Residential (~~Zones~~) zones

4 1. For new public school construction on new public school sites, the maximum  
5 lot coverage permitted for all structures is (~~45 percent of the lot area for one-story structures or~~  
6 ~~35 percent of the lot area if any structure or portion of a structure has more than one-story~~) as  
7 provided in Section 23.44.080.

8 2. For new public school construction and additions to existing public school  
9 structures on existing public school sites, the maximum lot coverage permitted is the greater of  
10 the following:

11 a. The lot coverage (~~permitted in subsection 23.51B.002.C.1~~) provided in  
12 Section 23.44.080; or

13 b. The lot coverage of the former school structures on the site, provided  
14 that the height of the new structure or portion of structure is no greater than that of the former  
15 structures when measured according to (~~Section 23.86.006.F~~) subsection 23.86.006.E, and at  
16 least 50 percent of the footprint of the new principal structure is constructed on a portion of the  
17 lot formerly occupied by the footprint of the former principal structure.

18 3. Departures from lot coverage limits may be granted or required pursuant to the  
19 procedures and criteria set forth in Chapter 23.79. (~~Up to 55 percent lot coverage may be~~  
20 ~~allowed for single-story structures, and up to 45 percent lot coverage for structures of more than~~  
21 ~~one-story.~~) Lot coverage restrictions may be waived by the Director as a Type I decision when  
22 waiver would contribute to reduced demolition of residential structures.

23 (~~4. The exceptions to lot coverage set forth in subsection 23.44.010.D apply.~~)

1 D. Height

2 1. Neighborhood Residential and (~~(Lowrise Zones)~~) lowrise zones

3 a. For new public school construction on new public school sites, the  
4 maximum permitted height is (~~(30)~~) 32 feet plus 5 feet for a pitched roof. For gymnasiums and  
5 auditoriums that are accessory to the public school, the maximum permitted height is 35 feet plus  
6 10 feet for a pitched roof if all portions of the structure above 30 feet are set back at least 20 feet  
7 from all lot lines. All parts of a pitched roof above the height limit must be pitched at a rate of  
8 not less than 4:12. No portion of a shed roof on a gymnasium or auditorium is permitted to  
9 extend above the 35-foot height limit under this (~~(provision)~~) subsection 23.51B.002.D.1.a.

10 b. For new public school construction on existing public school sites, the  
11 maximum permitted height is 35 feet plus 15 feet for a pitched roof. All parts of the roof above  
12 the height limit must be pitched at a rate of not less than 4:12. No portion of a shed roof is  
13 permitted to extend beyond the 35-foot height limit under this (~~(provision)~~) subsection  
14 23.51B.002.D.1.b.

15 c. For additions to existing public schools on existing public school sites,  
16 the maximum height permitted is the height of the existing school or 35 feet plus 15 feet for a  
17 pitched roof, whichever is greater. When the height limit is 35 feet, the ridge of the pitched roof  
18 on a principal structure may extend up to 15 feet above the height limit, and all parts of the roof  
19 above the height limit must be pitched at a rate of not less than 4:12. No portion of a shed roof is  
20 permitted to extend beyond the 35-foot limit under this (~~(provision)~~) subsection  
21 23.51B.002.D.1.c.

1                   2. Midrise and (~~(Highrise Zones)~~) highrise zones. The maximum permitted height  
2 for any public school located in a MR or HR zone is the base height permitted in that zone for  
3 multifamily structures.

4                   3. In (~~(Lowrise)~~) lowrise zones, departures from height limits may be granted or  
5 required pursuant to the procedures and criteria set forth in Chapter 23.79. For construction of  
6 new structures on new and existing public school sites to the extent not otherwise permitted  
7 outright, the maximum height that may be granted as a development standard departure is 35 feet  
8 plus 15 feet for a roof pitched at a rate of not less than 4:12 for elementary schools and 60 feet  
9 plus 15 feet for a roof pitched at a rate of not less than 4:12 for secondary schools. No departures  
10 may be granted for a portion of a shed roof to extend beyond 35 feet in height under this  
11 (~~(provision)~~) subsection 23.51B.002.D.3.

12                   4. Height maximums in all residential zones may be waived by the Director as a  
13 Type I decision when the waiver would contribute to reduced demolition of residential  
14 structures.

15                   5. The provisions of subsection (~~(B of Section 23.44.012)~~) 23.44.070.B and the  
16 exemptions of subsection (~~(C of Section 23.44.012)~~) 23.44.070.C apply.

17                   6. Light (~~(Standards)~~) standards

18                   a. Light standards for illumination of athletic fields on new and existing  
19 public school sites may be allowed to exceed the maximum permitted height, up to a maximum  
20 height of 100 feet, if the Director determines that the additional height is necessary to ensure  
21 adequate illumination and that impacts from light and glare are minimized to the greatest extent  
22 practicable. The applicant must submit an engineer's report demonstrating that impacts from light  
23 and glare are minimized to the greatest extent practicable. When proposed light standards are

1 reviewed as part of a project being reviewed pursuant to Chapter 25.05, (~~Environmental Policies~~  
2 ~~and Procedures,~~) and requiring a SEPA determination, the applicant must demonstrate that the  
3 additional height contributes to a reduction in impacts from light and glare.

4 b. When proposed light standards are not included in a proposal being  
5 reviewed pursuant to Chapter 25.05, the Director may permit the additional height as a special  
6 exception subject to Chapter 23.76(~~Procedures for Master Use Permits and Council Land Use~~  
7 ~~Decisions~~)).

8 1) When seeking a special exception for taller light standards, the  
9 applicant must submit an engineer's report demonstrating that the additional height contributes to  
10 a reduction in impacts from light and glare. When the proposal will result in extending the  
11 lighted area's duration of use, the applicant must address and mitigate potential impacts,  
12 including but not limited to, increased duration of noise, traffic, and parking demand. The  
13 applicant also shall conduct a public workshop for residents within 1/8 (~~of a~~) mile of the  
14 affected school in order to solicit comments and suggestions on design as well as potential  
15 impacts.

16 2) The Director may condition a special exception to address  
17 negative impacts from light and glare on surrounding areas, and conditions may also be imposed  
18 to address other impacts associated with increased field use due to the addition of lights,  
19 including, but not limited to, increased noise, traffic, and parking demand.

## 20 E. Setbacks

### 21 1. General requirements

22 a. No setbacks are required for new public school construction or for  
23 additions to existing public school structures for that portion of the site across a street or an alley

1 or abutting a lot in a nonresidential zone. If any portion of the site is across a street or an alley  
2 from or abuts a lot in a residential zone, setbacks are required for areas facing or abutting  
3 residential zones, as provided in subsections ~~((E.2 through E.5 of this Section 23.51B.002))~~  
4 23.51B.002.E.2 through 23.51B.002.E.5. Setbacks for sites across a street or alley from or  
5 abutting lots in Residential-Commercial (RC) zones are based upon the residential zone  
6 classification of the RC lot.

7                   b. The minimum setback requirement may be averaged along the structure  
8 facade with absolute minimums for areas abutting lots in residential zones as provided in  
9 subsections ~~((E.2.b, E.3.b and E.4.b of this Section 23.51B.002))~~ 23.51B.002.E.2.b,  
10 23.51B.002.E.3.b, and 23.51B.002.E.4.b.

11                   c. Trash disposals, operable windows in a gymnasium, main entrances,  
12 play equipment, kitchen ventilators, or other similar items shall be located at least 30 feet from  
13 any ~~((neighborhood residential))~~ Neighborhood Residential zoned lot and 20 feet from any multi-  
14 family zoned lot.

15                   d. The exceptions of subsections ~~((23.44.014.C.5, 23.44.014.C.6,  
16 23.44.014.C.7, 23.44.014.C.8, 23.44.014.C.9, 23.44.014.C.10, 23.44.014.C.11, and  
17 23.44.014.C.12)) 23.44.090.D, 23.44.090.E, 23.44.090.G, 23.44.090.H, and 23.44.090.I apply.~~

18                   2. New public school construction on new public school sites~~((:))~~

19                   a. New public school construction on new public school sites across a  
20 street or alley from lots in residential zones shall provide minimum setbacks according to the  
21 height of the school and the designation of the facing residential zone, as shown in Table A for  
22 23.51B.002~~((:))~~ .

1 ~~((Table A for 23.51B.002: Minimum Setbacks for a New Public School Site Located Across~~  
 2 ~~a Street or Alley from a Residential Zone))~~

<u>Table A for 23.51B.002</u> <u>Average setbacks for a new public school site located across a street or alley from a residential zone (in feet)</u>				
<del>((Minimum Setbacks Across a Street or Alley from the Following Zones (in feet):))</del> <u>Zone across street or alley and average setback</u>				
<del>((Height))</del> <u>Facade height</u>	<del>((NR/L1))</del> <u>NR/LR1</u>	<u>LR2/LR3</u>	<u>MR</u>	<u>HR</u>
<del>((Average))</del>				
20 or less	15	10	5	0
Greater than 20 up to 35	15	10	5	0
Greater than 35 up to 50	20	15	5	0
Greater than 50	35	20	10	0

3  
 4 b. New public school construction on new public school sites abutting lots  
 5 in residential zones shall provide minimum setbacks according to the height of the school and the  
 6 designation of the abutting residential zone, as shown in Table B for 23.51B.002((:)) .

7 ~~((Table B for 23.51B.002: Minimum Setbacks for a New Public School Site Abutting a~~  
 8 ~~Residential Zone))~~

<u>Table B for 23.51B.002</u> <u>Setbacks for a new public school site abutting a residential zone (in feet)</u>				
<del>((Minimum Setbacks Abutting the Following Zones (in feet):))</del> <u>Abutting zone and setbacks</u>				
<del>((Height))</del> <u>Facade height</u>	<u>NR/LR1</u>	<u>LR2/LR3</u>	<u>MR</u>	<u>HR</u>
<del>((Average (minimum)))</del>				
20 or less	20(10)	15(10)	10(5)	0(0)
Greater than 20 up to 35	25(10)	15(10)	10(5)	0(0)
Greater than 35 up to 50	25(10)	20(10)	10(5)	0(0)
Greater than 50	30(15)	25(10)	15(5)	0(0)

Footnote to Table B for 23.51B.002  
Average setbacks are shown outside of the parentheses and minimum setbacks are shown in parentheses.

9



3. New public school construction on existing public school sites((:))

a. New public school construction on existing public school sites across a street or alley from lots in residential zones shall provide either the setback of the previous structure on the site or minimum setbacks according to the ((£)) height of the school and the designation of the facing residential zone as shown in Table C for 23.51B.002, whichever is less((:)) .

~~((Table C for 23.51B.002: Minimum Setbacks for New Construction on an Existing Public School Site Located Across a Street or Alley from a Residential Zone))~~

<u>Table C for 23.51B.002</u>				
<u>Setbacks for new construction on an existing public school site located across a street or alley from a residential zone (in feet)</u>				
	<del>((Minimum Setbacks If Across a Street or Alley from the Following Zones (in feet:))</del> <u>Zone across street or alley and average setback</u>			
<del>((Façade Height))</del> <u>Facade height</u>	NR/LR1	LR2/LR3	MR	HR
	<del>((Average))</del>			
20 or less	10	5	5	0
Greater than 20 up to 35	10	5	5	0
Greater than 35 up to 50	15	10	5	0
Greater than 50	20	15	10	0

b. New public school construction on existing public school sites abutting lots in residential zones shall provide either the setback of the previous structure on the site or minimum setbacks according to the height of the school and the designation of the abutting residential zone, as shown in Table D for 23.51B.002, whichever is less((:)) .

~~((Table D for 23.51B.002: Minimum Setbacks for New Construction on an Existing Public School Site Abutting a Residential Zone))~~

<b>Table D for 23.51B.002</b>				
<b><u>Setbacks for new construction on an existing public school site abutting a residential zone (in feet)</u></b>				
<b><del>((Minimum Setbacks Abutting the Following Zones (in feet:))</del> <u>Abutting zone and setback</u></b>				
<b><del>((Façade Height))</del> <u>Facade height</u></b>	<b>NR/LR1</b>	<b>LR2/LR3</b>	<b>MR</b>	<b>HR</b>
<b><del>((Average (minimum)))</del></b>				
20 or less	15(10)	10(5)	10(5)	0(0)
Greater than 20 up to 35	20(10)	15(10)	10(5)	0(0)
Greater than 35 up to 50	25(10)	20(10)	10(5)	0(0)
Greater than 50	30(15)	25(10)	15(5)	0(0)

Footnote to Table D for 23.51B.002  
Average setbacks are shown outside of the parentheses and minimum setbacks are shown in parentheses.

1                   4. Additions to ~~((Existing Public School Structures))~~ existing public school  
 2 structures on ~~((Existing Public School Sites.))~~ existing public school sites  
 3                   a. Additions to existing public school structures on existing public school  
 4 sites across a street or alley from lots in residential zones shall provide either the setback of the  
 5 previous structure on the site or minimum setbacks according to the height of the school and the  
 6 designation of the facing residential zone as shown in Table E for 23.51B.002, whichever is  
 7 less((:)) .  
 8 ~~((Table E for 23.51B.002: Minimum Setbacks for Additions on an Existing Public School~~  
 9 ~~Site Located Across a Street or Alley))~~

<b>Table E for 23.51B.002</b>				
<b><u>Setbacks for additions on an existing public school site located across a street or alley from a residential zone (in feet)</u></b>				
<b><del>((Minimum Setbacks (in feet) If Located Across a Street or Alley from:))</del> <u>Zone across street or alley and average setback</u></b>				
<b><del>((Façade Height))</del> <u>Facade height</u></b>	<b>NR/LR1</b>	<b>LR2/LR3</b>	<b>MR</b>	<b>HR</b>
<b><del>((Average))</del></b>				
20 or less	5	5	5	0
Greater than 20 up to 35	10	5	5	0

Greater than 35 up to 50	15	10	5	0
Greater than 50	20	15	10	0

1                                    b. Additions to public schools on existing public school sites abutting lots  
 2 in residential zones shall provide either the setback of the previous structure on the site or  
 3 minimum setbacks according to the height of the school and the designation of the abutting  
 4 residential zone as shown in Table F for 23.51B.002, whichever is less((:)) .

5 ~~((Table F for 23.51B.002: Minimum Setbacks for Additions on an Existing Public School  
 6 Site Abutting a Residential Zone))~~

<b>Table F for 23.51B.002</b>				
<b>Setbacks for additions on an existing public school site abutting a residential zone (in feet)</b>				
	<del>((Minimum Setbacks by Abutting Zone (in feet):))</del>			
	<b>Abutting zone and setback</b>			
<del>((Façade Height))</del> <b>Facade height</b>	<b>NR/LR1</b>	<b>LR2/LR3</b>	<b>MR</b>	<b>HR</b>
	<del>((Average (minimum)))</del>			
20 or less	10(5)	10(5)	10(5)	0(0)
Greater than 20 up to 35	15(5)	10(5)	10(5)	0(0)
Greater than 35 up to 50	20(10)	20(10)	10(5)	0(0)
Greater than 50	25(10)	25(10)	15(5)	0(0)
<u>Footnote to Table F for 23.51B.002</u>				
<u>Average setbacks are shown outside of the parentheses and minimum setbacks are shown in parentheses.</u>				

7                                    5. Departures from setback requirements may be granted or required pursuant to  
 8 the procedures and criteria set forth in Chapter 23.79 as follows:

9                                    a. The minimum average setback may be reduced to 10 feet and the  
 10 minimum setback to 5 feet for structures or portions of structures across a street or alley from  
 11 lots in residential zones.

12                                    b. The minimum average setback may be reduced to 15 feet and the  
 13 minimum setback to 5 feet for structures or portions of structures abutting lots in residential  
 14 zones.

1 c. The limits in subsections (~~E.5.a and E.5.b of this Section 23.51B.002~~)  
2 23.51B.002.E.5.a and 23.51B.002.E.5.b may be waived by the Director if a waiver would  
3 contribute to reduced demolition of residential structures.

4 F. Structure (~~Width~~) width

5 1. When a new public school structure is built on a new public school site or on an  
6 existing public school site, the maximum width of a structure is 66 feet unless either the  
7 modulation option in subsection 23.51B.002.F.1.a (~~below~~) or the landscape option in  
8 subsection 23.51B.002.F.1.b (~~below~~) is met.

9 a. Modulation (~~Option~~) option. Facades shall be modulated according to  
10 the following provisions:

- 11 1) The minimum depth of modulation is 4 feet.  
12 2) The minimum width of modulation is 20 percent of the total  
13 structure width or 10 feet, whichever is greater.

14 b. Landscape (~~Option~~) option. The (~~yards provided by the required~~)  
15 setbacks shall be landscaped as follows:

16 1) One tree and three shrubs are required for each 300 square feet  
17 of (~~required yard~~) setback area.

18 2) Trees and shrubs that already exist in the required planting area  
19 or have their trunk or center within 10 feet of the area may be substituted for required plantings  
20 on a one-tree-to-one-tree or one-shrub-(~~-~~)to-one-shrub basis. In order to qualify, a tree must be  
21 6 inches or greater in diameter, measured 4.5 feet above the ground.



1                   2. On any portion of the lot except the front setback, provided that the parking is  
2 separated from streets and from abutting lots in residential zones by an area with a minimum  
3 depth of 5 feet that is landscaped with trees and ground cover determined by the Director, as a  
4 Type I decision, as adequate to soften the view of the parking from adjacent properties. In the  
5 case of a through lot, parking may also be located in one front setback when landscaped as  
6 described in this subsection 23.51B.002.H.2;

7                   3. Departures may be granted or required pursuant to the procedures set forth in  
8 Chapter 23.79 to permit parking location anywhere on the lot and to reduce required landscaping.  
9 Landscaping may be waived in whole or in part if the topography of the site or other  
10 circumstances result in the purposes of landscaping being served, as, for example, when a steep  
11 slope shields parking from the view of abutting properties. This test may be waived by the  
12 Director, as a Type I decision, when waiver would contribute to reduced demolition of  
13 residential structures.

14                   I. Bus and ~~((Truck Loading))~~ truck loading and ~~((Unloading))~~ unloading

15                   1. Unless subsection ~~((I.4 of this section 23.51B.002))~~ 23.51B.002.I.4 applies, an  
16 off-street bus loading and unloading area of a size reasonable to meet the needs of the school  
17 shall be provided and may be located in any required ~~((yard))~~ setback. The bus loading and  
18 unloading area may be permitted in landscaped areas provided under subsection  
19 23.51B.002.F.1.b if the Director determines that landscaping around the loading and unloading  
20 area softens the impacts of its appearance on abutting properties.

21                   2. One off-street truck loading berth that is 13 feet wide and 40 feet long is  
22 required for new public school construction.

1                   3. Departures from the requirements and standards for bus and truck loading and  
2 unloading areas and berths may be granted or required pursuant to the procedures and criteria set  
3 forth in Chapter 23.79 only when departure would contribute to reduced demolition of residential  
4 structures.

5                   4. When a public school is remodeled or rebuilt at the same site, an existing on-  
6 street bus loading area is allowed if the following conditions are met:

- 7                               a. The school site is not proposed to be expanded;
- 8                               b. The student capacity of the school is not being expanded by more than  
9 25 percent; and
- 10                              c. The location of the current on-street bus loading remains the same.

11                   J. Noise, (~~Odor, Light~~) odor, light, and (~~Glare~~) glare. The development standards for  
12 small institutions set forth in Section 23.45.570 apply. Departures from these standards may be  
13 granted or required pursuant to the procedures and criteria set forth in Chapter 23.79 only when  
14 departure would contribute to reduced demolition of residential structures.

15                   Section 57. Section 23.53.006 of the Seattle Municipal Code, last amended by Ordinance  
16 127099, is amended as follows:

17 **23.53.006 Pedestrian access and circulation**

18   \* \* \*

19                   F. Exceptions. The following exceptions to pedestrian access and circulation  
20 requirements and standards apply:

21                               1. Projects exempt from requirements. Pedestrian access and circulation  
22 improvements are not required for the following types of projects:

- 23                                       a. Change of use;

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b. Alterations to existing structures;

c. Additions to existing structures that are exempt from environmental review;

d. Construction of a detached structure that does not contain a dwelling unit and is accessory to ((a single family)) an existing dwelling unit in any zone, if the property owner enters into a no-protest agreement, as authorized by chapter 35.43 RCW, to future pedestrian access and circulation improvements and that agreement is recorded with the King County ((Recorder)) Recorder's Office;

e. Construction of ((a single family)) one dwelling unit on a lot in any zone, if the property owner enters into a no-protest agreement, as authorized by chapter 35.43 RCW, to future pedestrian access and circulation improvements and that agreement is recorded with the King County ((Recorder)) Recorder's Office, and if at least one of the following conditions is met:

1) The lot is on a block front where there are no existing pedestrian access and circulation improvements within 100 feet of the lot; or

2) Construction of pedestrian access and circulation improvements is not necessary because, for example, the existing right-of-way has suitable width and surface treatment for pedestrian use; or the existing right-of-way has a limited amount of existing and potential vehicular traffic; or the Director anticipates limited, if any, additional development near the lot because the development near the lot is at or near zoned capacity under current zoning designations;

f. Construction of accessory dwelling units;



1    ((f)) g. Expansions of surface parking, outdoor storage, outdoor sales and  
2 outdoor display of rental equipment of less than 20 percent of the parking, storage, sales or  
3 display area, or number of parking spaces;

4    ((g)) h. In the MML zone, the addition of:

5    1) Fewer than ten artist's studio dwellings;  
6    2) Less than 750 square feet of gross floor area of major and minor  
7 vehicle repair uses and multipurpose retail sales; ~~((and))~~ or

8    3) Less than 4,000 square feet of gross floor area of ~~((non-~~  
9 ~~residential))~~ nonresidential uses not listed in subsection ~~((23.53.006.F.1.g.2))~~ 23.53.006.F.1.h.2;  
10 and

11    ~~((h))~~ i. Construction of a new ~~((non-residential))~~ nonresidential structure  
12 of up to 4,000 square feet of gross floor area if the structure is at least 50 feet from any lot line  
13 abutting an existing street that does not have pedestrian access and circulation improvements.

14    2. Waiver or modification of pedestrian access and circulation requirements. The  
15 Director, in consultation with the Director of Transportation, may waive or modify pedestrian  
16 access and circulation requirements when one or more of the following conditions are met. The  
17 waiver or modification shall provide the minimum relief necessary to accommodate site  
18 conditions while maximizing pedestrian access and circulation.

19    a. Location in an environmentally critical area or buffer makes installation  
20 of a sidewalk, curb, and/or curb ramp structurally impracticable or technically infeasible;

21    b. The existence of a bridge, viaduct, or structure such as a substantial  
22 retaining wall in proximity to the project site makes installation of a sidewalk, curb, and/or curb  
23 ramp structurally impracticable or technically infeasible;

1 c. Sidewalk, curb, and/or curb ramp construction would result in  
2 undesirable disruption of existing drainage patterns, or disturbance to or removal of natural  
3 features such as significant trees or other valuable and character-defining mature vegetation; or

4 d. Sidewalk, curb, and/or curb ramp construction would preclude vehicular  
5 access to the lot, for example on project sites where topography would render driveway access in  
6 excess of the maximum 15 percent slope.

7 3. Notwithstanding any provision of Section 23.76.026, the applicant for a Master  
8 Use Permit or a building permit to which ~~((the Land Use Code))~~ Title 23 in effect prior to  
9 October 30, 2009, applies may, by written election, use the exemptions in subsections  
10 23.53.006.F.1 and 23.53.006.F.2.

11 Section 58. Section 23.53.025 of the Seattle Municipal Code, last amended by Ordinance  
12 126682, is amended as follows:

13 **23.53.025 Access easement standards**

14 If access by easement has been approved by the Director, the easement shall meet the following  
15 standards. Surfacing of easements, pedestrian walkways required within easements, and  
16 turnaround dimensions shall meet the requirements of the Right-of-Way Improvements Manual.

17 A. Vehicle access easements serving one or two ~~((single-family))~~ dwelling units ~~((or one~~  
18 ~~multifamily residential use with a maximum of two units))~~ shall meet the following standards:

19 1. Easement width shall be a minimum of 10 feet.

20 2. No maximum easement length shall be set. If easement length is more than 150  
21 feet, a vehicle turnaround shall be provided.

22 3. ~~((Curbcut))~~ Curb cut width from the easement to the street shall be the  
23 minimum necessary for safety and access.

1 B. Vehicle access easements serving at least three but fewer than ~~((five single-family))~~  
2 ten dwelling units shall meet the following standards:

- 3 1. Easement width shall be a minimum of 10 feet.
- 4 2. The easement shall provide a hard-surfaced roadway at least 10 feet wide.
- 5 3. No maximum easement length shall be set. If the easement is over 600 feet  
6 long, a fire hydrant may be required by the Director.
- 7 4. A turnaround shall be provided unless the easement extends from street to  
8 street.
- 9 5. ~~((Curbcut))~~ Curb cut width from the easement to the street shall be the  
10 minimum necessary for safety and access.

11 C. ~~((Vehicle access easements serving at least five but fewer than ten single-family  
12 dwelling units, or at least three but fewer than ten multifamily dwelling units~~

- 13 1. ~~Easement width, surfaced width, length, turn around, and curbcut width shall  
14 be as required in subsection 23.53.025.B.~~
- 15 2. ~~No single family structure shall be closer than 5 feet to the easement, except  
16 that structural features allowed to extend into required yards under subsection 23.44.014.C.6 are  
17 also allowed to extend into the 5-foot setback from an easement.~~

18 ~~D.))~~ Vehicle ~~((Access Easements Serving Ten))~~ access easements serving ten or more  
19 ~~((Residential Units.))~~ dwelling units shall meet the following standards:

- 20 1. Easement width shall be a minimum of 32 feet~~((;))~~ .
- 21 2. The easement shall provide a surfaced roadway at least 24 feet wide, except in  
22 the MPC-YT zone, where the minimum surfaced roadway width is 20 feet~~((;))~~ .

1 3. No maximum length shall be set. If the easement is over 600 feet long, a fire  
2 hydrant may be required by the Director((;)) .

3 4. A turnaround shall be provided unless the easement extends from street to  
4 street((;)) .

5 5. ((~~Curbcut~~)) Curb cut width from the easement to the street shall be the  
6 minimum necessary for safety access((;)) .

7 6. No ((~~single-family structure;~~)) detached dwelling unit shall be located closer  
8 than ((10)) 5 feet to an easement, except that architectural features such as cornices, eaves,  
9 gutters, roofs, fireplaces, chimneys, and other similar features shall not be located closer than 3  
10 feet to a required easement.

11 7. One pedestrian walkway shall be provided, extending the length of the  
12 easement.

13 ((~~E. Vehicle Access Easements Serving Nonresidential or Live-work Uses.~~

14 ~~4.))~~ D. For nonresidential or live-work uses providing fewer than ten ((10)) parking  
15 spaces, the easement shall meet the requirements of subsection ((C)) 23.53.025.B.

16 ((2)) E. For nonresidential or live-work uses providing ten ((10)) or more parking  
17 spaces, the easement shall meet the requirements of subsection ((D)) 23.53.025.C.

18 F. Pedestrian ((~~Access Easements~~)) access easements. Where a lot proposed for a  
19 residential use abuts an alley but does not abut a street and the provisions of the zone require  
20 access by vehicles from the alley, or where the alley access is an exercised option, an easement  
21 providing pedestrian access to a street from the lot shall be provided meeting the following  
22 standards:

23 1. Easement width shall be a minimum of ((five-)) 5 ((+)) feet;

1                   2. Easements serving one (~~((1))~~) or two (~~((2))~~) dwelling units shall provide a  
2 paved pedestrian walkway at least (~~((three-))~~) 3 (~~((3))~~) feet wide;

3                   3. Easements serving three (~~((3))~~) or more dwelling units shall provide a paved  
4 pedestrian walkway at least (~~((five-))~~) 5 (~~((3))~~) feet wide;

5                   4. Easements over (~~((one hundred-))~~) 100 (~~((3))~~) feet in length shall provide lighting  
6 at intervals not to exceed (~~((fifty-))~~) 50 (~~((3))~~) feet. Lighting placement shall not exceed (~~((fifteen-))~~)  
7 15 (~~((3))~~) feet in height;

8                   5. Pedestrian access easements shall not exceed (~~((two hundred-))~~) 200 (~~((3))~~) feet in  
9 length.

10                  G. Vertical (~~((Clearance Above Easements))~~) clearance above easements. When an  
11 easement serves fewer than ten (~~((10))~~) residential units and crosses a residentially zoned lot,  
12 portions of structures may be built over the easement provided that a minimum vertical clearance  
13 of (~~((sixteen and one half (16 1/2))~~) 16.5 feet is maintained above the surface of the easement  
14 roadway and a minimum turning path radius in accordance with (~~((Section 23.54.030 C))~~)  
15 subsection 23.54.030.D is maintained. (~~((See))~~) Exhibit (~~((23.53.025 A))~~) A for 23.53.025.

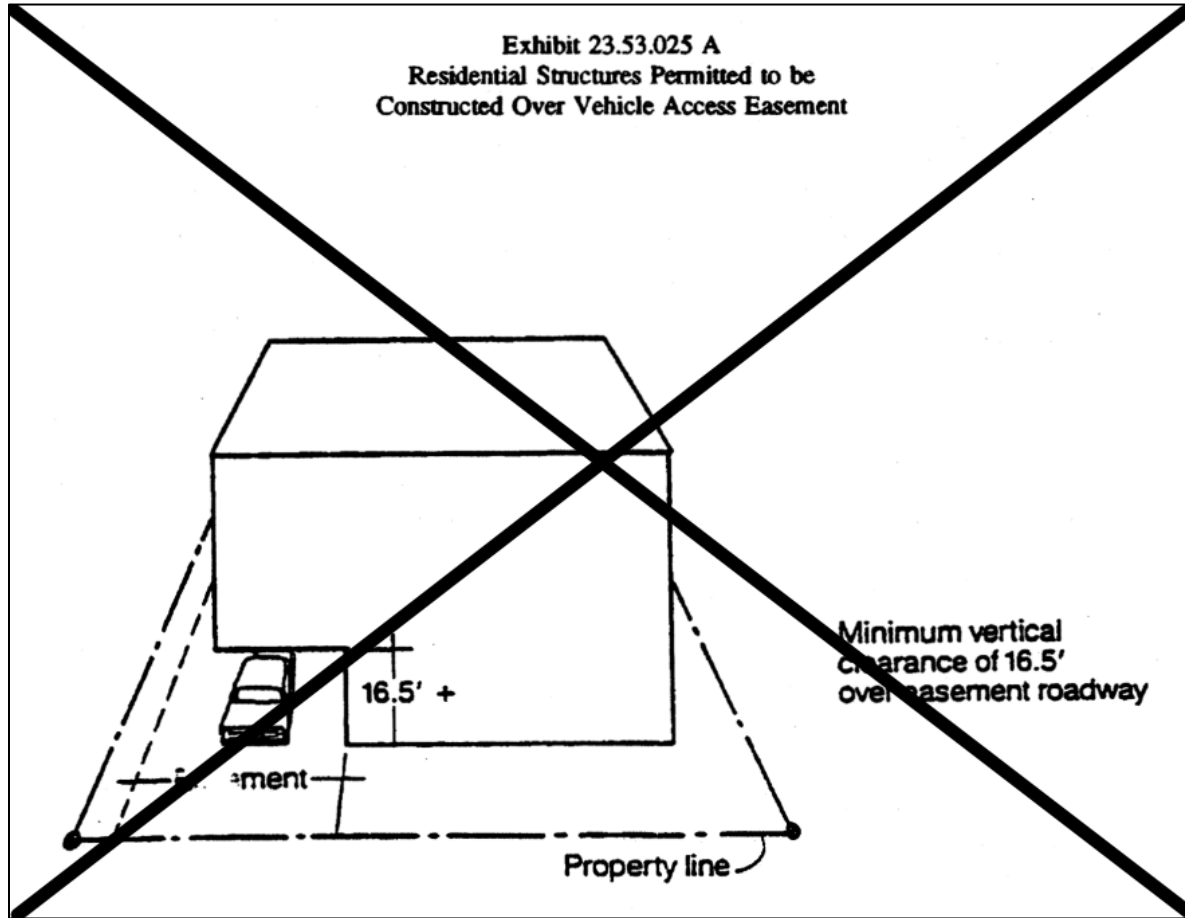
16                  H. Exceptions (~~((From Access Easement Standards))~~) from access easement standards. The  
17 Director, in consultation with the Fire Chief, may modify the requirements for easement width  
18 and surfacing for properties located in environmentally critical areas or their buffers when it is  
19 determined that:

20                   1. Such modification(s) would reduce adverse effects to identified  
21 environmentally critical areas or buffers; and

22                   2. Adequate access and provisions for fire protection can be provided for  
23 structures served by the easement.

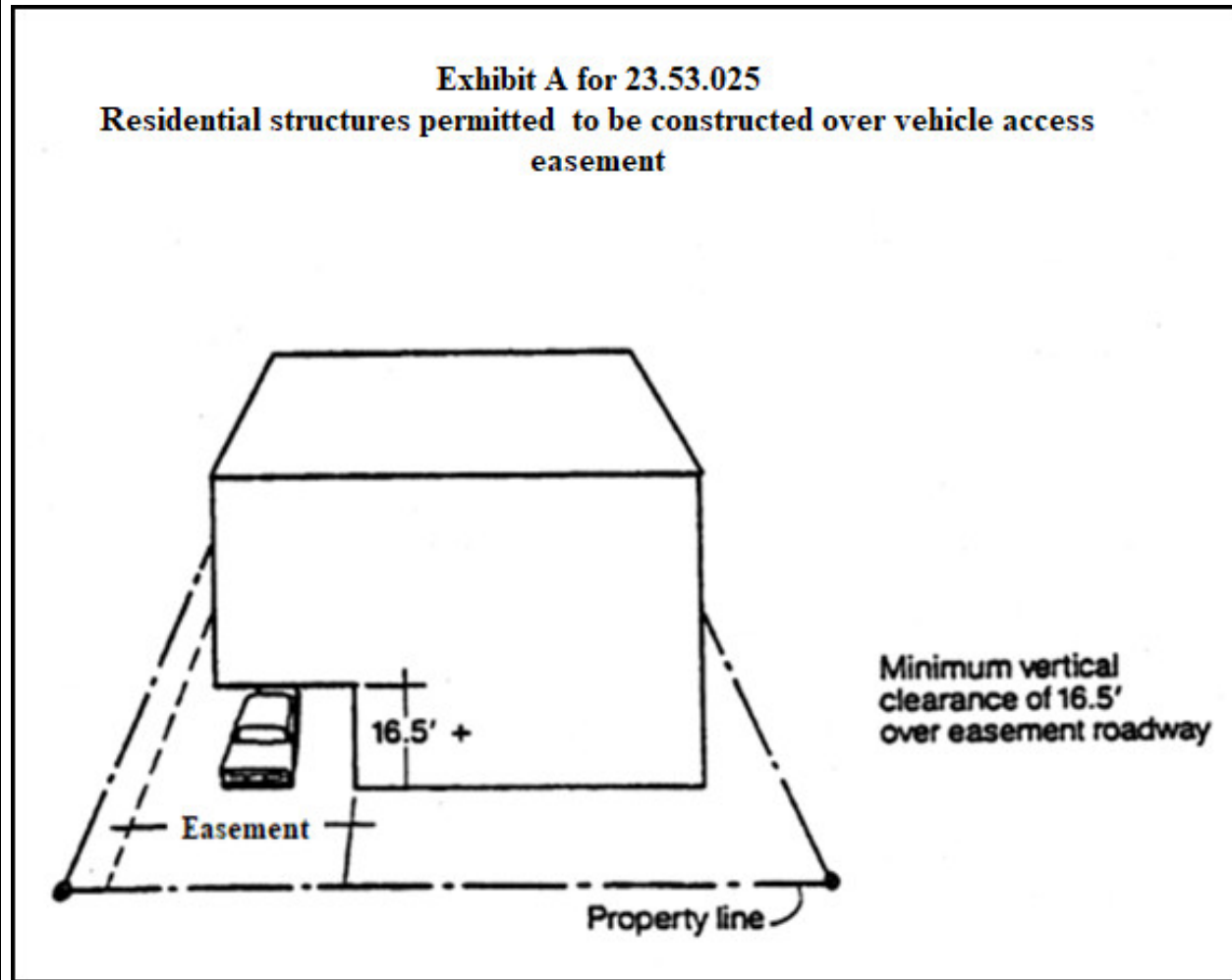
1 **Exhibit A for 23.53.025**

2 **Residential structures permitted to be constructed over vehicle access easement**



3

4



1  
2 Section 59. Section 23.54.015 of the Seattle Municipal Code, last amended by Ordinance  
3 ~~127099~~ [127228](#), is amended as follows:

4 **23.54.015 Required vehicular parking and maximum vehicular parking limits**

5 A. Required parking. The minimum number of off-street motor vehicle parking spaces  
6 required for specific uses is set forth in Table A for 23.54.015 for ((~~non-residential~~))  
7 nonresidential uses other than institutional uses, Table B for 23.54.015 for residential uses, and  
8 Table C for 23.54.015 for institutional uses, except as otherwise provided in this Chapter 23.54.  
9 Required parking is based upon gross floor area of a use within a structure minus gross floor area  
10 in parking uses, and the square footage of a use when located outside of an enclosed structure, or  
11 as otherwise specified. Maximum parking limits for specific uses and specific areas are set forth

1 in subsection 23.54.015.C. Exceptions to motor vehicle parking requirements set forth in this  
2 Section 23.54.015 are provided in((:)) subsections 23.54.015.B and 23.54.015.C((:)) and in  
3 Section 23.54.020 ((~~unless otherwise specified~~)). This Chapter 23.54 does not apply to parking  
4 for construction activity, which is regulated by Section 23.42.044.

5 B. Required parking for specific zones and areas

6 1. Parking in downtown zones is regulated by Chapters 23.49 and 23.66, and not  
7 by this Section 23.54.015.

8 2. Parking in the MPC-YT zone is regulated by Section 23.75.180 and not by this  
9 Section 23.54.015.

10 3. Parking for major institution uses in the Major Institution Overlay District is  
11 regulated by Sections 23.54.015 and 23.54.016.

12 4. The Director shall adopt by rule a map of frequent transit and major transit  
13 service areas based on proximity to a transit station or stop served by a frequent transit route or a  
14 major transit service. The determination whether a proposed development site is in a scheduled  
15 frequent transit or major transit service area shall be based on the ((~~frequent transit service area~~))  
16 map adopted by rule that exists on the date a project vests according to the standards of Section  
17 23.76.026, provided that a rule that takes effect on a date after the project vests may be applied to  
18 determine whether the site is in a scheduled frequent transit or major transit service area, at the  
19 election of the project applicant in accordance with subsection ~~23.76.026.E~~ 23.76.026.F.

20 C. Maximum parking limits for specific zones or areas

21 1. In the Stadium Transition Area Overlay District certain uses are subject to a  
22 maximum parking ratio pursuant to subsection 23.74.010.A.1.b. When there are multiple uses on



1 a lot, the total parking requirement for all uses subject to a maximum ratio cannot exceed the  
2 aggregate maximum for those uses under Section 23.74.010.

3 2. In all commercial zones, except C2 zones outside of urban villages, no more  
4 than 145 spaces per lot may be provided as surface parking or as flexible-use parking.

5 3. In all Neighborhood Residential and multifamily zones, commercial uses are  
6 limited to no more than ten parking spaces per business establishment.

7 4. In the Northgate Overlay District, the Director may permit parking to exceed  
8 applicable maximum parking limits as a Type I decision pursuant to Chapter 23.76 if:

9 a. The parking is provided in a structure according to a joint-use parking  
10 agreement with King County Metro Transit; and

11 b. It can be demonstrated to the satisfaction of the Director through a  
12 parking demand study that the spaces are only needed to meet evening and weekend demand or  
13 as overflow on less than ten percent of the weekdays in a year, and the spaces shall otherwise be  
14 available for daytime use by the general public.

15 5. Notwithstanding the minimum parking requirements set out in Table A for  
16 23.54.015, in the Industry and Innovation zones, the maximum parking ratio for all uses is one  
17 space per 1,000 square feet of gross floor area.

18 D. Parking waivers (~~for non-residential uses~~)

19 1. (~~In all commercial zones, no~~) No parking is required for the first ~~((1,500))~~  
20 3,000 square feet of each business establishment (~~(or the first 15 fixed seats for motion picture~~  
21 ~~and performing arts theaters)~~)).

22 2. (~~In all other zones, no parking is required for the first 2,500 square feet of~~  
23 ~~gross floor area of non-residential uses in a structure, except for the following:~~

1 ~~a. Structures or portions of structures occupied by restaurants with drive-in~~  
2 ~~lanes,~~

3 ~~b. Motion picture theaters,~~

4 ~~c. Offices, or~~

5 ~~d. Institution uses, including Major Institution uses.~~

6 ~~When two or more uses with different parking ratios occupy a structure, the 2,500~~  
7 ~~square foot waiver is prorated based on the area occupied by the non-residential uses for which~~  
8 ~~the parking waiver is permitted.)) No parking is required for ground level nonresidential spaces~~  
9 ~~in mixed-use structures.~~

10 E. Fleet vehicles. Notwithstanding any other provisions of this ~~((section))~~ Section  
11 23.54.015, off-street parking shall be provided for all fleet vehicles and those parking spaces will  
12 not be counted toward the parking requirements of Table A for 23.54.015, Table B for  
13 23.54.015, or Table C for 23.54.015.

14 F. Use and reuse of schools. For non-school uses permitted to locate in a former or  
15 existing public school, parking requirements will be determined by school use pursuant to  
16 criteria adopted according to Chapter 23.78~~((, Establishment of Criteria for Joint Use or Reuse of~~  
17 ~~Schools))~~.

18 G. Changes of use:

19 1. New ~~((non-residential))~~ nonresidential uses in existing structures ~~((in~~  
20 ~~commercial and industrial zones))~~. ~~((Up))~~ Except as otherwise provided in this subsection  
21 23.54.015.G, up to 20 required parking spaces are waived for a new ~~((non-residential))~~  
22 nonresidential use established in an existing structure or the expansion of an existing ~~((non-~~

1 ~~residential~~) nonresidential use entirely within an existing structure. Existing required parking  
2 shall remain.

3 2. Residential uses in existing structures. No parking is required for a change of  
4 use from a nonresidential use to a residential use in an existing structure.

5 3. Commercial uses in existing structures. No parking is required for a change of  
6 use to a commercial use in an existing structure.

7 4. For purposes of this Section 23.54.015, "existing structure" means a structure  
8 that was established under permit, or for which a building permit has been granted and has not  
9 expired, at least two years prior to the application to establish the new use or expand the use.

10 5. Parking spaces required for loading and unloading of passengers are not  
11 eligible for the waiver under this subsection 23.54.015.G.

12 H. Uses not shown on parking tables. In the case of a use not shown on Table A for  
13 23.54.015, Table B for 23.54.015, or Table C for 23.54.015, the requirements for off-street  
14 parking will be determined by the Director based on the requirements for the most comparable  
15 use. Where, in the judgment of the Director, none of the uses on Table A for 23.54.015, Table B  
16 for 23.54.015, and Table C for 23.54.015 are comparable to a proposed use, the Director may  
17 base ~~((his or her))~~ a determination as to the amount of parking required for the proposed use on  
18 detailed information provided by the applicant. The information required may include, but not be  
19 limited to, a description of the physical structure(s), identification of potential users, and analysis  
20 of likely parking demand.

21 I. Uses in multiple parking table categories. If an entire use or structure, or the same  
22 portion of a use or structure, falls under more than one category in Table A for 23.54.015, Table  
23 B for 23.54.015, or Table C for 23.54.015 then, unless otherwise specified, the category

1 requiring the smallest number of parking spaces applies except as expressly set forth on such  
 2 tables.

3 J. Existing parking deficits. Existing legal parking deficits of legally established uses are  
 4 allowed to continue even if a change of use occurs. This subsection 23.54.015.J will not be  
 5 construed to permit a parking deficit caused by the failure to satisfy conditions of a reduced  
 6 parking requirement for any use or structure.

<b>Table A for 23.54.015</b>			
<b>Required parking for (<del>non-residential</del>) <u>nonresidential</u> uses other than institutions</b>			
<b>Use</b>		<b>Minimum parking required</b>	
<b>I. General (<del>non-residential</del>) <u>nonresidential</u> uses (other than institutions)</b>			
* * *			
<b>B.</b>	<b>COMMERCIAL USES</b>		
	B.1.	Animal shelters and kennels	1 space for each 2,000 square feet
	B.2.	Eating and drinking establishments	1 space for each ( <del>250</del> ) <u>500</u> square feet
	B.3.	Entertainment uses, general, except as noted below <sup>2</sup>	For public assembly areas: 1 space for each 8 fixed seats, or 1 space for each 100 square feet of public assembly area not containing fixed seats, or 1 space for each <u>500 square feet of total floor area in entertainment use, whichever is less</u>
		B.3.a.	Adult cabarets
		B.3.b.	Sports and recreation uses <sup>3</sup>
	B.4.	Food processing and craft work	1 space for each 2,000 square feet
	B.5.	Laboratories, research and development	1 space for each 1,500 square feet

**Table A for 23.54.015  
 Required parking for ((non-residential)) nonresidential uses other than institutions**

Use		Minimum parking required
B.6.	Lodging uses	1 space for each 4 rooms or 1 space for each 500 square feet of total floor area in lodging use, whichever is less; For bed and breakfast facilities in ((neighborhood residential)) <u>Neighborhood Residential</u> and multifamily zones, 1 space for each 2 dwelling units, plus 1 space for each 2 guest rooms, or 1 space for each 500 square feet of total floor area in lodging use, whichever is less
B.7.	Medical services	1 space for each 500 square feet
B.8.	Offices	1 space for each 1,000 square feet
B.9.	Sales and services, automotive	1 space for each 2,000 square feet
B.10.	Sales and services, general, except as noted below	1 space for each 500 square feet
	B.10.a. Pet daycare centers <sup>4</sup>	((1 space for each 10 animals or 1 space for each staff member, whichever is greater,)) 1 space for each 500 square feet ((plus 1 loading and unloading space for each 20 animals))
B.11.	Sales and services, heavy	1 space for each 2,000 square feet
B.12.	Sales and services, marine	1 space for each 2,000 square feet

\* \* \*

**II. ((Non-residential)) Nonresidential use requirements for specific areas**

I.	((Non-residential)) <u>Nonresidential</u> uses in urban centers or the Station Area Overlay District <sup>5</sup>	No minimum requirement
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**Table A for 23.54.015  
 Required parking for ((~~non-residential~~)) nonresidential uses other than institutions**

Use	Minimum parking required
J. (( <del>Non-residential</del> )) <u>Nonresidential</u> uses in urban villages that are not within an urban center or (( <del>the</del> )) a Station Area Overlay District, if the (( <del>non-residential</del> )) <u>nonresidential</u> use is located within a frequent transit service area <sup>5</sup>	No minimum requirement
K. (( <del>Non-residential</del> )) <u>Nonresidential</u> uses permitted in MR and HR zones pursuant to Section 23.45.504	No minimum requirement
L. (( <del>Non-residential</del> )) <u>Nonresidential</u> uses permitted in II zones	No minimum requirement

Footnotes for Table A for 23.54.015

<sup>1</sup> No parking is required for urban farms or community gardens in residential zones.

<sup>2</sup> Required parking for spectator sports facilities or exhibition halls must be available when the facility or exhibition hall is in use. A facility shall be considered to be "in use" during the period beginning three hours before an event is scheduled to begin and ending one hour after a scheduled event is expected to end. For sports events of variable or uncertain duration, the expected event length shall be the average length of the events of the same type for which the most recent data are available, provided it is within the past five years. During an inaugural season, or for nonrecurring events, the best available good faith estimate of event duration will be used. A facility will not be deemed to be "in use" by virtue of the fact that administrative or maintenance personnel are present. The Director may reduce the required parking for any event when projected attendance for a spectator sports facility is certified to be 50 percent or less of the facility's seating capacity, to an amount not less than that required for the certified projected attendance, at the rate of one space for each ten fixed seats of certified projected attendance. An application for reduction and the certification shall be submitted to the Director at least 15 days prior to the event. When the event is one of a series of similar events, such certification may be submitted for the entire series 15 days prior to the first event in the series. If the Director finds that a certification of projected attendance of 50 percent or less of the seating capacity is based on satisfactory evidence such as past attendance at similar events or advance ticket sales, the Director shall, within 15 days of such submittal, notify the facility operator that a reduced parking requirement has been approved, with any conditions deemed appropriate by the Director to ensure adequacy of parking if expected attendance should change. The parking requirement reduction may be applied for only if the goals of the facility's Transportation Management Plan are otherwise being met. The Director may revoke or modify a parking requirement reduction approval during a series, if projected attendance is exceeded.

**Table A for 23.54.015**  
**Required parking for ((non-residential)) nonresidential uses other than institutions**

Use	Minimum parking required
<p><sup>3</sup> For indoor sports and recreation uses that exceed 25,000 square feet in size in a Manufacturing Industrial Center, the minimum requirement is ((+)) <u>one</u> space for each 2,000 square feet.</p> <p><sup>4</sup> <u>Parking for pet daycare centers shall include at least one space for each 20 animals that is reserved for loading and unloading of animals, provided that a total of no more than one parking space per 500 square feet is required.</u> The ((<del>amount</del>)) <u>number</u> of required ((<del>parking</del>)) <u>loading and unloading spaces</u> is calculated based on the maximum number of ((<del>staff or</del>)) animals the center is designed to accommodate.</p> <p><sup>5</sup> The general minimum requirements of Part I of Table A for 23.54.015 are superseded to the extent that a use, structure, or development qualifies for either a greater or a lesser minimum parking requirement (which may include no requirement) under any other provision. To the extent that a ((non-residential)) <u>nonresidential</u> use fits within more than one line in Table A for 23.54.015, the least of the applicable minimum parking requirements applies. The different parking requirements listed for certain categories of ((non-residential)) <u>nonresidential</u> uses shall not be construed to create separate uses for purposes of any requirements related to establishing or changing a use under this Title 23.</p>	

1

**Table B for 23.54.015**  
**Required parking for residential uses**

Use	Minimum parking required
<b>I. General residential uses</b> <sup>1, 2, 3</sup>	
((A-))	<del>Adult family homes</del>
B-)) <u>A.</u>	1 space for each dwelling unit
((C-)) <u>B.</u>	<del>1 space for each 2 dwelling units</del> <del>1 space for each 4 assisted living units;</del> <del>plus</del> <del>1 space for each 2 staff members on site at peak staffing time; plus</del> <del>1 barrier-free passenger loading and unloading space))</del> <u>No ((parking required)) minimum requirement</u>

**Table B for 23.54.015  
 Required parking for residential uses**

Use		Minimum parking required
<del>((D.))</del> <u>C.</u>	Caretaker's quarters	1 space for each <u>2</u> dwelling units
<del>((E.))</del> <u>D.</u>	Congregate residences	1 space for each 4 sleeping rooms
<del>((F.</del>	<del>Cottage housing developments<sup>-1</sup></del>	<del>1 space for each dwelling unit</del>
<del>G.</del>	<del>Floating homes</del>	<del>1 space for each dwelling unit</del>
<del>H.))</del> <u>E.</u>	Mobile home parks	1 space for each <u>2</u> mobile home lots as defined in Chapter 22.904
<del>((I.</del>	<del>Multifamily residential uses, except as otherwise provided in this Table B for 23.54.015<sup>-1,2</sup></del>	<del>1 space per dwelling unit, or 1 space for each 2 small efficiency dwelling units</del>
<del>J.</del>	<del>Nursing homes</del>	<del>1 space for each 2 staff doctors; plus 1 additional space for each 3 employees; plus 1 space for each 6 beds</del>
<del>K.))</del> <u>F.</u>	<del>((Single family dwelling units)) Housing<sup>((1,3)) 4,5</sup></del>	1 space for each <u>2</u> dwelling units

**II. Residential use requirements for specific areas<sup>1</sup>**

<del>((L.))</del> <u>G.</u>	All residential uses within urban centers or within <del>((the))</del> a Station Area Overlay District <sup>((2))</sup>	No minimum requirement
<del>((M.))</del> <u>H.</u>	All residential uses <del>((in commercial, RSL, and multifamily zones))</del> within urban villages that are not within an urban center or <del>((the))</del> a Station Area Overlay District <del>((,))</del> if the residential use is located within a frequent transit or major transit service area <sup>((2,4))</sup>	No minimum requirement



**Table B for 23.54.015  
 Required parking for residential uses**

Use		Minimum parking required
I.	<u>All residential uses within a major transit service area</u>	<u>No minimum requirement</u>
<del>((N.</del>	<del>Multifamily residential uses within the University of Washington parking impact area shown on Map A for 23.54.015<sup>2</sup></del>	<del>1 space per dwelling unit for dwelling units with fewer than 2 bedrooms; plus 1.5 spaces per dwelling units with 2 or more bedrooms; plus 0.25 spaces per bedroom for dwelling units with 3 or more bedrooms</del>
<del>O.</del>	<del>Multifamily dwelling units, within the Alki area shown on Map B for 23.54.015<sup>2</sup></del>	<del>1.5 spaces for each dwelling unit))</del>
<del>P.))</del> J.	<del>Congregate residences located within ((one-half mile walking distance of a major transit stop))</del> <u>a frequent transit service area</u>	<u>No minimum requirement</u>

**III. Residential use requirements for specific unit types or sizes <sup>1</sup>**

K.	<u>Dwelling units and congregate residences that are less than 1,200 square feet in size</u>	<u>No minimum requirement</u>
L.	<u>Housing, Low-income</u>	<u>No minimum requirement</u>
M.	<u>Residential structures serving seniors or persons with disabilities</u>	<u>No minimum requirement</u>

Footnotes to Table B for 23.54.015

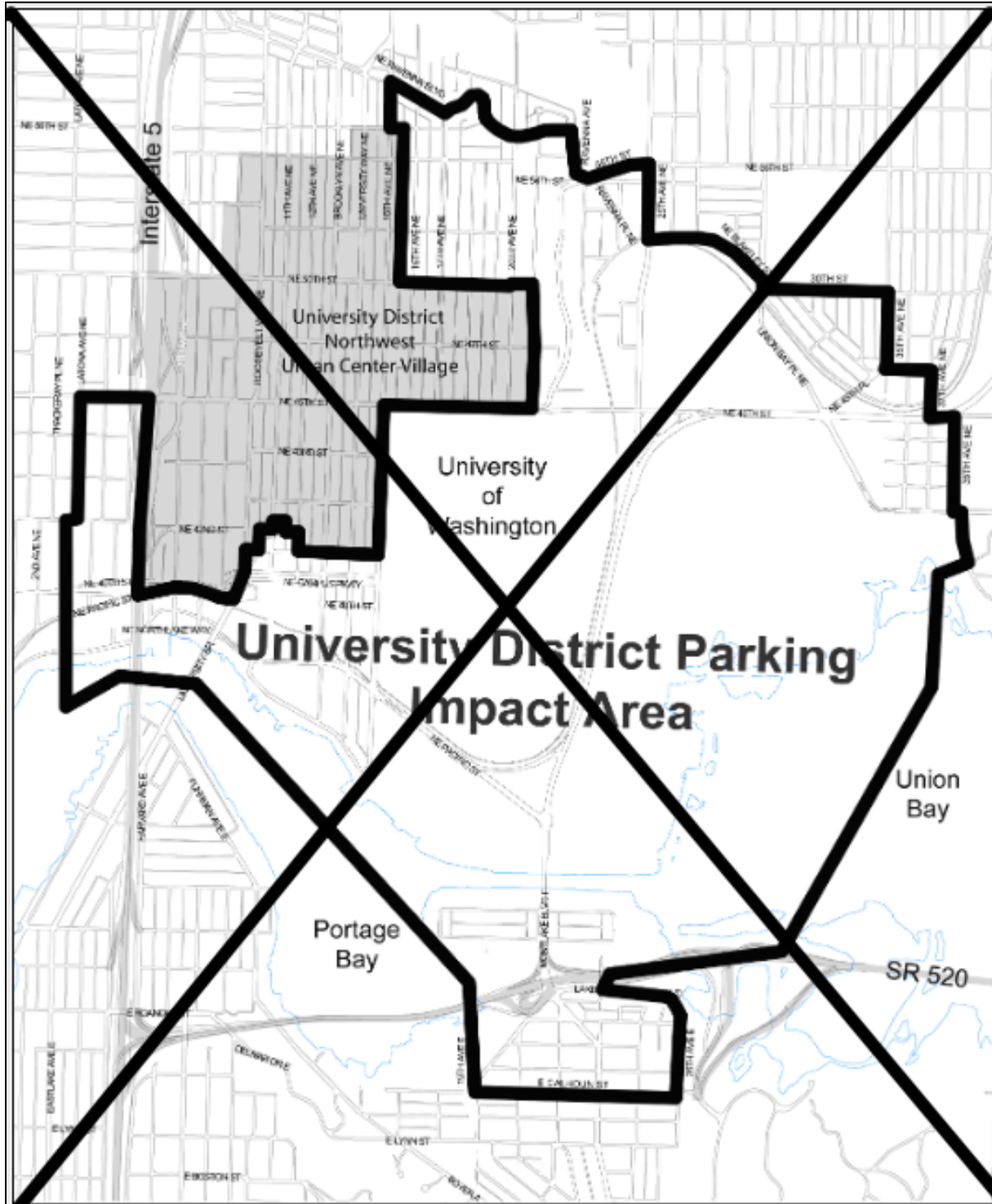
<sup>1</sup> ~~((For each moderate income unit and each low income unit, no minimum amount of parking is required.~~

<sup>2</sup>) The minimum amount of parking prescribed by Part I of Table B for 23.54.015 does not apply if a use, structure, or development qualifies for a ~~((greater or a))~~ lesser amount of minimum parking, including no parking, under any other provision of this Section 23.54.015. If more than one provision in this Table B for 23.54.015 is applicable, the provision requiring the least amount of minimum parking applies ~~((, except that if item O in~~

**Table B for 23.54.015**  
**Required parking for residential uses**

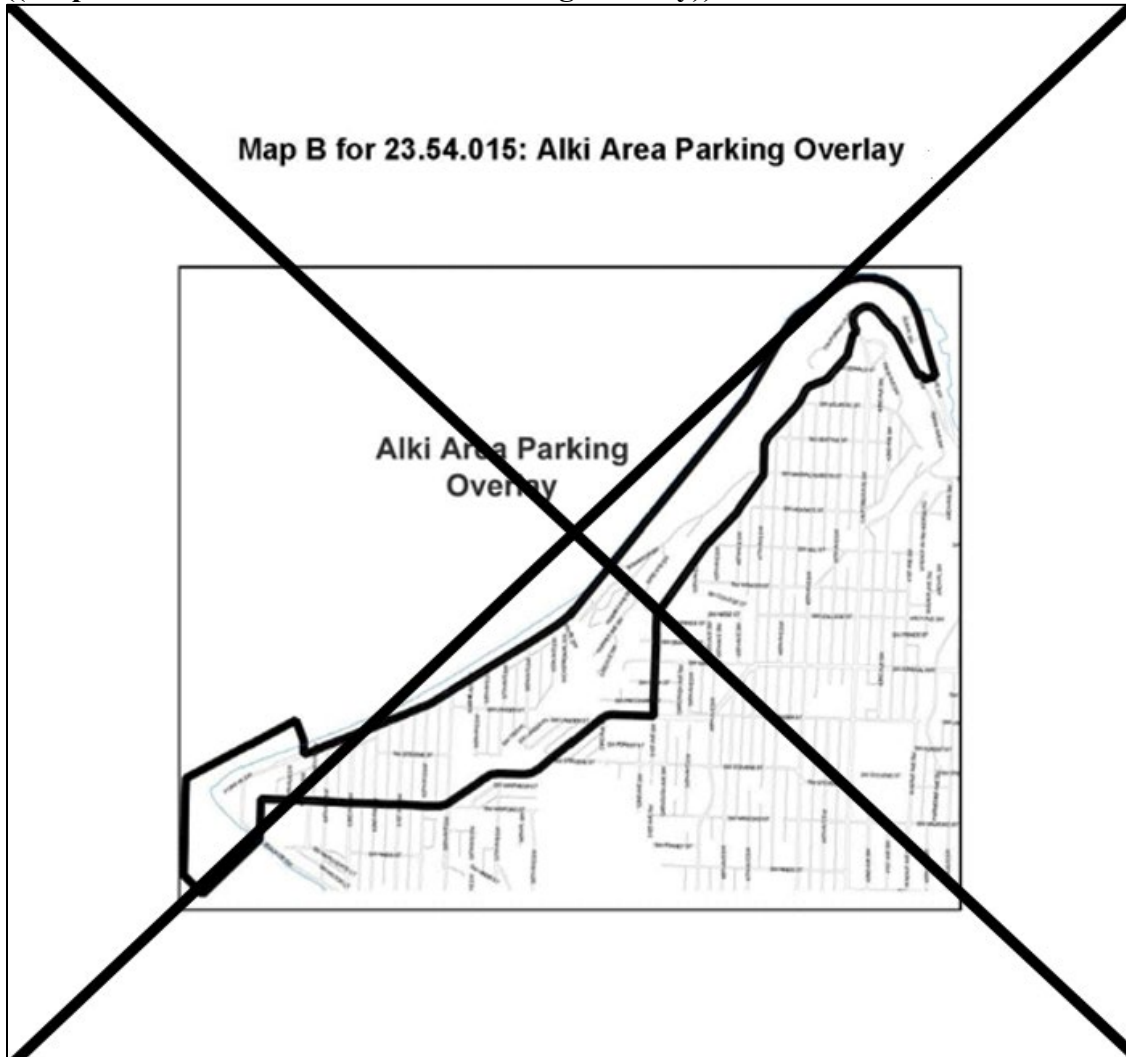
Use	Minimum parking required
<p>Part II of Table B for 23.54.015 applies, it shall supersede any other requirement in Part I or Part II of this Table B for 23.54.015)).</p> <p><sup>2</sup> For each moderate-income unit and each low-income unit, no minimum amount of parking is required(<del>(, or achieves a tree point score under Section 23.44.120, through planting or preserving medium and large trees that would result in a 10 percent canopy coverage for the site at tree maturity)</del>).</p> <p><sup>3</sup> The Director shall waive parking requirements for any development that retains a Tier 2 tree <u>or achieves a tree point score under Section 23.44.120, through planting or preserving medium/large or large trees, that would result in a ten percent canopy coverage for the lot at tree maturity.</u> A reduction or waiving of parking requirements may be permitted if the Director finds that the reduction or waiver is necessary (<del>(in order)</del>) to protect a Tier 3 tree as defined in Chapter 25.11.</p> <p><sup>4</sup> No parking is required for (<del>(single-family residential uses)</del>) <u>accessory dwelling units.</u></p> <p><sup>5</sup> No parking is required for <u>principal dwelling units</u> on lots in any residential zone that are less than 3,000 square feet in size or less than 30 feet in width where access to parking is permitted through a required (<del>(yard or)</del>) setback abutting a street according to the standards of subsections (<del>(23.44.016.B.2)</del>) <u>23.44.160.F.2, 23.45.536.C.2, or 23.45.536.C.3.</u></p> <p><del>((<sup>4</sup> Except as provided in Footnote 4, the minimum amounts of parking prescribed by Part I of Table B for 23.54.015 apply within 1,320 feet of the Fauntleroy Ferry Terminal.))</del></p>	

1 **((Map A for 23.54.015: University District Parking Impact Area))**



2

1 **((Map B for 23.54.015: Alki Area Parking Overlay))**



2

**Table C for 23.54.015  
 Required parking for public uses and institutions**

Use	Minimum parking required
<b>I. General public uses and institutions <sup>1</sup></b>	
A.	Adult care centers <sup>((+)) 2, 3</sup> 1 space for each 10 adults (clients) or 1 space for each staff member, whichever is greater; plus 1 loading and unloading space for each 20 adults (clients)

**Table C for 23.54.015  
 Required parking for public uses and institutions**

Use		Minimum parking required
B.	Child care centers <del>((2, 3, 4, 5))</del> (12)	<del>((1 space for each 10 children or 1 space for each staff member, whichever is greater; plus 1 loading and unloading space for each 20 children))</del> <u>No ((parking)) <u>minimum requirement</u></u>
C.	Colleges	A number of spaces equal to 15 percent of the maximum number of students that the facility is designed to accommodate; plus 30 percent of the number of employees the facility is designed to accommodate; plus 1 space for each 100 square feet of spectator assembly area in outdoor spectator sports facilities
D.	Community centers owned and operated by the Seattle Department of Parks and Recreation (SPR) <del>((4))</del> 6	1 space for each 555 square feet; or for family support centers, 1 space for each 100 square feet
E.	Community clubs <del>((,))</del> and community centers not owned and operated by SPR <del>((4, 5))</del> 7, 8	1 space for each 80 square feet of floor area of all auditoria and public assembly rooms containing fixed seats; plus 1 space for each 350 square feet of all other indoor areas
F.	Community farms <del>((5))</del> 8	1 space plus 1 space for each 10,000 square feet of site area, or 10 spaces, whichever is less
G.	Hospitals	1 space for each 2 staff doctors; plus 1 additional space for each 5 employees other than staff doctors; plus 1 space for each 6 beds
<del>((H.</del>	<del>Institutes for advanced study, except in neighborhood residential zones</del>	<del>1 space for each 1,000 square feet of offices and similar spaces; plus 1 space for each 10 fixed seats in all auditoria and public assembly rooms; or 1 space for each 100</del>

**Table C for 23.54.015  
 Required parking for public uses and institutions**

Use		Minimum parking required
		<del>square feet of public assembly area not containing fixed seats))</del>
(( <del>F.</del> ) <u>H.</u> )	Institutes for advanced study in <del>((neighborhood residential))</del> <u>Neighborhood Residential</u> zones (existing) <sup>1</sup>	3.5 spaces for each 1,000 square feet of office space; plus 10 spaces for each 1,000 square feet of additional building footprint to house and support conference center activities; or 37 spaces for each 1,000 square feet of conference room space, whichever is greater
(( <del>J.</del> ) <u>I.</u> )	Libraries <sup>((4,5)) 8,9</sup>	1 space for each 80 square feet of floor area of all auditoria and public meeting rooms containing fixed seats; plus 1 space for each 500 square feet of floor area of all other areas
(( <del>K.</del> ) <u>J.</u> )	Museums <sup>(+)</sup>	1 space for each 80 square feet of all auditoria and public assembly rooms, not containing fixed seats; plus 1 space for every 10 fixed seats for floor area containing fixed seats; plus 1 space for each 250 square feet of other gross floor area open to the public
(( <del>L.</del> ) <u>K.</u> )	Private clubs	1 space for each 80 square feet of floor area of all auditoria and public assembly rooms not containing fixed seats; or 1 space for every 8 fixed seats for floor area containing fixed seats; or if no auditorium or assembly room, 1 space for each 350 square feet, excluding ball courts
(( <del>M.</del> ) <u>L.</u> )	Religious facilities <sup>(+)</sup>	1 space for each 80 square feet of all auditoria and public assembly rooms
(( <del>N.</del> ) <u>M.</u> )	<del>Schools, private elementary and secondary</del> <sup>(+)</sup>	<del>1 space for each 80 square feet of all auditoria and public assembly rooms, or if no auditorium or assembly room, 1 space for each staff member</del>

**Table C for 23.54.015  
 Required parking for public uses and institutions**

Use		Minimum parking required
<del>Q</del> ) <u>M</u> .	Schools, ( <del>public</del> ) elementary and secondary <sup>7</sup> , ( <del>9</del> ) <sup>10,11</sup>	1 space for each 80 square feet of all auditoria ( <del>(or)</del> ) and public assembly rooms <u>without fixed seats</u> , or 1 space for every 8 fixed seats in auditoria or public assembly rooms containing fixed seats ( <del>(, for new public schools on a new or existing public school site</del> )
<del>P</del> ) <u>N</u> .	Vocational or fine arts schools	1 space for each 2 faculty that the facility is designed to accommodate; plus 1 space for each 2 full-time employees other than faculty that the facility is designed to accommodate; plus 1 space for each 5 students, based on the maximum number of students that the school is designed to accommodate
<b>II. General public uses and institutions for specific areas</b>		
<del>Q</del> ) <u>O</u> .	General public uses, institutions and Major Institution uses, except hospitals, in urban centers or the Station Area Overlay District ( <del>(+)</del> ) <sup>12</sup>	No minimum requirement
<del>R</del> ) <u>P</u> .	General public uses and institutions, except hospitals, including institutes for advanced study in ( <del>neighborhood residential</del> ) <u>Neighborhood Residential</u> zones, within urban villages that are not within the Station Area Overlay District, if the use is located within a frequent transit service area	No minimum requirement

**Table C for 23.54.015**  
**Required parking for public uses and institutions**

Use	Minimum parking required
<p>Footnotes to Table C for 23.54.015</p> <p><sup>1</sup> <del>((When this use is permitted in a neighborhood residential zone as a conditional use, the))</del> <u>The Director may modify the parking requirements in this Table A for 23.54.015 for institutions in Neighborhood Residential and multifamily zones pursuant to the conditional uses provisions in Section ((23.44.022)) 23.44.030 ((; when the use is permitted in a multifamily zone as a conditional use, the Director may modify the parking requirements pursuant to)) and Section 23.45.570.</u></p> <p><sup>2</sup> <u>The amount of required parking is calculated based on the maximum number of staff((; children,)) or clients that the center is designed to accommodate on site at any one time. No parking is required for adult care centers that provide housing for clients.</u></p> <p><sup>3</sup> <u>As a Type I decision, the Director, in consultation with the Director of the Seattle Department of Transportation, may allow adult care ((and child care)) centers to provide loading and unloading spaces on street, if not prevented by current or planned transportation projects adjacent to their property, when no other alternative exists.</u></p> <p><sup>4</sup> <del>((A child care facility, when co-located with an assisted living facility, may count the passenger load/unload space required for the assisted living facility toward its required passenger load/unload spaces.))</del> <u>[Reserved]</u></p> <p><sup>5</sup> <del>((When this use is permitted outright in a neighborhood residential or multifamily zone, the Director may reduce the parking and loading requirements of Section 23.54.015 and the requirements of Section 23.44.016 or Section 23.45.536 on a case-by-case basis if the applicant can demonstrate that the modification is necessary due to the specific features, activities, or programs of the institution and links the reduction to the features of the institution that justify the reduction. Such modifications shall be valid only under the conditions specified, and if those conditions change, the standard requirement shall be satisfied.))</del> <u>[Reserved]</u></p> <p><sup>6</sup> <u>When family support centers are located within community centers owned and operated by the Department of Parks and Recreation, the Director may lower the combined parking requirement by up to a maximum of 15 percent, pursuant to subsection 23.54.020.I.</u></p> <p><sup>7</sup> <u>Indoor gymnasiums are not considered ball courts, nor are they considered auditoria or public assembly rooms unless they contain bleachers (fixed seats). If the gymnasium contains bleachers, the parking requirement for the gymnasium is one parking space for every eight fixed seats. Each 20 inches of width of bleachers is counted as one fixed seat for the purposes of determining parking requirements. If the gymnasium does not contain bleachers and is in a school, there is no parking requirement for the gymnasium. If the gymnasium does not contain bleachers and is in a community center, the parking requirement is one space for each 350 square feet.</u></p> <p><sup>8</sup> <u>The Director may reduce the parking and loading requirements of Section 23.54.015 and the requirements of Section 23.44.080 or Section 23.45.536 on a case-by-case</u></p>	



**Table C for 23.54.015  
 Required parking for public uses and institutions**

**Use**

**Minimum parking required**

basis if the applicant can demonstrate that the modification is necessary due to the specific features, activities, or programs of the institution and links the reduction to the features of the institution that justify the reduction. Such modifications shall be valid only under the conditions specified, and if those conditions change, the standard requirement shall be satisfied.

<sup>2</sup> When a library is permitted in a multifamily or commercial zone as a conditional use, the Director may modify the parking requirements of Section 23.54.015 and the requirements of Section 23.45.536 or Sections 23.47A.030 and 23.47A.032 on a case-by-case basis if the applicant can demonstrate that the modification is necessary due to the specific features, activities, or programs of the institution and links the reduction to the features of the institution that justify the reduction. Such modifications shall be valid only under the conditions specified, and if those conditions change, the standard requirement shall be satisfied.

~~((9))~~ <sup>10</sup> For public schools, when an auditorium or other place of assembly is demolished and a new one built in its place, parking requirements are determined based on the new construction. When an existing public school on an existing public school site is remodeled, additional parking is required if any auditorium or other place of assembly is expanded or additional fixed seats are added. Additional parking is required as shown in this Table C for 23.54.015 for the increase in floor area or increase in number of seats only. If the parking requirement for the increased area or seating is ~~((10))~~ <sup>ten</sup> percent or less than that for the existing auditorium or other place of assembly, then no additional parking is required.

~~((40))~~ <sup>11</sup> ~~((Development))~~ For public schools, development standard departures may be granted or required pursuant to the procedures and criteria set forth in Chapter 23.79 to reduce the required or permitted number of parking spaces.

~~((44))~~ <sup>12</sup> The general requirements of lines A through P of this Table C for 23.54.015 for general public uses and institutions, and requirements of subsection 23.54.016.B for Major Institution uses, are superseded to the extent that a use, structure, or development qualifies for either a greater or a lesser parking requirement (which may include no requirement) under any other provision. To the extent that a general public use, institution, or Major Institution use fits within more than one line in this Table C for 23.54.015, the least of the applicable parking requirements applies. The different parking requirements listed for certain categories of general public uses or institutions shall not be construed to create separate uses for purposes of any requirements related to establishing or changing a use under this Title 23.

~~((42))~~ <sup>The Director may reduce the minimum parking requirements for a child care center in any zone if a portion of its parking demand can be accommodated in nearby on-street parking.</sup>

1           ~~((K. Bicycle parking.))~~

2    **23.54.037 Bicycle parking**

3           A. Number of spaces

4                 1. The minimum number of parking spaces for bicycles required for specified  
5 uses is set forth in Table ~~((D for 23.54.015))~~ A for 23.54.037.

6                 2. Long-term parking for bicycles shall be for bicycles parked four or more hours.  
7 Short-term parking for bicycles shall be for bicycles parked less than four hours. In the case of a  
8 use not shown on Table ~~((D for 23.54.015))~~ A for 23.54.037, one bicycle parking space per  
9 10,000 gross square feet of either short- or long-term bicycle parking is required~~((, except single-~~  
10 ~~family residential use is exempt from bicycle parking requirements))~~ .

11                3. The minimum requirements are based upon gross floor area of the use in a  
12 structure minus gross floor area in parking uses, or the square footage of the use when located  
13 outside of an enclosed structure, or as otherwise specified.

14                ~~((1.))~~ 4. Rounding. For long-term bicycle parking, calculation of the minimum  
15 requirement shall round up the result to the nearest whole number. For short-term bicycle  
16 parking, calculation of the minimum requirement shall round up the result to the nearest whole  
17 even number.

18                ~~((2.))~~ B. Performance standards. Provide bicycle parking in a highly visible, safe, and  
19 convenient location, emphasizing user convenience and theft deterrence, based on rules  
20 promulgated by the Director of the Seattle Department of Transportation that address the  
21 considerations in this subsection ~~((23.54.015.K.2))~~ 23.54.037.B.

1                    ~~((a.))~~ 1. Provide secure locations and arrangements of long-term bicycle parking,  
2 with features such as locked rooms or cages and bicycle lockers. The bicycle parking should be  
3 installed in a manner that avoids creating conflicts with automobile accesses and driveways.

4                    ~~((b.))~~ 2. For a garage with bicycle parking and motor vehicle parking for more  
5 than two dwelling units, provide pedestrian and bicycle access to long-term bicycle parking that  
6 is separate from other vehicular entry and egress points or uses the same entry or egress point but  
7 has a marked walkway for pedestrians and bicyclists.

8                    ~~((c.))~~ 3. Provide adequate lighting in the bicycle parking area and access routes to  
9 it.

10                   ~~((d.))~~ 4. If short-term bicycle parking facilities are not clearly visible from the  
11 street or sidewalk or adjacent on-street bicycle facilities, install directional signage in adequate  
12 amounts and in highly visible locations in a manner that promotes easy wayfinding for bicyclists.

13                   ~~((e.))~~ 5. Provide signage to long-term bicycle parking that is oriented to building  
14 users.

15                   ~~((f.))~~ 6. Long-term bicycle parking shall be located where bicyclists are not  
16 required to carry bicycles on exterior stairs with more than five steps to access the parking. The  
17 Director, as a Type I decision, may allow long-term bicycle parking for rowhouse and townhouse  
18 development to be accessed by stairs with more than five steps, if the slope of the lot makes  
19 access with five or fewer steps infeasible.

20                   ~~((g.))~~ 7. Where practicable, long-term bicycle parking shall include a variety of  
21 rack types to accommodate different types of bicycles.

1                    ~~((h-))~~ 8. Install bicycle parking hardware so that it can perform to its  
2 manufacturer's specifications and any design criteria promulgated by the Director of the Seattle  
3 Department of Transportation, allowing adequate clearance for bicycles and their riders.

4                    ~~((i-))~~ 9. Provide full weather protection for all required long-term bicycle parking.

5                    ~~((3-))~~ C. Location of bicycle parking

6                    ~~((a-))~~ 1. Long-term bicycle parking required for residential uses shall be located  
7 on-site except as provided in subsection ~~((23.54.015.K.3.e))~~ 23.54.037.C.3.

8                    ~~((b-))~~ 2. Short-term bicycle parking may be provided on the lot or in an adjacent  
9 right-of-way, subject to approval by the Director of the Seattle Department of Transportation, or  
10 as provided in subsection ~~((23.54.015.K.3.e))~~ 23.54.037.C.3.

11                   ~~((e-))~~ 3. Both long-term and short-term bicycle parking for residential uses may be  
12 provided off-site if within 600 feet of the residential use to which the bicycle parking is  
13 accessory and if the site of the bicycle parking is functionally interrelated to the site of the  
14 residential use to which the bicycle parking is accessory, such as within a unit lot subdivision or  
15 if the sites are connected by access easements, or if a covenant or similar property right is  
16 established to allow use of the off-site bicycle parking.

17                   ~~((4-))~~ D. Long-term bicycle parking required for small efficiency dwelling units and  
18 congregate residence sleeping rooms is required to be covered for full weather protection. If the  
19 required, covered long-term bicycle parking is located inside the building that contains small  
20 efficiency dwelling units or congregate residence sleeping rooms, the space required to provide  
21 the required long-term bicycle parking shall be exempt from floor area ratio (FAR) limits.  
22 Covered long-term bicycle parking that is provided beyond the required bicycle parking shall not  
23 be exempt from FAR limits.

1           ~~((5-))~~ E. Bicycle parking facilities shared by more than one use are encouraged.

2           ~~((6-))~~ F. Except as provided in subsection ~~((23.54.015.K.7))~~ 23.54.015.G, bicycle parking  
3 facilities required for ~~((non-residential))~~ nonresidential uses shall be located:

4                   ~~((a-))~~ 1. On the lot; or

5                   ~~((b-))~~ 2. For a functionally interrelated campus containing more than one building,  
6 in a shared bicycle parking facility within 600 feet of the lot; or

7                   ~~((e-))~~ 3. Short-term bicycle parking may be provided in an adjacent right-of-way,  
8 subject to approval by the Director of the Seattle Department of Transportation.

9           ~~((7-))~~ G. For ~~((non-residential))~~ nonresidential uses on a functionally interrelated campus  
10 containing more than one building, both long-term and short-term bicycle parking may be  
11 located in an off-site location within 600 feet of the lot, and short-term public bicycle parking  
12 may be provided in a right-of-way, subject to approval by the Director of the Seattle Department  
13 of Transportation. The Director of the Seattle Department of Transportation may consider  
14 whether bicycle parking in the public place shall be sufficient in quality to effectively serve  
15 bicycle parking demand from the site.

16           ~~((8-))~~ H. Bicycle commuter shower facilities. Structures containing 100,000 square feet or  
17 more of office use floor area shall include shower facilities and clothing storage areas for bicycle  
18 commuters. Two showers shall be required for every 100,000 square feet of office use. They  
19 shall be available in a manner that results in equal shower access for all users. The facilities shall  
20 be for the use of the employees and occupants of the building, and shall be located where they  
21 are easily accessible to bicycle parking facilities, which may include in places accessible by  
22 elevator from the bicycle parking location.

1            ~~((9-))~~ I. Bicycle parking spaces within dwelling units or on balconies do not count toward  
 2 the bicycle parking requirement, except if the bike parking spaces are located:  
 3            ~~((a-))~~ 1. In a private garage; or  
 4            ~~((b-))~~ 2. Within the ground floor of a dwelling unit in ~~((a townhouse or rowhouse~~  
 5 ~~development))~~ an attached dwelling unit.

**Table ~~((D for 23.54.015)) A for 23.54.037~~**  
**Parking for bicycles <sup>1</sup>**

<del>((USE))</del> <u>Use</u>	Bike parking requirements		
	Long-term	Short-term	
* * *			
<b>D. RESIDENTIAL USES <sup>3</sup></b>			
<u>D.1</u>	<u>Assisted living facility</u>	<u>None</u>	<u>None</u>
<del>((D.1))</del> <u>D.2</u>	Congregate residences <sup>4,5,6</sup>	1 per 4 sleeping rooms	1 per 80 sleeping rooms. 2 spaces minimum
<del>((D.2</del>	<del>Multifamily structures other than townhouse and rowhouse developments <sup>4,5</sup></del>	<del>1 per dwelling unit</del>	<del>1 per 20 dwelling units</del>
<u>D.3</u>	<u>Single-family residences</u>	<u>None</u>	<u>None</u>
<u>D.4</u>	<u>Townhouse and rowhouse developments <sup>5,6</sup></u>	<u>1 per dwelling unit</u>	<u>None))</u>
<u>D.3</u>	<u>Permanent supportive housing</u>	<u>None</u>	<u>None</u>
<u>D.4</u>	<u>Other residential uses <sup>4,5</sup></u>	<u>1 per dwelling unit <sup>6</sup></u>	<u>1 per 20 dwelling units, except none for projects with less than 20 dwelling units</u>

**Table ((~~D for 23.54.015~~) A for 23.54.037)  
 Parking for bicycles <sup>1</sup>**

<b>((<del>USE</del>) <u>Use</u></b>		<b>Bike parking requirements</b>	
		<b>Long-term</b>	<b>Short-term</b>
<b>E. TRANSPORTATION FACILITIES</b>			
E.1( <del>(+)</del> )	Park and ride facilities on surface parking lots	At least 20 <del>(+)</del> <sup>7</sup>	At least 10
E.2( <del>(+)</del> )	Park and ride facilities in parking garages	At least 20 if parking is the principal use of a property; zero if non-parking uses are the principal use of a property	At least 10 if parking is the principal use of a property; zero if non-parking uses are the principal use of a property
E.3( <del>(+)</del> )	Flexible-use parking garages and flexible-use parking surface lots	1 per 20 auto spaces	None
E.4((-))	<del>Rail transit facilities and passenger</del> Passenger terminals	Spaces for 5 percent of projected AM peak period daily ridership <del>(+)</del> <sup>7</sup>	Spaces for 2 percent of projected AM peak period daily ridership <sup>7</sup>
E.5((-))	Light rail transit stations	Regulated by subsection 23.80.008.L	Regulated by subsection 23.80.008.L

Footnotes to Table ((~~D for 23.54.015~~) A for 23.54.037)

<sup>1</sup> Required bicycle parking includes long-term and short-term amounts shown in this Table ((~~D for 23.54.015~~) A for 23.54.037).

<sup>2</sup> The Director may reduce short-term bicycle parking requirements for theaters and spectator sports facilities that provide bicycle valet services authorized through a Transportation Management Program. A bicycle valet service is a service that allows bicycles to be temporarily stored in a secure area, such as a monitored bicycle corral.

<sup>3</sup> For residential uses, after the first 50 spaces for bicycles are provided, additional spaces are required at three-quarters the ratio shown in this Table ((~~D for 23.54.015~~) A for 23.54.037).

<sup>4</sup> For ((~~congregate residences or multifamily~~)) residential structures that are owned and

**Table (~~D for 23.54.015~~) A for 23.54.037  
 Parking for bicycles <sup>1</sup>**

<del>(USE)</del> <u>Use</u>	Bike parking requirements	
	Long-term	Short-term

operated by a not-for-profit entity serving seniors or persons with disabilities, or that are licensed by the State and provide supportive services for seniors or persons with disabilities, as a Type I decision, the Director shall have the discretion to reduce the amount of required bicycle parking to as few as zero if it can be demonstrated that residents are less likely to travel by bicycle.

<sup>5</sup> In low-income housing, there is no minimum required long-term bicycle parking requirement for each unit subject to affordability limits no higher than 30 percent of median income and long-term bicycle parking requirements may be waived by the Director as a Type I decision for each unit subject to affordability limits greater than 30 percent of median income and no higher than 80 percent of median income if a reasonable alternative is provided (e.g., in-unit vertical bike storage).

<sup>6</sup> Long-term bike parking is not required in NR zones.

~~(6)~~<sup>7</sup> The Director, in consultation with the Director of Transportation, may require more bicycle parking spaces based on the following factors: area topography; pattern and volume of expected bicycle users; nearby residential and employment density; proximity to the Urban Trails system and other existing and planned bicycle facilities; projected transit ridership and expected access to transit by bicycle; and other relevant transportation and land use information.

1  
 2 Section 60. Section 23.54.016 of the Seattle Municipal Code, last amended by Ordinance  
 3 125558, is amended as follows:

4 **23.54.016 Major Institutions—(~~parking~~) Parking and transportation**

5 Except in the MPC-YT zone, Major Institution uses are subject to the following transportation  
 6 and parking requirements:

7 \* \* \*

8 B. Parking (~~Quantity Required~~) quantity required

9 1. In urban centers and the Station Area Overlay District, no parking is required  
 10 for Major Institution uses, except for hospitals.



1                   2. For all other Major Institutions the minimum number of parking spaces  
2 required is as follows:

3                   a. Long-term (~~(Parking-)~~) parking

4                   1) Medical (~~(Institutions)~~) institutions. A number of spaces equal  
5 to 80 percent of hospital-based doctors; plus 25 percent of staff doctors; plus 30 percent of all  
6 other employees present at peak hour;

7                   2) Educational (~~(Institutions)~~) institutions. A number of spaces  
8 equal to 15 percent of the maximum students present at peak hour, excluding resident students;  
9 plus 30 percent of employees present at peak hour; plus 25 percent of the resident unmarried  
10 students; plus one space for each married student apartment unit.

11                   b. Short-term (~~(Parking-)~~) parking

12                   1) Medical (~~(Institutions)~~) institutions. A number of spaces equal  
13 to one space per six beds; plus one space per five average daily outpatients;

14                   2) Educational (~~(Institutions)~~) institutions. A number of spaces  
15 equal to five percent of the maximum students present at peak hour excluding resident students.

16                   c. Additional (~~(Short-term Parking Requirements)~~) short-term parking  
17 requirements. When one of the following uses is a Major Institution use, the following additional  
18 short-term parking requirements shall be met. Such requirements may be met by joint use of  
19 parking areas and facilities if the Director determines that the uses have different hours of  
20 operation according to subsection 23.54.020.G:

21                   1) Museum. One space for each 250 square feet of public floor  
22 area;

1                                 2) Theater, (~~(Auditorium)~~) auditorium, or (~~(Assembly Hall)~~)  
2 assembly hall. One space for each 200 square feet of audience assembly area not containing  
3 fixed seats, and one space for every (~~(10)~~) ten seats for floor area containing fixed seats;

4                                 3) Spectator (~~(Sports Facility Containing Fewer)~~) sports facility  
5 containing fewer than 20,000 (~~(Seats)~~) seats. One space for each (~~(10)~~) ten permanent seats and  
6 one space for each 100 square feet of spectator assembly area not containing fixed seats;

7                                 4) Spectator (~~(Sports Facility Containing)~~) sports facility  
8 containing 20,000 or (~~(More Seats)~~) more seats. One space for each (~~(10)~~) ten permanent seats  
9 and one bus space for each 300 permanent seats.

10                                 d. Bicycle (~~(Parking)~~) parking. Bicycle parking meeting the development  
11 standards of subsections (~~(23.54.015.K.2)~~) 23.54.037.B through (~~(23.54.015.K.6)~~) 23.54.037.G  
12 and subsection 23.54.016.D.2 shall be provided in the following quantities:

13                                 1) Medical (~~(Institutions)~~) institutions. A number of spaces equal  
14 to two percent of employees, including doctors, present at peak hour;

15                                 2) Educational (~~(Institutions)~~) institutions. A number of spaces  
16 equal to (~~(10)~~) ten percent of the maximum students present at peak hour plus five percent of  
17 employees.

18                                 If at the time of application for a master use permit, the applicant can demonstrate  
19 that the bicycle parking requirement is inappropriate for a particular institution because of  
20 topography, location, nature of the users of the institution or other reasons, the Director may  
21 modify the bicycle parking requirement.

22                                 3. Parking (~~(Deficits)~~) deficits. In addition to providing the minimum required  
23 parking for a new structure, five percent of any vehicular or bicycle parking deficit as determined

1 by the minimum requirements of this subsection 23.54.016.B, existing on (~~the effective date of~~  
2 ~~the ordinance codified in this section~~) May 2, 1990, shall be supplied before issuance of a  
3 certificate of occupancy.

4 \* \* \*

5 Section 61. Section 23.54.020 of the Seattle Municipal Code, last amended by Ordinance  
6 126509, is amended as follows:

7 **23.54.020 Parking quantity exceptions**

8 The motor vehicle parking quantity exceptions set forth in this Section 23.54.020 apply in all  
9 zones except downtown zones, which are regulated by Section 23.49.019, and Major Institution  
10 zones, which are regulated by Section 23.54.016.

11 A. Adding (~~Units~~) units to (~~Existing Structures~~) existing structures in Multifamily and  
12 Commercial (~~Zones~~) zones

13 1. For the purposes of this Section 23.54.020, "existing structures" means those  
14 structures that were established under permit, or for which a permit has been granted and has not  
15 expired as of the applicable date, as follows:

16 a. In multifamily zones, August 10, 1982;

17 b. In commercial zones, June 9, 1986.

18 2. In locations in a multifamily or commercial zone where there is a minimum  
19 parking requirement, one dwelling unit may either be added to an existing structure or may be  
20 built on a lot that contains an existing structure without additional parking if both of the  
21 following requirements are met:

1 a. Either the existing parking provided on the lot meets development  
2 standards, or the lot area is not increased and existing parking is screened and landscaped to the  
3 greatest extent practical; and

4 b. Any additional parking shall meet all development standards for the  
5 zone.

6 3. In locations in a multifamily or commercial zone where there is a minimum  
7 parking requirement, the Director may authorize a reduction or waiver of the parking  
8 requirement as a Type I decision when dwelling units are proposed to be added either to an  
9 existing structure or on a lot that contains an existing structure, in addition to the exception  
10 permitted in subsection 23.54.020.A.2, if the ~~((conditions in subsections 23.54.020.A.3.a and b  
11 below are met, and either of the conditions in subsections 23.54.020.A.3.c or d below are met:~~

12 ~~a. The))~~ only use of the structure will be residential~~((;))~~ and one of the  
13 following conditions is met:

14 ~~((b. The lot is not located in either the University District Parking Overlay  
15 Area (Map A for 23.54.015) or the Alki Area Parking Overlay (Map B for 23.54.015); and~~

16 ~~e-))~~ a. The topography of the lot or location of existing structures makes  
17 provision of an off-street parking space physically infeasible in a conforming location; or

18 ~~((d-))~~ b. The lot is located in a residential parking zone (RPZ) and a  
19 current parking study is submitted showing a utilization rate of less than 75 percent for on-street  
20 parking within 400 feet of all lot lines.

21 B. Tandem ~~((Parking))~~ parking in ~~((Multifamily Structures))~~ multifamily structures. ~~((1-))~~  
22 Off-street parking required for multifamily structures may be provided as tandem parking, as  
23 defined in Section 23.54.030. ~~((A tandem parking space counts as one and one-half parking~~

1 ~~spaces, except as provided in subsection 23.54.020.B.2 below, and must meet the minimum size~~  
2 ~~requirements of subsection 23.54.030.A.~~

3 ~~2. When a minimum of at least one parking space per dwelling unit in a~~  
4 ~~multifamily structure is required, the total number of parking spaces provided, counting each~~  
5 ~~tandem parking space as one space, may not be less than the total number of dwelling units.)) A~~  
6 ~~tandem parking space counts at a rate of one space for every 20 linear feet of depth excluding~~  
7 ~~required access aisles.~~

8 \* \* \*

9 Section 62. Section 23.54.030 of the Seattle Municipal Code, last amended by Ordinance  
10 127099, is amended as follows:

11 **23.54.030 Parking space and access standards**

12 All parking spaces provided, whether required by Section 23.54.015 or not, and required barrier-  
13 free parking, shall meet the standards of this Section 23.54.030.

14 A. Parking space dimensions

15 1. "Large vehicle" means the minimum size of a large vehicle parking space shall  
16 be ~~((8.5)) 8~~ feet in width and 19 feet in length.

17 2. "Medium vehicle" means the minimum size of a medium vehicle parking space  
18 shall be 8 feet in width and 16 feet in length.

19 3. "Small vehicle" means the minimum size of a small vehicle parking space shall  
20 be 7.5 feet in width and 15 feet in length.

21 4. "Barrier-free parking" means a parking space meeting the following standards:

22 a. Parking spaces shall not be less than 8 feet in width and shall have an  
23 adjacent access aisle not less than 5 feet in width. Van-accessible parking spaces shall have an

1 adjacent access aisle not less than 8 feet in width. Where two adjacent spaces are provided, the  
2 access aisle may be shared between the two spaces. Boundaries of access aisles shall be marked  
3 so that aisles will not be used as parking space.

4                   b. A minimum length of 19 feet or when more than one barrier-free  
5 parking space is provided, at least one shall have a minimum length of 19 feet, and other spaces  
6 may be the lengths of small, medium, or large spaces in approximate proportion to the number of  
7 each size space provided on the lot.

8                   5. "Tandem parking" means a parking space equal to the width and two times the  
9 length of the vehicle size standards in subsections 23.54.030.A.1, 23.54.030.A.2, and  
10 23.54.030.A.3 for the size of the vehicle to be accommodated.

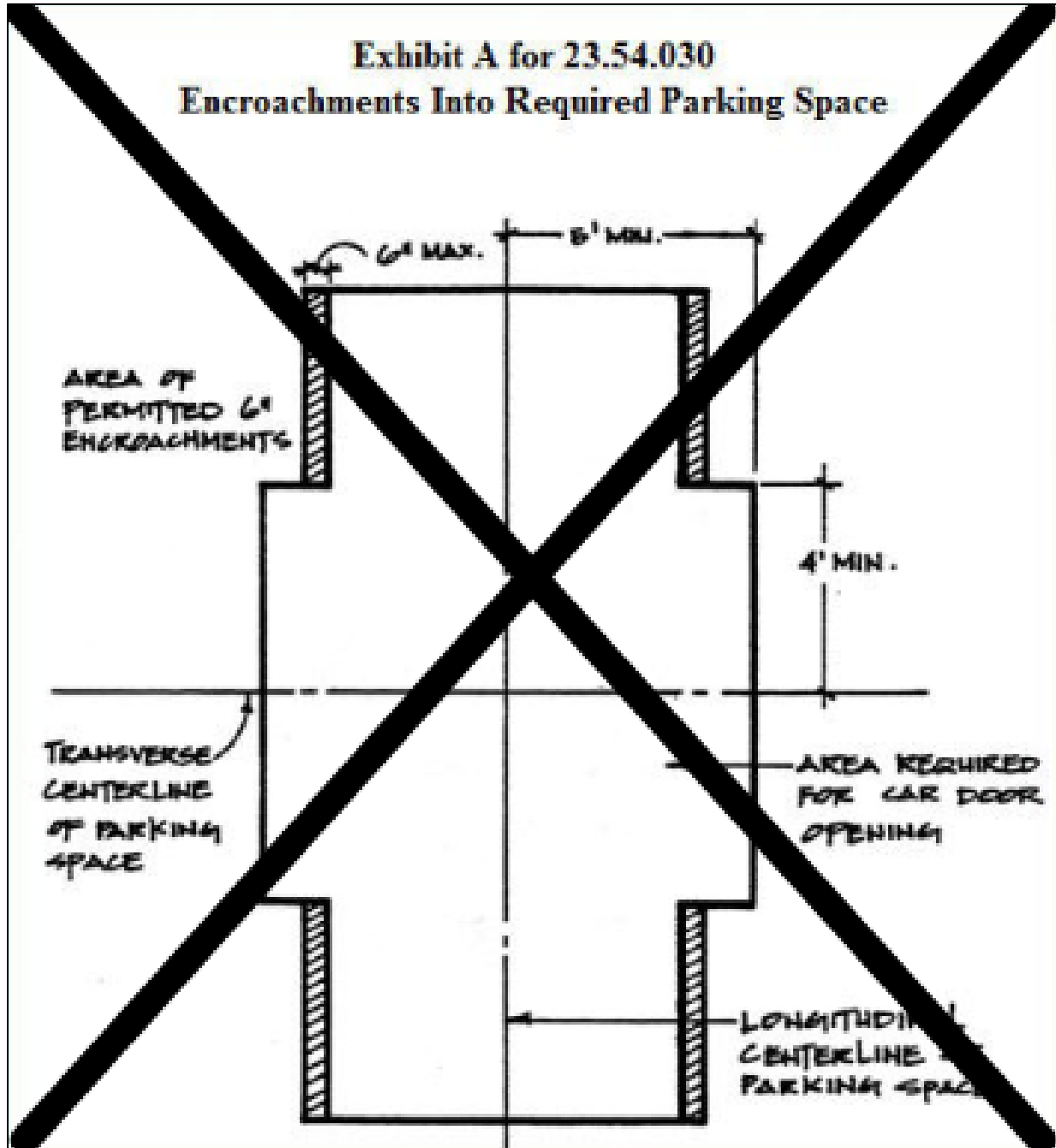
11                   6. No wall, post, guardrail, or other obstruction, or lot line, is permitted within the  
12 area for car door opening. Columns or other structural elements may encroach into the parking  
13 space a maximum of 6 inches on a side, except in the area for car door opening 5 feet from the  
14 longitudinal centerline, or 4 feet from the transverse centerline of a parking space (see Exhibit A  
15 for 23.54.030).

16                   7. If the parking space is next to a lot line and the parking space is parallel to the  
17 lot line, the minimum width of the space is 9 feet.

1 **Exhibit A for 23.54.030**

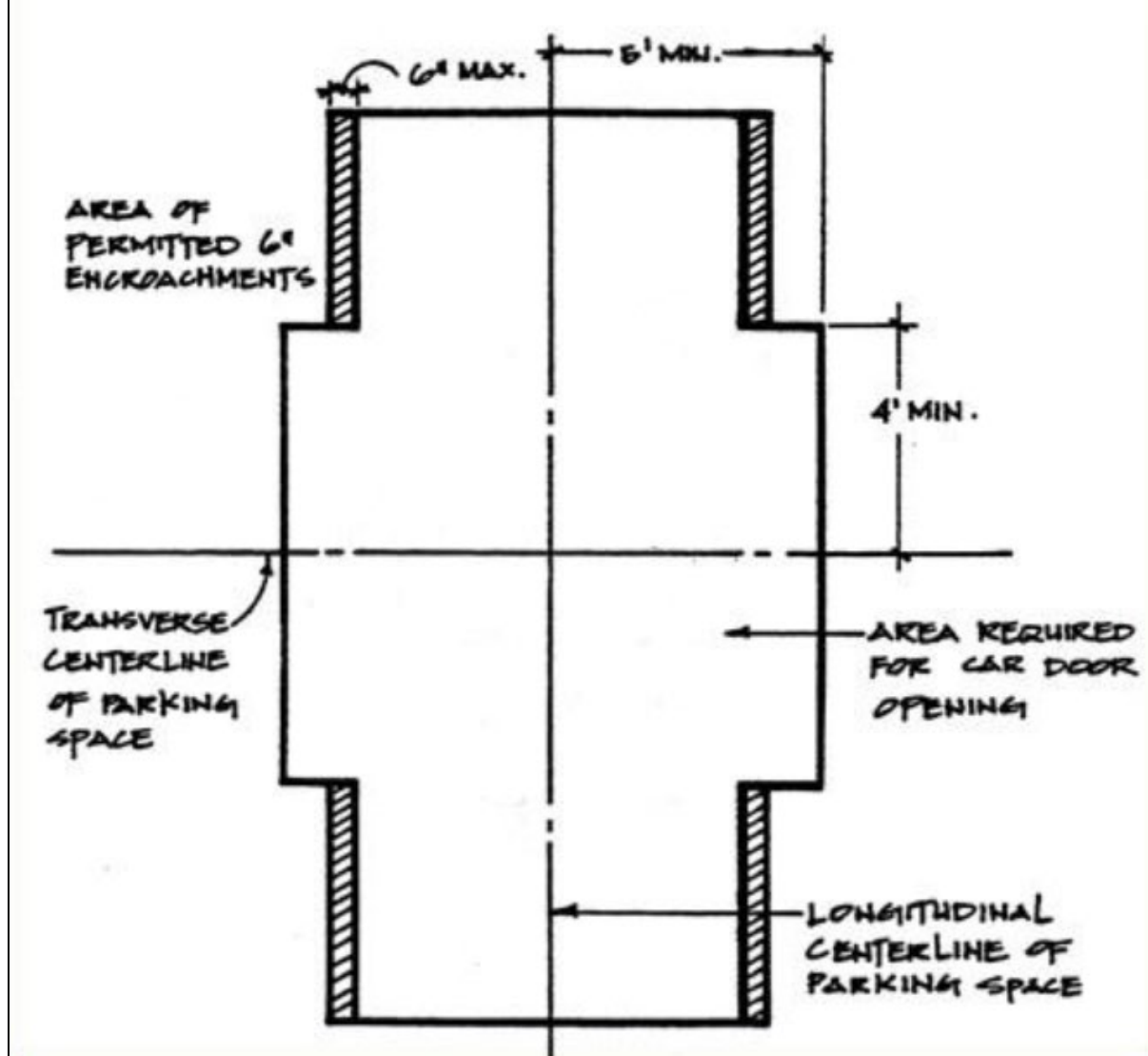
2 **Encroachments ((Into Required Parking Space)) into required parking**

3



4

### Exhibit A for 23.54.030 Encroachments into required parking



1  
2 B. Parking space requirements. The required size of parking spaces shall be determined  
3 by whether the parking is for a residential, live-work, or ((non-residential)) nonresidential use. In  
4 structures containing residential uses and also containing either ((non-residential)) nonresidential  
5 uses or live-work units, parking that is clearly set aside and reserved for residential or live-work  
6 use shall meet the standards of subsection 23.54.030.B.1. Parking for all other uses within the



1 structure shall meet the standards of subsection 23.54.030.B.2. All uses shall provide barrier-free  
2 accessible parking if required by the Seattle Building Code or the Seattle Residential Code.

3 1. Residential uses

4 a. When five or fewer parking spaces are provided, the minimum required  
5 size of a parking space shall be for a medium vehicle, as described in subsection 23.54.030.A.2,  
6 except as provided in subsection 23.54.030.B.1.d.

7 b. When more than five parking spaces are provided, a minimum of 60  
8 percent of the parking spaces shall be striped for medium vehicles. The minimum size for a  
9 medium parking space shall also be the maximum size. Forty percent of the parking spaces may  
10 be striped for any size category in subsection 23.54.030.A, provided that when parking spaces  
11 are striped for large vehicles, the minimum required aisle width shall be as shown for medium  
12 vehicles.

13 c. Assisted living facilities. Parking spaces shall be provided as in  
14 subsections 23.54.030.B.1.a and 23.54.030.B.1.b, except that a minimum of two spaces shall be  
15 striped for a large vehicle.

16 d. ~~((Townhouse units.))~~ For an individual garage serving ~~((a townhouse))~~  
17 an individual dwelling unit, the minimum required size of a parking space shall be for a medium  
18 vehicle, as described in subsection 23.54.030.A.

19 2. ~~((Non-residential))~~ Nonresidential uses

20 a. When ten or fewer parking spaces are provided, a maximum of 25  
21 percent of the parking spaces may be striped for small vehicles. A minimum of 75 percent of the  
22 spaces shall be striped for large vehicles.

1                   b. When between 11 and 19 parking spaces are provided, a minimum of  
2 25 percent of the parking spaces shall be striped for small vehicles. The minimum required size  
3 for these small parking spaces shall also be the maximum size. A maximum of 65 percent of the  
4 parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall  
5 be striped for large vehicles.

6                   c. When 20 or more parking spaces are provided, a minimum of 35  
7 percent of the parking spaces shall be striped for small vehicles. The minimum required size for  
8 small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking  
9 spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped  
10 for large vehicles.

11                   d. The minimum vehicle clearance shall be at least 6 feet 9 inches on at  
12 least one floor, and there shall be at least one direct entrance that is at least 6 feet 9 inches in  
13 height for all parking garages accessory to ~~((non-residential))~~ nonresidential uses and live-work  
14 units and for all flexible-use parking garages.

15                   3. Live-work uses. The first required parking space shall meet the parking  
16 standards for residential use. Additional required parking for a live-work use shall meet the  
17 parking standards for ~~((non-residential))~~ nonresidential use.

18                   C. Backing ~~((Distances))~~ distances and ~~((Moving Other Vehicles.))~~ moving other vehicles

19                   1. Adequate ingress to and egress from all parking spaces shall be provided  
20 without having to move another vehicle, except in the case of multiple spaces provided for a  
21 ~~((single-family))~~ dwelling unit ~~((or an accessory dwelling unit associated with a single-family~~  
22 ~~dwelling,))~~ or in the case of tandem parking authorized under ~~((Section))~~ subsection  
23 23.54.020.B.

1                   2. Except for lots with fewer than three parking spaces, ingress to and egress from  
2 all parking spaces shall be provided without requiring backing more than 50 feet.

3                   D. Driveways. Driveway requirements for residential and nonresidential uses are  
4 described below. When a driveway is used for both residential and nonresidential parking, it  
5 shall meet the standards for nonresidential uses described in subsection 23.54.030.D.2.

6                   1. Residential uses((~~-~~))

7                   a. Driveway width. Driveways less than 100 feet in length that serve 30 or  
8 fewer parking spaces shall be a minimum of 10 feet in width for one-way or two-way traffic.

9                   b. Except for driveways serving one (~~(single-family)~~) dwelling unit,  
10 driveways more than 100 feet in length that serve 30 or fewer parking spaces shall either:

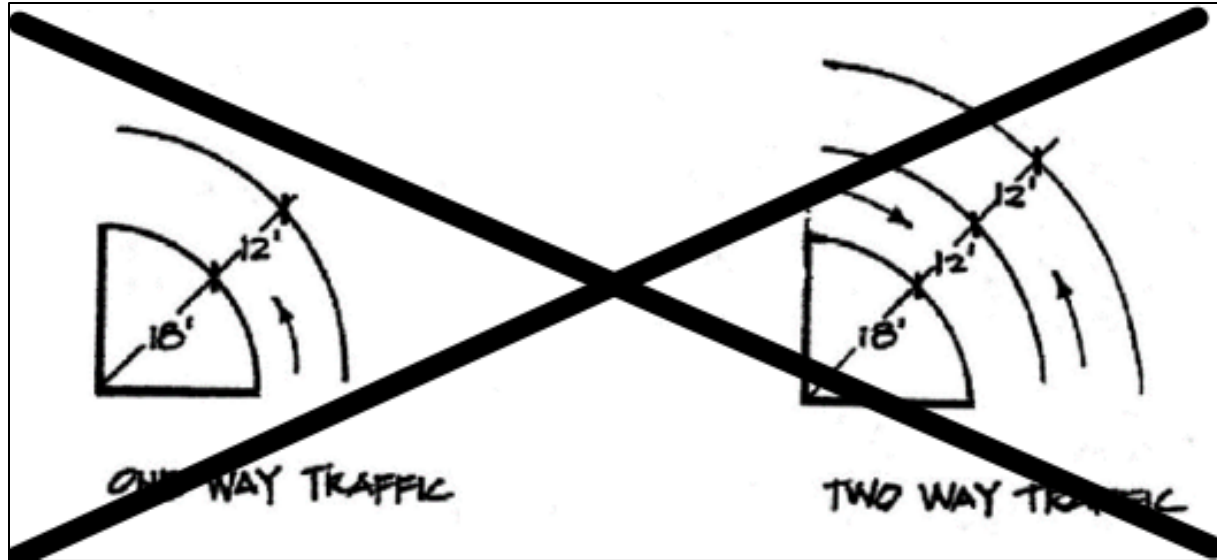
11                               1) (~~(be)~~) Be a minimum of 16 feet wide, tapered over a 20 foot  
12 distance to a 10 foot opening at the lot line; or

13                               2) (~~(be)~~) Be a minimum of 10 feet wide and provide a passing area  
14 at least 20 feet wide and 20 feet long. The passing area shall begin 20 feet from the lot line, with  
15 an appropriate taper to meet the 10-foot opening at the lot line. If a taper is provided at the other  
16 end of the passing area, it shall have a minimum length of 20 feet.

17                   c. Driveways of any length that serve more than 30 parking spaces shall be  
18 at least 10 feet wide for one-way traffic and at least 20 feet wide for two-way traffic.

19                   d. Driveways for two attached (~~(rowhouse or townhouse)~~) dwelling units  
20 may be paired so that there is a single curb cut providing access. The maximum width of the  
21 paired driveway is 18 feet.

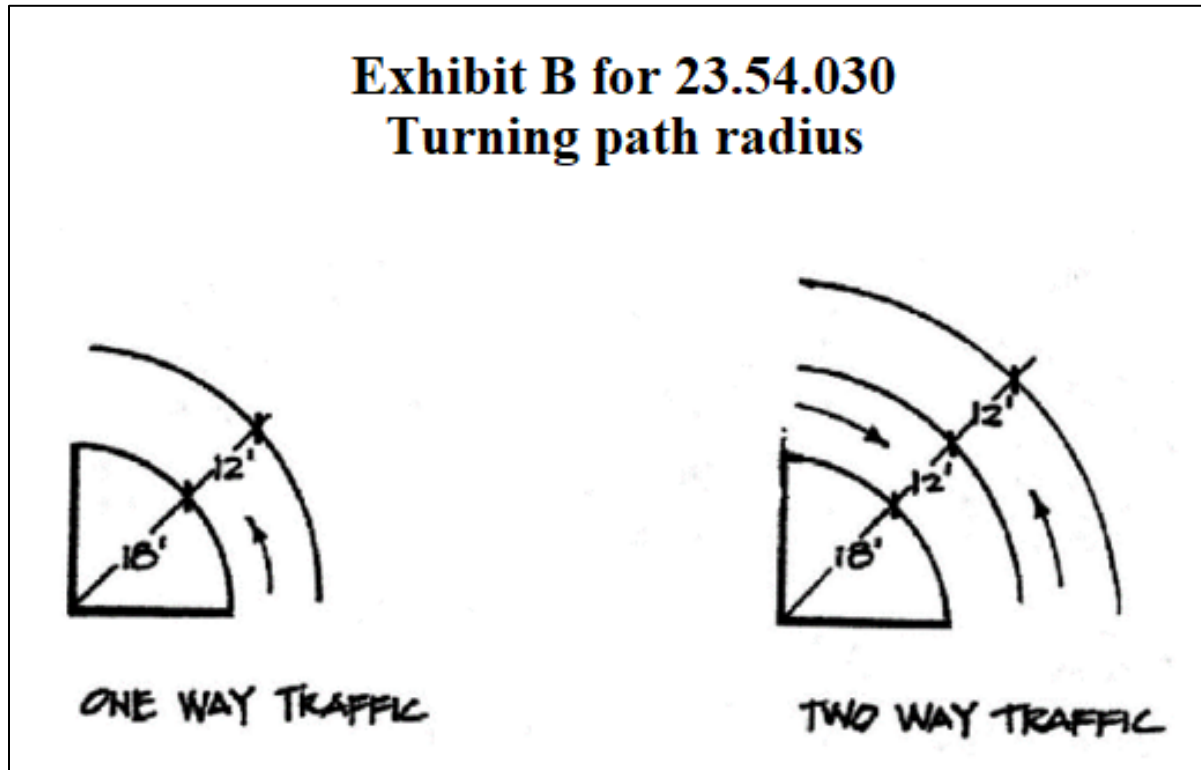
22                   e. Driveways with a turning radius of more than 35 degrees shall conform  
23 to the minimum turning path radius shown in Exhibit B for 23.54.030.



1  
2 ((Exhibit B for 23.54.030: Turning Path Radius))

3 Exhibit B for 23.54.030

4 Turning path radius



5  
6 f. Vehicles may back onto a street from a parking area serving five or  
7 fewer vehicles, provided that either:

1 1) The street is not an arterial as defined in Section 11.18.010; or

2 2) For a lot with one (~~(single-family)~~) dwelling unit or one parking  
3 space, the Director may permit backing onto an arterial based on a safety analysis that addresses  
4 visibility, traffic volume, and other relevant issues.

5 g. Nonconforming driveways. The number of parking spaces served by an  
6 existing driveway that does not meet the standards of this subsection 23.54.030.D.1 shall not be  
7 increased. This prohibition may be waived by the Director after consulting with the Director of  
8 the Seattle Department of Transportation, based on a safety analysis.

9 2. Nonresidential (~~(Uses-)~~) uses

10 a. Driveway (~~(Widths-)~~) widths

11 1) The minimum width of driveways for (~~(one-way)~~) one-way  
12 traffic shall be 12 feet and the maximum width shall be 15 feet.

13 2) The minimum width of driveways for (~~(two-way)~~) two-way  
14 traffic shall be 22 feet and the maximum width shall be 25 feet.

15 b. Driveways shall conform to the minimum turning path radius shown in  
16 Exhibit B for 23.54.030.

17 c. For driveways that provide access to a solid waste management use the  
18 Director may allow both a maximum driveway width greater than the limits set in subsection  
19 23.54.030.D.2.a and appropriate turning path radii, as determined necessary for truck  
20 maneuvering.

21 3. Driveway slope for all uses. No portion of a driveway, whether located on a lot  
22 or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection  
23 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of

1 the right-of-way to which the driveway connects, and to the proposed finished grade of the right-  
 2 of-way if it is different from the current grade. The ends of a driveway shall be adjusted to  
 3 accommodate an appropriate crest and sag. The Director may permit a driveway slope of more  
 4 than 15 percent if it is found that:

5 a. The topography or other special characteristic of the lot makes a 15  
 6 percent maximum driveway slope infeasible;

7 b. The additional amount of slope permitted is the least amount necessary  
 8 to accommodate the conditions of the lot; and

9 c. The driveway is still useable as access to the lot.

10 E. Parking aisles

11 1. Parking aisles shall be provided according to the requirements of Table  
 12 A for 23.54.030 and Exhibit C for 23.54.030.

<b><u>Table A for 23.54.030</u></b>						
<b><u>Parking aisle dimensions</u></b>						
<u>Parking angle</u>	<u>Stall width</u>	<u>Stall length (in feet)</u>	<u>Aisle width (in feet)<sup>1</sup></u>	<u>Curb depth per car (in feet)</u>	<u>Unit width (in feet)<sup>2</sup></u>	<u>Curb length per car (in feet)</u>
0°	<u>Small</u>	<u>18</u>	<u>10</u>	<u>7.5</u>	<u>25</u>	<u>18</u>
	<u>Medium</u>	<u>20</u>	<u>10</u>	<u>8</u>	<u>26</u>	<u>20</u>
	<u>Large</u>	<u>24</u>	<u>12</u>	<u>8</u>	<u>28</u>	<u>24</u>
45°	<u>Small</u>	<u>15</u>	<u>11</u>	<u>15.91</u>	<u>42.82</u>	<u>10.61</u>
	<u>Medium</u>	<u>16</u>	<u>13</u>	<u>16.97</u>	<u>46.94</u>	<u>11.3</u>
	<u>Large</u>	<u>19</u>	<u>13</u>	<u>19.09</u>	<u>51.18</u>	<u>11.3</u>
60°	<u>Small</u>	<u>15</u>	<u>13</u>	<u>16.74</u>	<u>46.48</u>	<u>8.66</u>
	<u>Medium</u>	<u>16</u>	<u>15</u>	<u>17.86</u>	<u>50.72</u>	<u>9.24</u>
	<u>Large</u>	<u>19</u>	<u>17.5</u>	<u>20.45</u>	<u>58.41</u>	<u>9.24</u>
75°	<u>Small</u>	<u>15</u>	<u>16.5</u>	<u>16.43</u>	<u>49.36</u>	<u>7.76</u>
	<u>Medium</u>	<u>16</u>	<u>18.5</u>	<u>17.52</u>	<u>53.55</u>	<u>8.25</u>
	<u>Large</u>	<u>19</u>	<u>20</u>	<u>20.42</u>	<u>60.84<sup>2</sup></u>	<u>8.25</u>
90°	<u>Small</u>	<u>15</u>	<u>20</u>	<u>15</u>	<u>50</u>	<u>7.5</u>
	<u>Medium</u>	<u>16</u>	<u>22</u>	<u>16</u>	<u>54</u>	<u>8</u>

**Table A for 23.54.030**  
**Parking aisle dimensions**

	Large	19	24 <sup>3</sup>	19	62 <sup>2</sup>	8
--	-------	----	-----------------	----	-----------------	---

Footnotes for Table A for 23.54.030

<sup>1</sup> Required aisle width is for one-way traffic only. If two-way traffic is proposed, then the minimum aisle width shall be 20 feet or greater.

<sup>2</sup> 60 feet may be substituted for required unit width on lots where the available width is in 60-foot whole multiples, provided that the minimum width of the parking stalls shall be 9 feet.

<sup>3</sup> For lots 44 feet in width or less, the Director may reduce the aisle width to as low as 20 feet if large parking spaces are provided at 90 degrees as long as the spaces are 9 feet wide.

1

A Parking Angle	B Stall Width	C Stall Length	D Aisle Width <sup>1</sup>	E Curb Depth Per Car	F Unit Width <sup>3</sup>	G Curb Length Per Car
0°	7.5	18.0	10.0	7.5	25.0	18.0
	8.0	20.0	10.0	8.0	26.0	20.0
	8.5	24.0	12.0	8.5	29.0	24.0
45°	7.5	15.0	11.0	15.91	42.82	10.61
	8.0	16.0	13.0	16.97	46.90	11.3
	8.5	19.0	13.0	19.44	51.88	12.02
60°	7.5	15.0	13.0	16.74	46.45	8.66
	8.0	16.0	15.0	17.86	50.72	9.24
	8.5	19.0	17.5	20.70	57.90	9.82
75°	7.5	15.0	16.5	16.43	49.36	7.76
	8.0	16.0	18.5	17.52	53.54	8.25
	8.5	19.0	20.0	20.55	61.10 <sup>2</sup>	8.80
90°	7.5	15.0	20.0	15.0	50.0	7.5
	8.0	16.0	22.0	16.0	54.0	8.0
	8.5	19.0 <sup>2</sup>	24.0 <sup>2</sup>	19.0	62.0 <sup>3</sup>	8.5

<sup>1</sup> Required aisle width is for one-way traffic only. If two-way traffic is proposed, then the minimum aisle width shall be 20 feet or greater.

<sup>2</sup> When lot width is less than 43 feet, 40 feet may be substituted for a two-way aisle and a single row of cars at 90° to the aisle, provided that the minimum width of the parking stalls shall be 9½ feet.

<sup>3</sup> 60 feet may be substituted for required unit width on lots where the available width is in 60-foot whole multiples, provided that the minimum width of the parking stalls shall be 9½ feet.

The following equations may be used to compute dimensions for parking angles other than those provided in the chart above:

$$E = C \sin A + B \cos A$$

$$G = B / \sin A$$

NOTE: Aisle widths shall be provided as required for the next greater parking angle shown in the chart above.

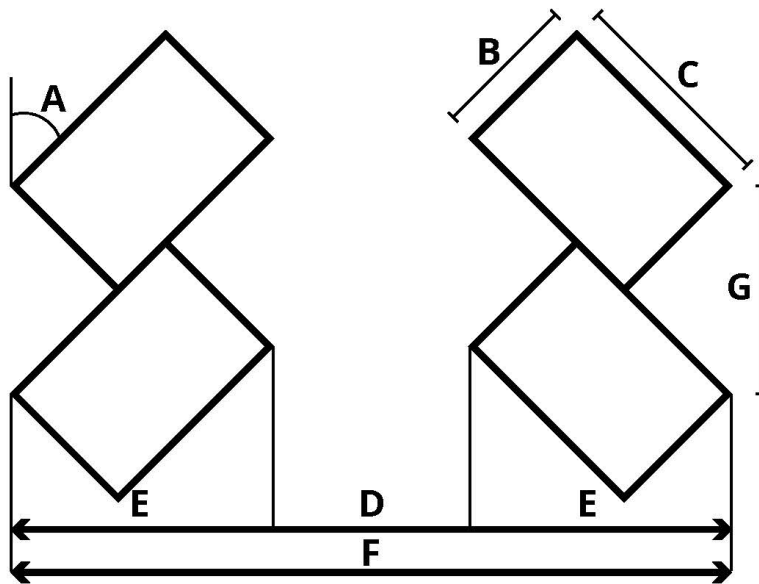
2

1 ((Exhibit C for 23.54.030: Parking Aisle Dimensions))

2 **Exhibit C for 23.54.030**

3 **Parking aisle dimension measurement**

**Exhibit C for 23.54.030**  
**Parking aisle dimension measurement**



The following equations may be used to compute dimensions for parking angles other than those provided in the chart above.

$$E = C \sin A + B \cos A$$
$$G = B / \sin A$$

Note: Aisle widths shall be provided as required for the next greater parking angle shown in the chart above.

4

5 2. Minimum aisle widths shall be provided for the largest vehicles served by the  
6 aisle.

7 3. Turning and maneuvering areas shall be located on private property, except that  
8 alleys may be credited as aisle space.

9 4. Aisle slope shall not exceed 17 percent provided that the Director may permit a  
10 greater slope if the criteria in subsections 23.54.030.D.3.a, 23.54.030.D.3.b, and 23.54.030.D.3.c  
11 are met.

12

\* \* \*



1            ~~((H))~~ F. ~~Attendant ((Parking)) parking.~~ In downtown zones, any off-street parking area or  
2 structure providing more than ~~((5))~~ five parking spaces where automobiles are parked solely by  
3 attendants employed for that purpose shall have parking spaces at least 8 feet in width, and 15  
4 feet in length. Subsections ~~((A, B, C, D and E of this Section 23.54.030))~~ 23.54.030.A,  
5 23.54.030.B, 23.54.030.C, 23.54.030.D, and 23.54.030.E shall not apply, except that the grade  
6 curvature of any area used for automobile travel or storage shall not exceed that specified in  
7 subsection 23.54.030.D.3. Should attendant operation be discontinued, the provisions of  
8 subsections ~~((23.54.030 A, B, C, D and E))~~ 23.54.030.A, 23.54.030.B, 2054.030.C, 23.54.030.D,  
9 and 23.54.030.E shall apply to the parking.

10            ~~((I))~~ G. ~~Off-street ((Bus Parking)) bus parking.~~ Bus parking spaces, when required, shall  
11 be 13 feet in width and 40 feet in length. Buses parked ~~((en masse))~~ together shall not be  
12 required to have adequate ingress and egress from each parking space.

13            ~~((J))~~ H. The Director may, as a Type I decision, modify any required dimension or  
14 distribution percentage of parking spaces identified in subsections 23.54.030.A or 23.54.030.B to  
15 allow more efficient use of a surface parking area or parking garage, when the parking area or  
16 parking garage provides adequate and safe circulation.

17            \* \* \*

18            Section 63. Subsections 23.54.030.F and 23.54.030.G of the Seattle Municipal Code,  
19 which section was last amended by Ordinance 127099, are amended as follows:

20            \* \* \*

21            **23.54.031 Curb cuts**

22            ~~((F. Curb cuts.))~~ The number of permitted curb cuts is determined by whether the parking served  
23 by the curb cut is for residential or nonresidential use, and by the zone in which the use is

1 located. If a curb cut is used for more than one use or for one or more live-work units, the  
2 requirements for the use with the largest curb cut requirements shall apply.

3 ~~((1.))~~ A. Residential uses

4 ~~((a.))~~ 1. Number of curb cuts

5 ~~((1.))~~ a. For lots not located on a principal arterial as designated by the  
6 Seattle Department of Transportation, curb cuts are permitted according to Table A for  
7 ~~((23.54.030))~~ 23.54.031:

<b>Table A for <del>((23.54.030))</del> <u>23.54.031</u> Curb cuts for lots not located on a principal arterial or easement frontage</b>	
<b>Street or easement frontage of the lot</b>	<b>Number of curb cuts permitted</b>
80 feet or less	1
Greater than 80 feet up to 160 feet	2
Greater than 160 feet up to 240 feet	3
Greater than 240 feet up to 320 feet	4
For lots with frontage in excess of 320 feet, the pattern established above continues.	

8 ~~((2.))~~ b. For lots on principal arterials as designated by the Seattle  
9 Department of Transportation, curb cuts are permitted according to Table B for ~~((23.54.030))~~  
10 23.54.031:

<b>Table B for <del>((23.54.030))</del> <u>23.54.031</u> Curb cuts for principal arterial street frontage</b>	
<b>Street or easement frontage of the lot</b>	<b>Number of curb cuts permitted</b>
160 feet or less	1

**Table B for ((23.54.030)) 23.54.031  
Curb cuts for principal arterial street frontage**

Street or easement frontage of the lot	Number of curb cuts permitted
Greater than 160 feet up to 320 feet	2
Greater than 320 feet up to 480 feet	3
For lots with street frontage in excess of 480 feet, the pattern established above continues.	

1                           ((3)) c. On a lot that has both principal arterial and non-principal arterial  
2 street frontage, the total number of curb cuts on the principal arterial is calculated using only the  
3 length of the street lot line on the principal arterial.

4                           ((4)) d. If two adjoining lots share a common driveway, the combined  
5 frontage of the two lots will be considered as one in determining the maximum number of  
6 permitted curb cuts.

7                           ((b.)) 2. Curb cut width. Curb cuts shall not exceed a maximum width of 10 feet  
8 except that:

9                           ((4)) a. For lots on principal arterials as designated by the Seattle  
10 Department of Transportation, the maximum curb cut width is 23 feet;

11                           ((2)) b. One curb cut greater than 10 feet but in no case greater than 20  
12 feet in width may be substituted for each two curb cuts permitted by subsection

13 ((23.54.030.F.1.a)) 23.54.031.A.1;

14                           ((3)) c. A greater width may be specifically permitted by the development  
15 standards in a zone;

16                           ((4)) d. If subsection 23.54.030.D requires a driveway greater than 10 feet  
17 in width, the curb cut may be as wide as the required width of the driveway; and

1                            ~~((5))~~ e. A curb cut may be less than the maximum width permitted but  
2 shall be at least as wide as the minimum required width of the driveway it serves.

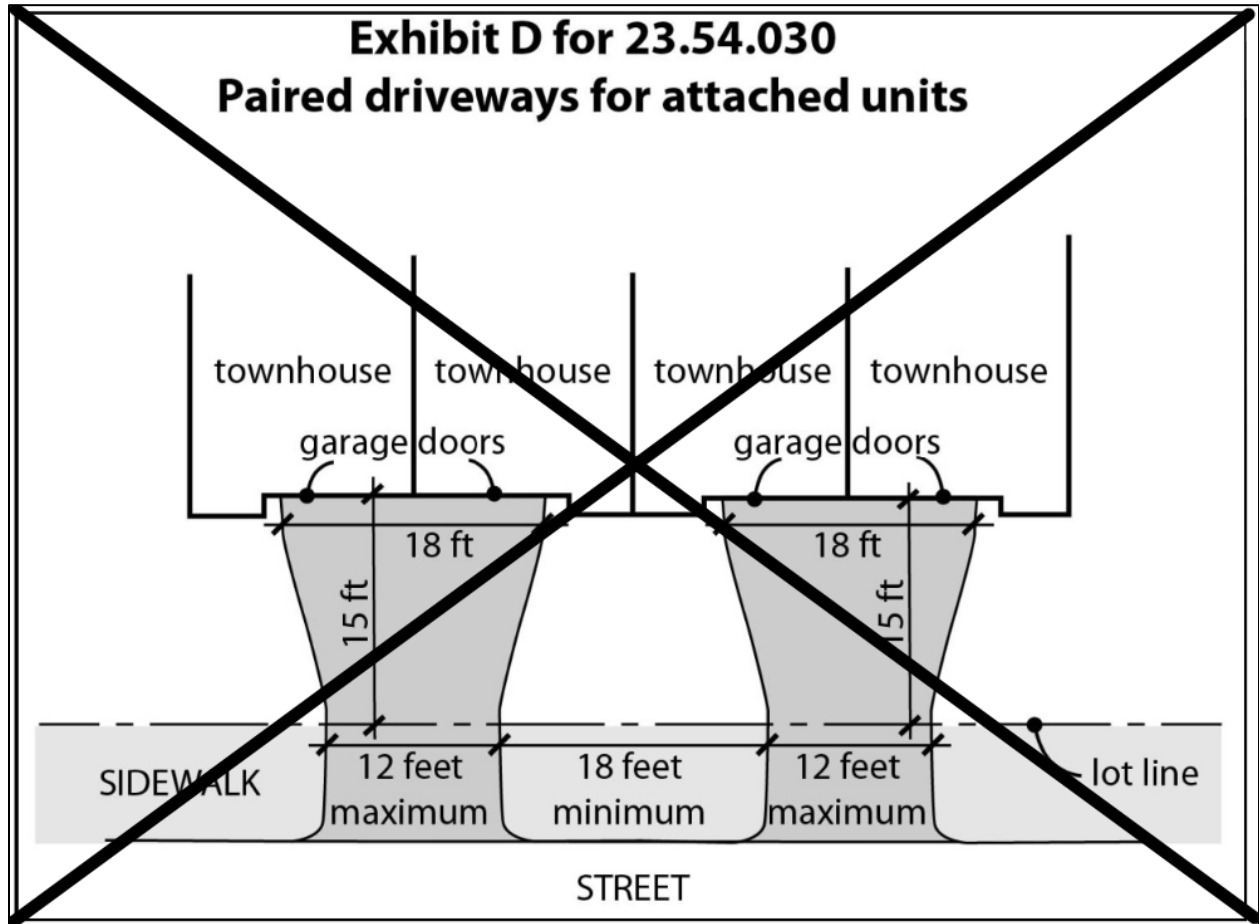
3                            ~~((e-))~~ 3. Distance between curb cuts

4                            ~~((1))~~ a. The minimum distance between any two curb cuts located on a lot  
5 is 30 feet, except as provided in subsection ~~((23.54.030.F.1.e.2))~~ 23.54.031.A.3.b.

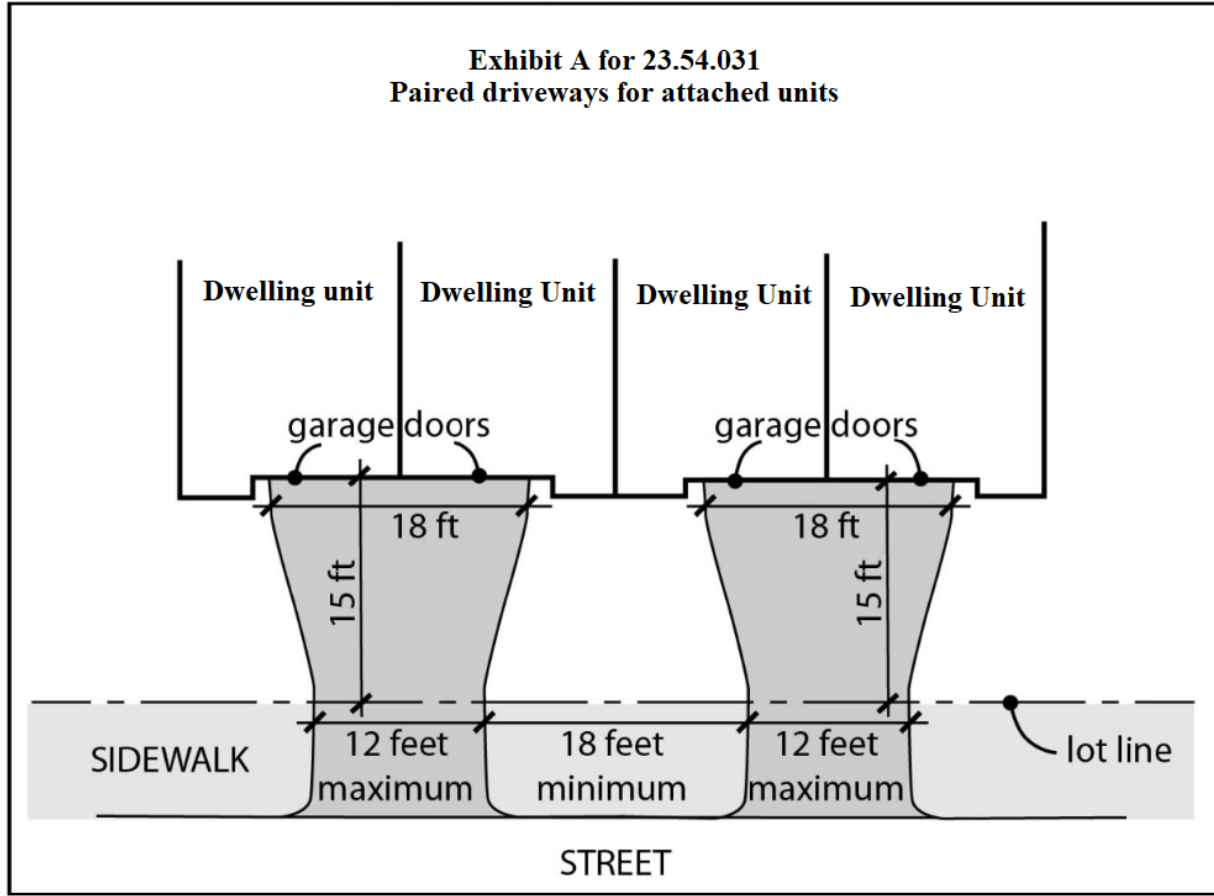
6                            ~~((2))~~ b. For ~~((rowhouse and townhouse developments))~~ attached dwelling  
7 units, the minimum distance between curb cuts is 18 feet (See Exhibit ~~((D for 23.54.030))~~ A for  
8 23.54.031). For ~~((rowhouse and townhouse developments))~~ attached dwelling units located on  
9 abutting lots, the minimum distance between curb cuts is 18 feet.

1 **Exhibit (~~D for 23.54.030~~) A for 23.54.031**

2 **Paired driveways for attached units**



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~~((2-))~~ B. Nonresidential uses in all zones except industrial zones

~~((a-))~~ 1. Number of curb cuts

~~((4))~~ a. In all residential zones, RC zones, and within the Major

Institution Overlay District, two-way curb cuts are permitted according to Table C for

~~((23.54.030))~~ 23.54.031:

**Table C for ~~((23.54.030))~~ 23.54.031**

**Number of curb cuts in residential zones, RC zones, and the Major Institution Overlay District**

Street frontage of the lot	Number of curb cuts permitted
80 feet or less	1

**Table C for ~~((23.54.030)) 23.54.031~~  
Number of curb cuts in residential zones, RC zones, and the Major Institution  
Overlay District**

<b>Street frontage of the lot</b>	<b>Number of curb cuts permitted</b>
Greater than 80 feet up to 240 feet	2
Greater than 240 feet up to 360 feet	3
Greater than 360 feet up to 480 feet	4
For lots with frontage in excess of 480 feet, one curb cut is permitted for every 120 feet of street frontage.	

1                                    ~~((2))~~ b. The Director may allow two one-way curb cuts to be substituted  
2 for one two-way curb cut, after determining, as a Type I decision, that there would not be a  
3 significant conflict with pedestrian traffic.

4                                    ~~((3))~~ c. The Director shall, as a Type I decision, determine the number  
5 and location of curb cuts in C1 and C2 zones and the location of curb cuts in SM zones.

6                                    ~~((4))~~ d. In downtown zones, a maximum of two curb cuts for one-way  
7 traffic at least 40 feet apart, or one curb cut for two-way traffic, are permitted on each street front  
8 where access is permitted by subsection 23.49.019.H. No curb cut shall be located within 40 feet  
9 of an intersection. These standards may be modified by the Director as a Type I decision on lots  
10 with steep slopes or other special conditions, to the minimum extent necessary to provide  
11 vehicular and pedestrian safety and facilitate a smooth flow of traffic.

12                                    ~~((5))~~ e. For public schools, the Director shall permit, as a Type I decision,  
13 the minimum number of curb cuts that the Director determines is necessary.

1                                    ~~((6))~~ f. In NC zones, curb cuts shall be provided according to subsection  
2 23.47A.032.A, or, when subsection 23.47A.032.A does not specify the maximum number of  
3 curb cuts, according to subsection (~~(23.54.030.F.2.a.1)~~) 23.54.031.B.1.a.

4                                    ~~((7))~~ g. For police and fire stations the Director shall permit the minimum  
5 number of curb cuts that the Director determines is necessary to provide adequate  
6 maneuverability for emergency vehicles and access to the lot for passenger vehicles.

7                                    ~~((b-))~~ 2. Curb cut widths

8                                    ~~((1))~~ a. For one-way traffic, the minimum width of curb cuts is 12 feet,  
9 and the maximum width is 15 feet.

10                                   ~~((2))~~ b. For two-way traffic, the minimum width of curb cuts is 22 feet,  
11 and the maximum width is 25 feet, except that the maximum width may be increased to 30 feet if  
12 truck and auto access are combined.

13                                   ~~((3))~~ c. For public schools, the maximum width of a curb cut is 25 feet.  
14 Development standard departures may be granted or required pursuant to the procedures and  
15 criteria set forth in Chapter 23.79.

16                                   ~~((4))~~ d. For fire and police stations, the Director may allow curb cuts up  
17 to, and no wider than, the minimum width necessary to provide access for official emergency  
18 vehicles that have limited maneuverability and that must rapidly respond to emergencies. Curb  
19 cuts for fire and police stations are considered curb cuts for two-way traffic.

20                                   ~~((5))~~ e. If one of the following conditions applies, the Director may  
21 require a curb cut of up to 30 feet in width, if it is found that a wider curb cut is necessary for  
22 safe access:





1           ~~((4.))~~ D. Curb cuts for access easements

2                   ~~((a.))~~ 1. If a lot is crossed by an access easement serving other lots, the curb cut  
3 serving the easement may be as wide as the easement roadway.

4                   ~~((b.))~~ 2. The curb cut serving an access easement shall not be counted against the  
5 number or amount of curb cuts permitted to a lot if the lot is not itself served by the easement.

6           ~~((5.))~~ E. Curb cut flare. A flare with a maximum width of 2.5 feet is permitted on either  
7 side of curb cuts in any zone.

8           ~~((6.))~~ F. Replacement of unused curb cuts. When a curb cut is no longer needed to  
9 provide access to a lot, the curb and any planting strip must be replaced.

10           ~~((7.))~~ G. Curb cuts are not allowed on streets if alley access to a lot is feasible but has not  
11 been provided.

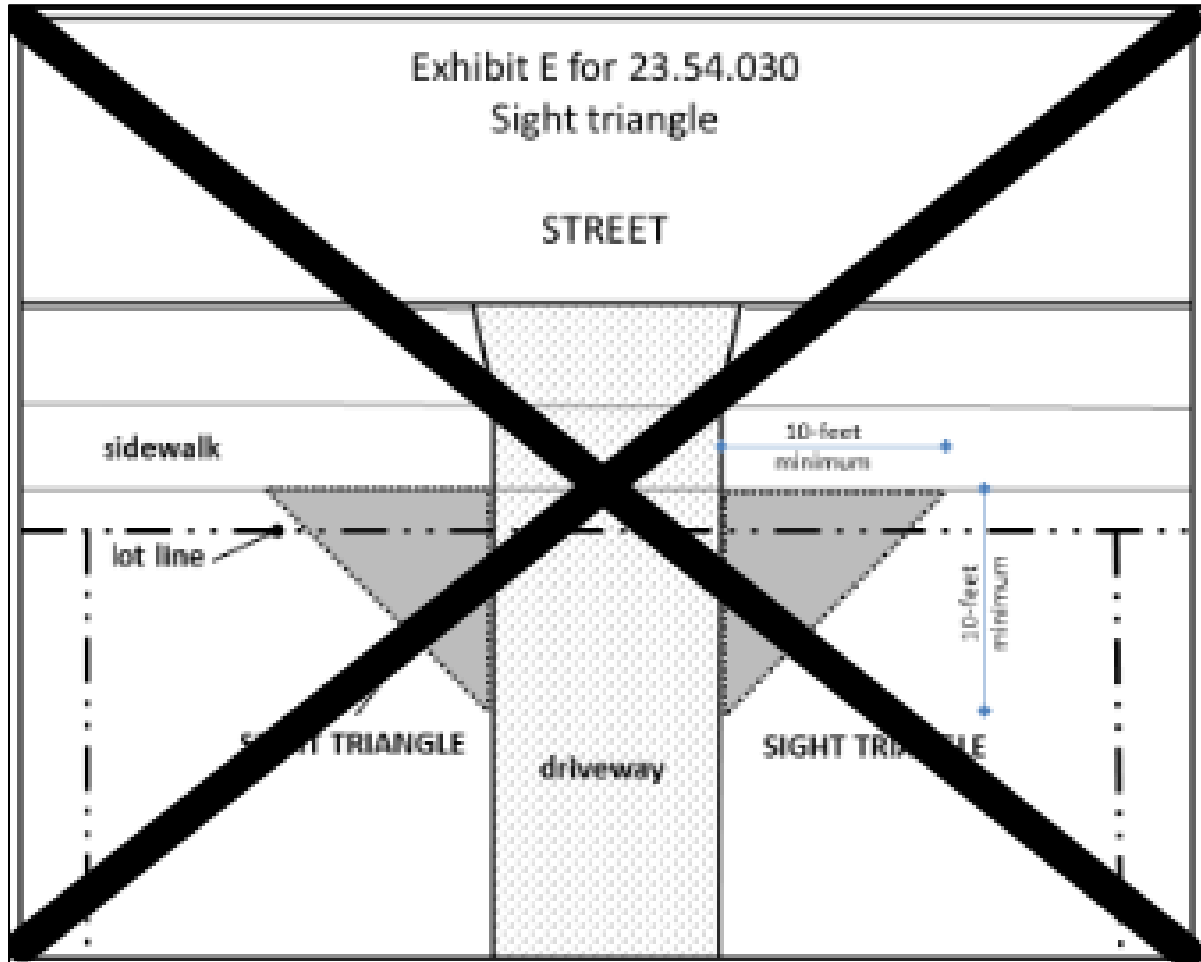
12 **23.54.032 Sight triangles**

13           ~~((G. Sight triangle~~

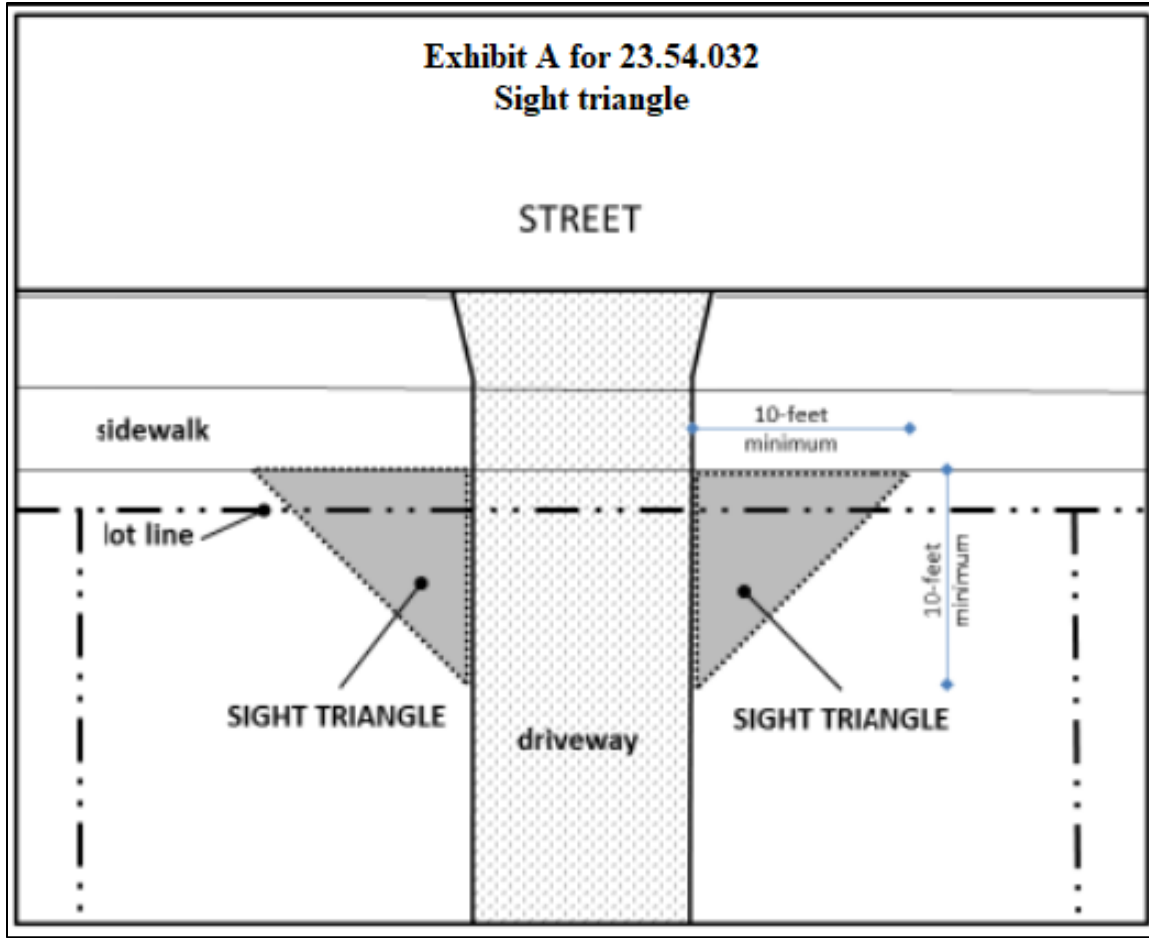
14           ~~4.))~~ A. For exit-only driveways and easements, and two-way driveways and easements  
15 less than 22 feet wide, a sight triangle on both sides of the driveway or easement shall be  
16 provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection  
17 of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is  
18 no sidewalk, as depicted in Exhibit ~~((E for 23.54.030))~~ A for 23.54.032.

1 **Exhibit ((~~E~~ for 23.54.030)) A for 23.54.032**

2 **Sight triangle**



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~~((2.))~~ B. For two-way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk. The entrance and exit lanes shall be clearly identified.

~~((3.))~~ C. The sight triangle shall also be kept clear of obstructions in the vertical spaces between 32 inches and 82 inches from the ground.

~~((4.))~~ D. When the driveway or easement is less than 10 feet from the lot line, the sight triangle may be provided as follows:

1                   ((a-)) 1. An easement may be provided sufficient to maintain the sight triangle.

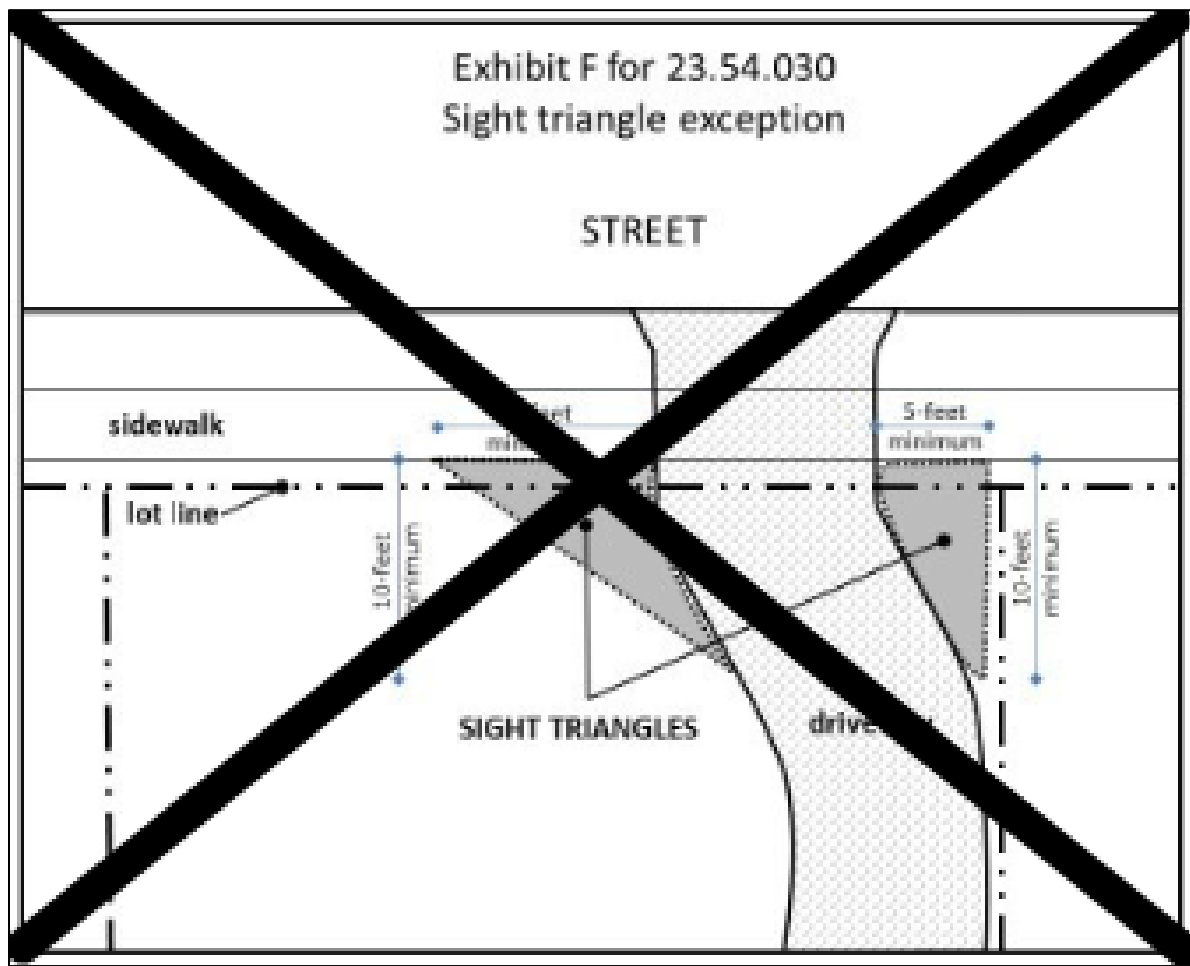
2                   The easement shall be recorded with the King County Recorder's Office; or

3                   ((b-)) 2. The driveway may be shared with a driveway on the neighboring lot; or

4                   ((c-)) 3. The driveway or easement may begin 5 feet from the lot line, as depicted  
5                   in Exhibit ((F for 23.54.030)) B for 23.54.032.

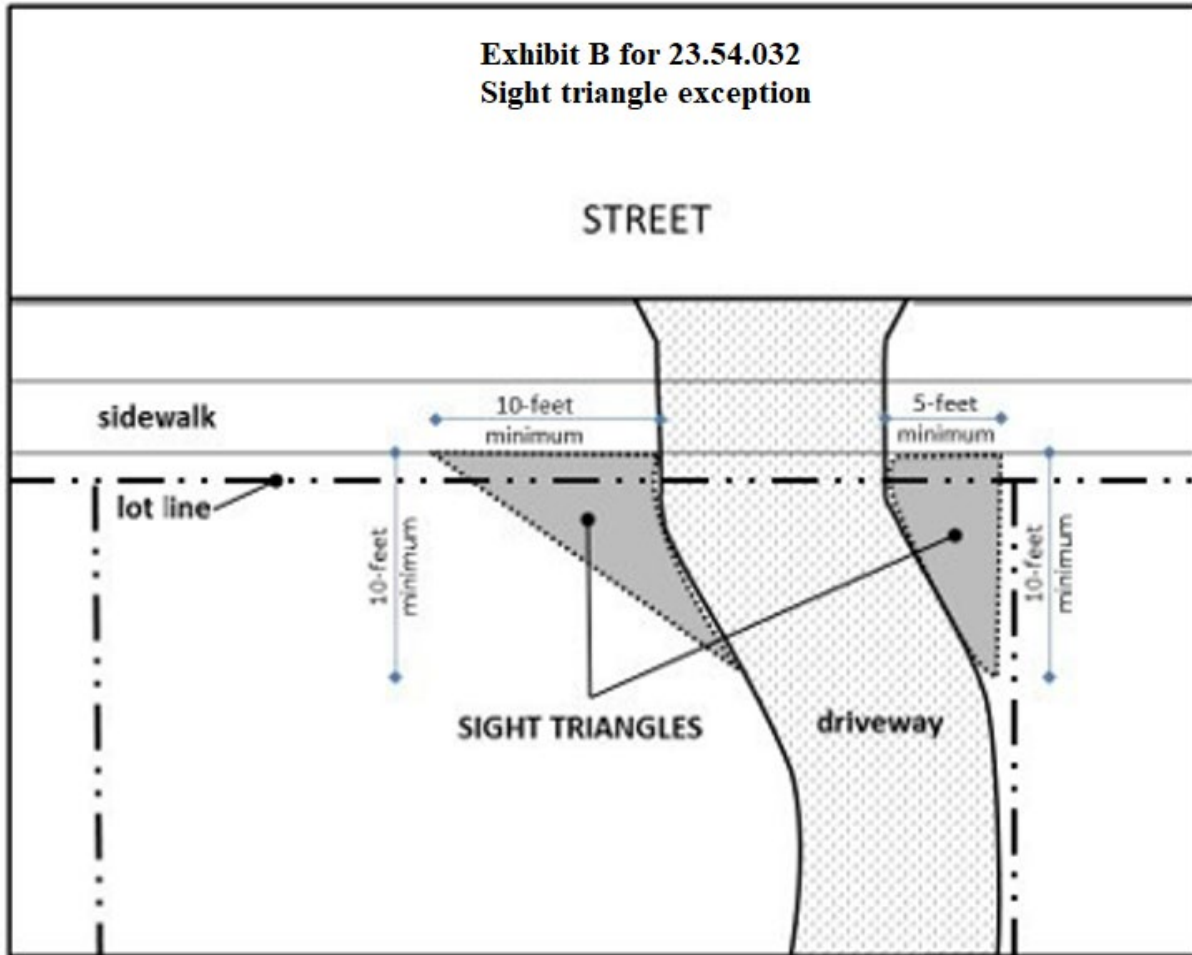
6                   **Exhibit ((F for 23.54.030)) B for 23.54.032**

7                   **Sight triangle exception**



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2        ~~((5.))~~ E. An exception to the sight triangle requirement may be made for driveways  
3 serving lots containing only residential uses and fewer than three parking spaces, when providing  
4 the sight triangle would be impractical.

5        ~~((6.))~~ F. In all ~~((Downtown, Industrial,))~~ downtown, industrial, Commercial 1, and  
6 Commercial 2 zones, the sight triangle at a garage exit may be provided by mirrors and/or other  
7 approved safety measures.

8        ~~((7.))~~ G. Sight triangles are not required for one-way entrances into a parking garage or  
9 surface parking area.



1 separate from any parking facilities serving other units, at least one parking space for each unit in  
2 that garage, carport, or surface parking area shall be EV-ready.

3 ~~((b.))~~ 2. Surface parking for multiple ~~((residences))~~ dwelling units. When parking  
4 for ~~((multifamily residential uses))~~ multiple dwelling units is provided in a surface parking area  
5 serving multiple ~~((residences))~~ dwelling units, the number of parking spaces that shall be EV-  
6 ready shall be as follows:

7 ~~((1) When between one and six parking spaces are provided, each~~  
8 ~~of those parking spaces shall be EV-ready;~~

9 ~~2) When between seven and 25 parking spaces are provided, a~~  
10 ~~minimum of six of those parking spaces shall be EV-ready; and~~

11 ~~3) When more than 25 parking spaces are provided, a minimum of~~  
12 ~~20 percent of those parking spaces shall be EV-ready.))~~

13 a. When up to 25 parking spaces are provided, the first 12 parking spaces  
14 shall be EV-ready; and

15 b. When more than 25 parking spaces are provided, 45 percent of all  
16 parking spaces shall be EV-ready.

17 ~~((c.))~~ 3. Parking garages for multiple ~~((residences))~~ dwelling units. When parking  
18 for ~~((multifamily residential uses))~~ multiple dwelling units is provided in a parking garage  
19 serving multiple ~~((residences))~~ dwelling units, a minimum of ~~((20))~~ 45 percent of those parking  
20 spaces shall be EV-ready.

21 ~~((d. Other residential uses. When parking is provided for all other~~  
22 ~~residential uses, a minimum of 20 percent of those spaces shall be EV-ready.~~

23 ~~2.))~~ B. Nonresidential uses~~((:))~~



1                    1. When parking is provided for nonresidential uses, a minimum of ~~((ten))~~ 30  
2 percent of those spaces shall be EV-ready((-)), except as provided in subsection 23.54.034.B.2  
3 and subsection 23.54.034.B.3.

4                    2. For the uses listed in subsection 23.54.034.B.3, the following requirements  
5 apply:

6                    a. Where fewer than ten parking spaces are provided for the use, one EV-  
7 ready space is required.

8                    b. Where ten or more parking spaces are provided for the use, 10 percent  
9 of parking spaces shall be EV-ready.

10                   3. The following uses are subject to the alternative requirements in 23.54.034.B.2:

11                   a. The following institutional uses:

12                         1. Community club or center;

13                         2. Child care center;

14                         3. Community farm;

15                         4. Library;

16                         5. Museum;

17                         6. Private club;

18                         7. Religious facility; and

19                         8. School, elementary or secondary;

20                   b. Entertainment uses;

21                   c. Eating and drinking establishments;

22                   d. Automotive sales and services;

23                   e. Multipurpose retail sales;

1                            f. Heavy sales and services, except heavy commercial services; and

2                            g. Marine sales and services.

3                    ~~((3.))~~ C. Rounding. When calculating the number of required EV-ready parking spaces,  
4 any fraction or portion of an EV-ready parking space required shall be rounded up to the nearest  
5 whole number.

6                    ~~((4.))~~ D. Reductions

7                            ~~((a.))~~ 1. The Director may, in consultation with the Director of Seattle City Light,  
8 reduce the requirements of this ~~((subsection 23.54.030.L))~~ Section 23.54.034 as a Type I  
9 decision ~~((where))~~ if there is substantial evidence ~~((substantiating))~~ that the added electrical load  
10 that can be attributed to meeting the requirements will:

11                            ~~((1.))~~ a. Alter the local utility infrastructure design requirements on the  
12 utility side of the legal point of service, so as to require on-property power transformation; or

13                            ~~((2.))~~ b. Require an upgrade to an existing residential electrical service.

14                            ~~((b.))~~ 2. In cases where the provisions of subsection ~~((23.54.030.L.4.a))~~  
15 23.54.034.D.1 have been met, the maximum quantity of EV charging infrastructure required to  
16 be installed shall be reduced to the maximum service size that would not require the changes to  
17 transformation or electrical service in subsection ~~((23.54.030.L.4.a))~~ 23.54.034.D.1. The Director  
18 may first reduce the required level of EV infrastructure at EV-ready parking spaces from 40-amp  
19 to 20-amp circuits. If necessary, the Director may also then reduce the number of required EV-  
20 ready parking spaces or otherwise reduce the level of EV infrastructure at EV-ready parking  
21 spaces.

22                            ~~((e.))~~ 3. The Director may establish by rule the procedures and documentation  
23 required for a reduction request.

1           ~~((5-))~~ E. All EV charging infrastructure shall be installed in accordance with the Seattle  
2 Electrical Code. Where EV-ready surface parking spaces are located more than 4 feet from a  
3 building, raceways shall be extended to a pull box or stub in the vicinity of the designated space  
4 and shall be protected from vehicles.

5           ~~((6-))~~ E. Accessible parking. Where new EV-ready parking spaces and new accessible  
6 parking are both provided, parking facilities shall be designed so that at least ~~((one))~~ 20 percent  
7 of the accessible parking spaces or two accessible parking spaces, whichever is greater, shall be  
8 EV-ready. The accessible parking EV-ready infrastructure may also serve adjacent parking  
9 spaces not designated as accessible parking. The EV-ready accessible parking spaces, rounded  
10 up to the next whole number, are allowed to be included in the total number of electric vehicle  
11 parking spaces required under 23.54.034.A. and 23.54.034.B.

12           ~~((7-))~~ G. Nothing in this ~~((subsection 23.54.030.L))~~ Section 23.54.034 shall be construed  
13 to modify the minimum number of off-street motor vehicle parking spaces required for specific  
14 uses or the maximum number of parking spaces allowed, as set forth in Section 23.54.015 or  
15 elsewhere in this Title 23.

16           ~~((8-))~~ H. This Section ~~((23.54.030))~~ 23.54.034 does not require EV supply equipment, as  
17 defined by Article 100 of the Seattle Electrical Code, to be installed.

18           Section 65. Section 23.58C.040 of the Seattle Municipal Code, last amended by  
19 Ordinance 126855, is amended as follows:

20 **23.58C.040 Affordable Housing – Payment option**

21           A. Payment amount

22                   1. An applicant complying with this Chapter 23.58C through the payment option  
23 shall provide a cash contribution to the City, calculated by multiplying the payment calculation

1 amount per square foot according to Table A or Table B for 23.58C.040 and Map A for  
2 23.58C.050, as applicable, by the total gross floor area in the development, excluding the floor  
3 area of parking located in stories, or portions of stories, that are underground, excluding the floor  
4 area contained in any accessory dwelling units, and excluding any floor area devoted to a  
5 domestic violence shelter, as follows:

6 a. In the case of construction of a new structure, the gross floor area in  
7 residential use and the gross floor area of live-work units;

8 b. In the case of construction of an addition to an existing structure that  
9 results in an increase in the total number of units within the structure, the gross floor area in  
10 residential use and the gross floor area of live-work units in the addition;

11 c. In the case of alterations within an existing structure that result in an  
12 increase in the total number of units within the structure, the gross floor area calculated by  
13 dividing the total gross floor area in residential use and gross floor area of live-work units by the  
14 total number of units in the proposed development, and multiplying that quotient by the net  
15 increase in units in the development;

16 d. In the case of change of use that results in an increase in the total  
17 number of units, the gross floor area that changed to residential use or live-work units; or

18 e. Any combination of the above.

<b>Table A for 23.58C.040</b>	
<b>Payment calculation amounts:</b>	
<b>In Downtown, SM-SLU, SM-U 85, and SM-NG zones</b>	
<b>Zone</b>	<b>Payment calculation amount per square foot</b>
DH1/45	Not applicable
DH2/55	Not applicable
DH2/75	\$16.85
DH2/85	Not applicable
DMC 75	\$16.85

DMC 85/75-170	\$27.42
DMC 95	\$16.85
DMC 145	\$15.95
DMC 170	\$7.27
DMC 240/290-440	\$10.90
DMC 340/290-440	\$10.90
DMR/C 75/75-95	\$27.42
DMR/C 75/75-170	\$27.42
DMR/C 95/75	\$16.85
DMR/C 145/75	\$13.53
DMR/C 280/125	\$15.95
DMR/R 95/65	\$16.85
DMR/R 145/65	\$13.53
DMR/R 280/65	\$15.95
DOC1 U/450-U	\$15.86
DOC2 500/300-550	\$13.55
DRC 85-170	\$13.22
IDM-65-150	Not applicable
IDM-75-85	Not applicable
IDM 85/85-170	\$27.42
IDM 165/85-170	\$27.42
All IDR and IDR/C zones	\$27.42
PMM-85	Not applicable
All PSM zones	Not applicable
SM-NG 145	\$16.04
SM-NG 240	\$24.21
SM-SLU 85/65-160	Not applicable
SM-SLU 85-280	\$13.22
SM-SLU 100/95	\$9.91
SM-SLU 100/65-145	\$10.24
SM-SLU 145	\$10.24
SM-SLU 175/85-280	\$13.22
SM-SLU 240/125-440	\$13.22
SM-SLU/R 65/95	\$16.85
SM-U 85	\$17.51

1

<b>Table B for 23.58C.040</b>			
<b>Payment calculation amounts:</b>			
<b>Outside Downtown, SM-SLU, SM-U 85, and SM-NG zones</b>			
<b>Zone</b>	<b>Payment calculation amount per square foot</b>		
	<b>Low</b>	<b>Medium</b>	<b>High</b>
Zones with an (M) suffix	\$9.25	\$17.25	\$27.42

Zones with an (M1) suffix	\$14.87	\$26.43	\$39.31
Zones with an (M2) suffix	\$16.52	\$29.40	\$43.28

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2. Automatic adjustments to payment amounts. The amounts for payment calculations according to Table A and Table B for 23.58C.040 shall automatically adjust annually on March 1, starting in 2024, by an amount in proportion to the increase, if any, for January 1 through December 31 of the prior calendar year, in the Consumer Price Index, All Urban Consumers, Seattle-Tacoma-Bellevue, WA, Shelter (1982-84=100), as determined by the U.S. Department of Labor, Bureau of Labor Statistics, or successor index.

\* \* \*

Section 66. Section 23.60A.156 of the Seattle Municipal Code, last amended by Ordinance 124750, is amended as follows:

**23.60A.156 Standards for environmentally critical areas in the Shoreline District**

\* \* \*

K. Subdivisions and short subdivisions

1. The standards for short subdivisions and subdivisions in Section 25.09.240 incorporated by reference into this Chapter 23.60A apply to short subdivisions and subdivisions in the Shoreline District, except as provided in subsections 23.60A.156.K.2 and 23.60A.156.K.3.

2. Subsection 25.09.240.B does not apply. Parcels shall be divided so that each lot contains an area for the principal structure, all accessory structures, and necessary walkways and access for this area that are outside the riparian corridor, wetlands, wetland buffers, and steep slope areas and buffers, except as follows:

a. Development on upland lots may be located on steep slope areas that have been created through previous legal grading activities, including rockeries or retaining

1 walls resulting from rights-of-way improvements, if steep slope erosion is not increased as  
2 determined by the Director based on a geotechnical report; and

3                   b. Development on upland lots may be located on steep slope areas that  
4 are less than 20 feet in vertical rise and that are 30 feet or more from other steep slope areas, if  
5 steep slope erosion is not increased as determined by the Director based on a geotechnical report.

6                   3. Subsection (~~(25.09.240.E)~~) 25.09.240.D does not apply. In computing the  
7 number of lots a parcel in a single-family zone may contain, the Director shall exclude easements  
8 and/or fee simple property used for shared vehicular access to proposed lots that are required  
9 under Section 23.53.005.

10                   L. (~~Environmentally critical areas administrative conditional use. The provisions of~~  
11 ~~Section 25.09.260 do not apply in the Shoreline District.~~) Reserved.

12   \* \* \*

13                   Section 67. Subsection 23.66.030.D of the Seattle Municipal Code, which section was  
14 last amended by Ordinance 126760, is amended as follows:

15 **23.66.030 Certificates of Approval – Application, review, and appeals**

16   \* \* \*

17                   D. Review

18                   1. Review when no special review board is established

19                   a. When there is no special review board, the Department of  
20 Neighborhoods Director shall, within 30 days of a determination that an application for a  
21 certificate of approval is complete, determine whether the proposed action is consistent with the  
22 use and development standards for the district and shall, within 15 additional days, issue, issue  
23 with conditions, or deny the requested certificate of approval.

1                   b. A copy of the Department of Neighborhoods Director's decision shall  
2 be sent to the Director and mailed to the owner and the applicant at the addresses provided in the  
3 application. Notice of the Director's decision also shall be provided to any person who, prior to  
4 the rendering of the decision, made a written request to receive notice of the decision or  
5 submitted written substantive comments on the application.

6                   2. Review when special review board is established

7                   a. When a special review board has been established, the board shall hold  
8 a public meeting to receive comments on certificate of approval applications.

9                   b. Notice of the board's public meeting shall be posted in two prominent  
10 locations in the district at least three days prior to the meeting.

11                  c. The board, after reviewing the application and considering the  
12 information received at the public meeting, shall make a written recommendation to the  
13 Department of Neighborhoods Director to grant, grant with conditions, or deny the certificate of  
14 approval application based upon the consistency of the proposed action with the requirements of  
15 this Chapter 23.66, the district use and development standards, and the purposes for creating the  
16 district. The board shall make its recommendation within 30 days of the receipt of a completed  
17 application by the board staff, except that the applicant may waive the deadlines in writing for  
18 the special review board to make a recommendation or the Director of the Department of  
19 Neighborhoods to make a decision, if the applicant also waives any deadlines on the review or  
20 issuance of related permits that are under review by the Seattle Department of Construction and  
21 Inspections.



1                                   d. The Department of Neighborhoods Director shall, within 15 days of  
2 receiving the board's recommendation, issue or deny a certificate of approval or issue an  
3 approval with conditions.

4                                   e. A copy of the decision shall be sent to the Director and mailed to the  
5 owner and the applicant at the addresses provided in the application. Notice of the decision shall  
6 be provided to any person who, prior to the rendering of the decision, made a written request for  
7 notice of the decision, or submitted substantive written comments on the application.

8                                   3. Notwithstanding any contrary provision of Section 23.66.020 or Title 23,  
9 applications for certificates of approval for the following items shall be subject to the process in  
10 subsection 23.66.030.D.1 rather than the process in subsection 23.66.030.D.2:

11                                   a. The installation, removal, or alteration of: fire escapes, ducts, conduits,  
12 HVAC vents, grilles, pipes, panels (including photovoltaic panels), weatherheads, wiring,  
13 meters, utility connections, downspouts and gutters, or other similar mechanical, electrical, or  
14 telecommunication elements necessary for the normal operation of the site, building, or structure.

15                                   b. Installation, removal, or alteration(~~(, or removal)~~) of minor  
16 communication utility equipment on rooftops or streetlight poles, when the location does not  
17 have impacts on other historic resources and otherwise complies with the City Design Guidelines  
18 for minor communication utilities.

19                                   c. Installation, removal, or alteration of exterior light fixtures, exterior  
20 security lighting, ~~((and))~~ or security system equipment.

21                                   d. Installation, removal, or alteration of exterior or interior signage.

22                                   e. Installation, removal, or alteration of awnings or canopies.

1 f. Installation, removal, or alteration(~~(, or removal)~~) of window treatments,  
2 including but not limited to blinds, curtains, shades, or window film.

3 g. Alterations to storefront systems, if the proposed alterations are  
4 sympathetic to and do not destroy historic building materials.

5 h. Alteration to exterior paint colors and other finishes when painting a  
6 previously painted or otherwise finished material.

7 i. Installation, removal, or alteration of the following landscape elements:  
8 shrubs; perennials; annuals; and similar low-lying plantings.

9 j. Installation, removal, or alteration of the following site furnishings:  
10 benches; movable tables and seating; movable planters; movable water features; trash/recycling  
11 receptacles; and bike racks.

12 k. Installation, removal, or alteration of fences, gates, and barriers.

13 l. Right-of-way alterations, including but not limited to alterations to  
14 sidewalks, curbs, and the roadway.

15 m. Repaving and restriping of existing asphalt paved areas not within  
16 public rights-of-way.

17 n. Installation of improvements for accessibility compliance.

18 o. Installation, removal, or alteration of fire and life safety equipment.

19 p. Temporary emergency alterations, if the proposed replacement material  
20 used is compatible with the historic building fabric.

21 q. Change of use, establishment of a new use, or expansion of use, if use is  
22 a preferred use per Chapter 23.66 or applicable district rules.

1                                    r. Replacement of non-original doors and windows within original  
2 openings, when the design intent is consistent with the Secretary of the Interior's Standards for  
3 Rehabilitation.

4                                    s. Revisions to a previously approved (~~Certificate~~) certificate of  
5 (~~Approval~~) approval, where the design revisions are sympathetic to and do not destroy historic  
6 building materials.

7                                    t. Alterations or changes to accommodate seismic improvements.

8                                    4. A decision denying a certificate of approval shall state the specific reasons for  
9 the denial and explain why the proposed changes are inconsistent with the requirements of this  
10 Subchapter I and adopted use and development standards for the district.

11                                    5. Essential public facilities. No certificate of approval may be denied or  
12 conditioned in a manner that would preclude the siting of an essential public facility as provided  
13 in Chapter 23.80.

14                                    \* \* \*

15                                    Section 68. Section 23.72.008 of the Seattle Municipal Code, last amended by Ordinance  
16 124378, is amended as follows:

17 **23.72.008 Uses permitted in specified areas within the Sand Point Overlay District**

18                                    \* \* \*

19                                    B. Uses (~~Permitted Within Portions~~) permitted within portions of Subarea B depicted on  
20 Map A for 23.72.008(~~(-)~~)

21                                    1. Principal (~~Uses Permitted Outright~~) uses permitted outright. In addition to the  
22 principal uses permitted by the provisions of Section (~~23.44.006~~) 23.44.020, the following

1 principal uses are permitted outright in Subarea B as depicted on Map A for 23.72.004, subject to  
2 subsection ((B4:)) 23.72.008.B.4:

- 3 a. Custom and craft work;
- 4 b. Dry boat storage, limited to storage of non-motorized, hand-launchable  
5 boats such as kayaks, canoes, and sail boats;
- 6 c. Indoor and outdoor sports and recreation;
- 7 d. Institutions, except hospitals;
- 8 e. Lecture and meeting halls;
- 9 f. Motion picture theater not to exceed 500 seats within Building 47;
- 10 g. Offices, limited to a total of 86,000 gross square feet in the entire  
11 subarea;
- 12 h. Performing arts theaters;
- 13 i. Research and development laboratories;
- 14 j. Restaurants without drive-in lanes, limited to no more than 2,500 square  
15 feet per business establishment;
- 16 k. Storage of fleet vehicles including accessory service and repair;
- 17 l. Warehouses; and
- 18 m. General retail sales and service, up to 6,000 square feet per business  
19 establishment.

20 2. Accessory ((Uses)) uses. Accessory uses that meet the following standards and  
21 that are customarily incidental to the principal uses permitted outright, are permitted outright:

- 22 a. The area devoted to the accessory use is limited no more than 20  
23 percent of the gross floor area of the principal use it serves;

1 b. Only principal uses permitted by this ((~~section~~)) Section 23.72.008 and  
2 by the applicable provisions of Chapter 23.60A are allowed as accessory uses.

3 3. When not in use as a motion picture studio, a structure with an established use  
4 as a motion picture studio as of July 18, 1997, may be used for indoor and outdoor sports and  
5 recreation.

6 4. Any area not occupied by structures in existence as of July 18, 1997, paved  
7 parking areas in existence as of July 18, 1997, or rights-of-way in existence as of July 18, 1997,  
8 is limited to open space, dry boat storage or recreation uses.

9 \* \* \*

10 Section 69. Section 23.75.180 of the Seattle Municipal Code, last amended by Ordinance  
11 124843, is amended as follows:

12 **23.75.180 Parking**

13 A. Parking is regulated by this Section 23.75.180 and not by Sections 23.54.015((~~7~~)) or  
14 23.54.016((~~7~~)) or subsections 23.54.030.A((~~7~~)) or 23.54.030.B, except for bicycle parking, which  
15 is required pursuant to ((~~subsection 23.54.015.K~~)) Section 23.54.037. Parking maximums in this  
16 Section 23.75.180 do not include parking for dwelling units existing as of January 1, 2012, so  
17 long as those units exist.

18 B. There is no minimum requirement for parking spaces for motor vehicles. Maximum  
19 motor vehicle parking space limits are as follows:

20 1. For the NW Sector, parking shall not exceed 1,350 spaces, plus 0.7  
21 spaces per dwelling unit or live-work unit in the sector, except that up to an additional 450  
22 parking spaces may be permitted as a special exception pursuant to Chapter 23.76. When  
23 deciding whether to grant a special exception, the Director shall consider evidence of parking

1 demand for nonresidential uses and alternative means of transportation, including but not limited  
2 to the following:

- 3 a. Whether the additional parking will substantially encourage the use of  
4 single occupancy vehicles;
- 5 b. Characteristics of the work force and employee hours, such as multiple  
6 shifts that end when transit service is not readily available;
- 7 c. Proximity of transit lines to the lot and headway times of those lines;
- 8 d. Whether the additional parking will adversely affect vehicular and  
9 pedestrian circulation in the area; and
- 10 e. Potential for shared use of additional parking as residential or short-term  
11 parking.

12 2. For the NE, SE, and SW Sectors, Table A for 23.75.180 establishes maximum  
13 parking allowed based on the uses on a lot, subject to any transfer of unused parking allowance  
14 between lots in the same sector under Section 23.75.040.

15 ~~((Table A for 23.75.180~~

16 ~~Maximum motor vehicle parking limits for NE, SE, and SW Sectors))~~

**Table A for 23.75.180**

**Maximum motor vehicle parking limits for NE, SE, and SW sectors**

<b>Use</b>	<b>Maximum parking allowed <sup>1</sup></b>
Residential	0.7 spaces/dwelling unit or live-work unit <sup>2</sup>
Office	1 space/1,000 square feet of gross floor area
All other uses	1 space/500 square feet of gross floor area

Footnote to Table A for 23.75.180

<sup>1</sup> Based on the development of one or more uses on the lot where the parking is located, subject to any transfer of unused allowance between lots in the same sector under Section 23.75.040.

<sup>2</sup> One additional space beyond this maximum limit shall be allowed for each dwelling unit with ((3)) three or more bedrooms.

17

1 C. Barrier-free parking is required consistent with Seattle Building Code requirements.

2 D. For purposes of this Section 23.75.180, all parking is classified as "surface parking,"  
3 as defined in Section 23.84A.030, or as "aboveground," "partially underground," or  
4 "underground," as shown in Exhibit A for 23.75.180 and described as follows:

5 1. "Aboveground parking" means any portion of a parking garage where:

6 a. ~~((the))~~ The structure projects more than 4 feet in height above finished  
7 grade within 30 feet of a build-to line or reduced setback area; or

8 b. ~~((the))~~ The structure projects more than 6 feet in height above finished  
9 grade in any other location.

10 2. "Partially underground parking" means any portion of a parking garage where:

11 a. ~~((the))~~ The structure projects 4 feet or less in height above finished  
12 grade within 30 feet of a build-to line or reduced setback area; or

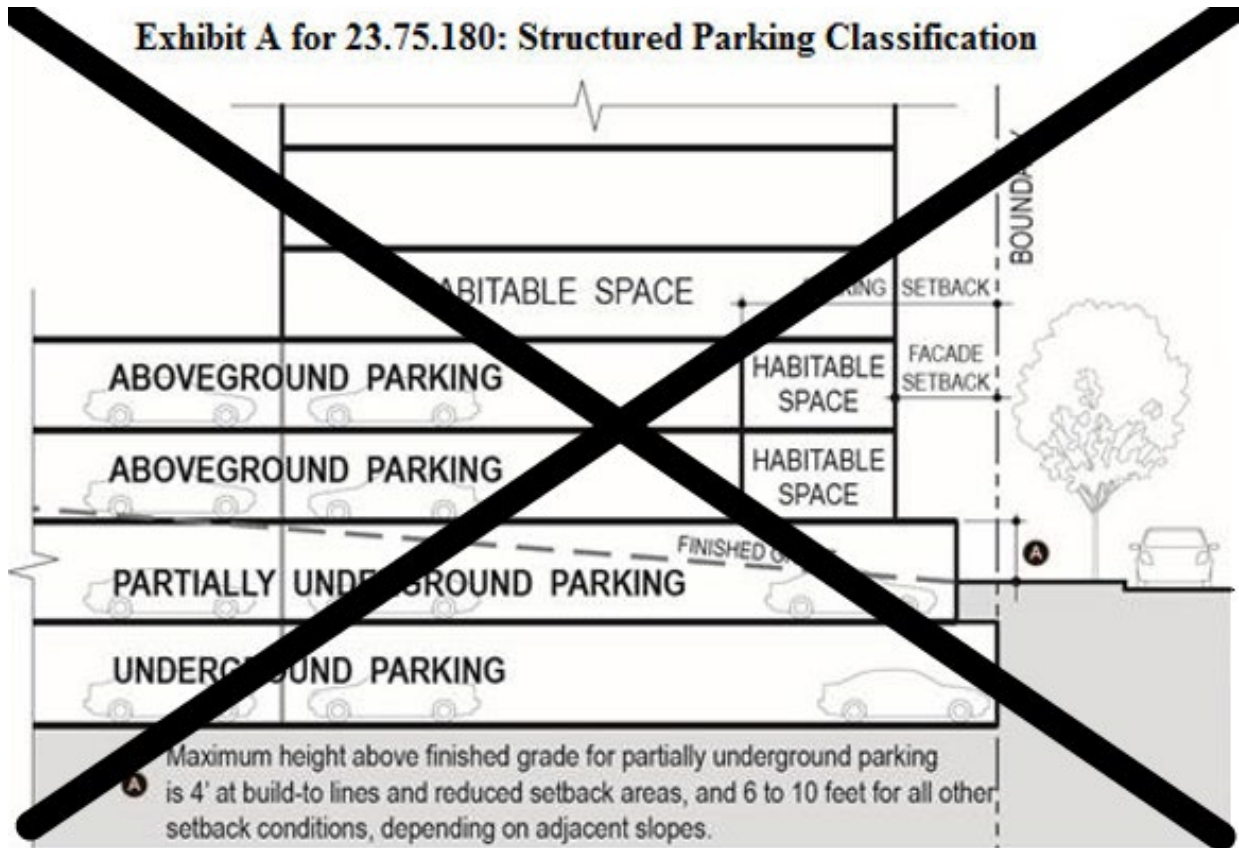
13 b. ~~((the))~~ The structure projects 6 feet or less in height above finished  
14 grade along any other location where the grade along the boundary has a slope of less than ~~((6))~~  
15 six percent; or

16 c. ~~((the))~~ The structure projects 10 feet or less in height above finished  
17 grade along any other location where the grade along the boundary has a slope of ~~((6))~~ six  
18 percent or greater.

19 3. "Underground parking" means a story of parking garage where all floor area,  
20 walls, and ceiling structure are entirely below finished grade, excluding access.

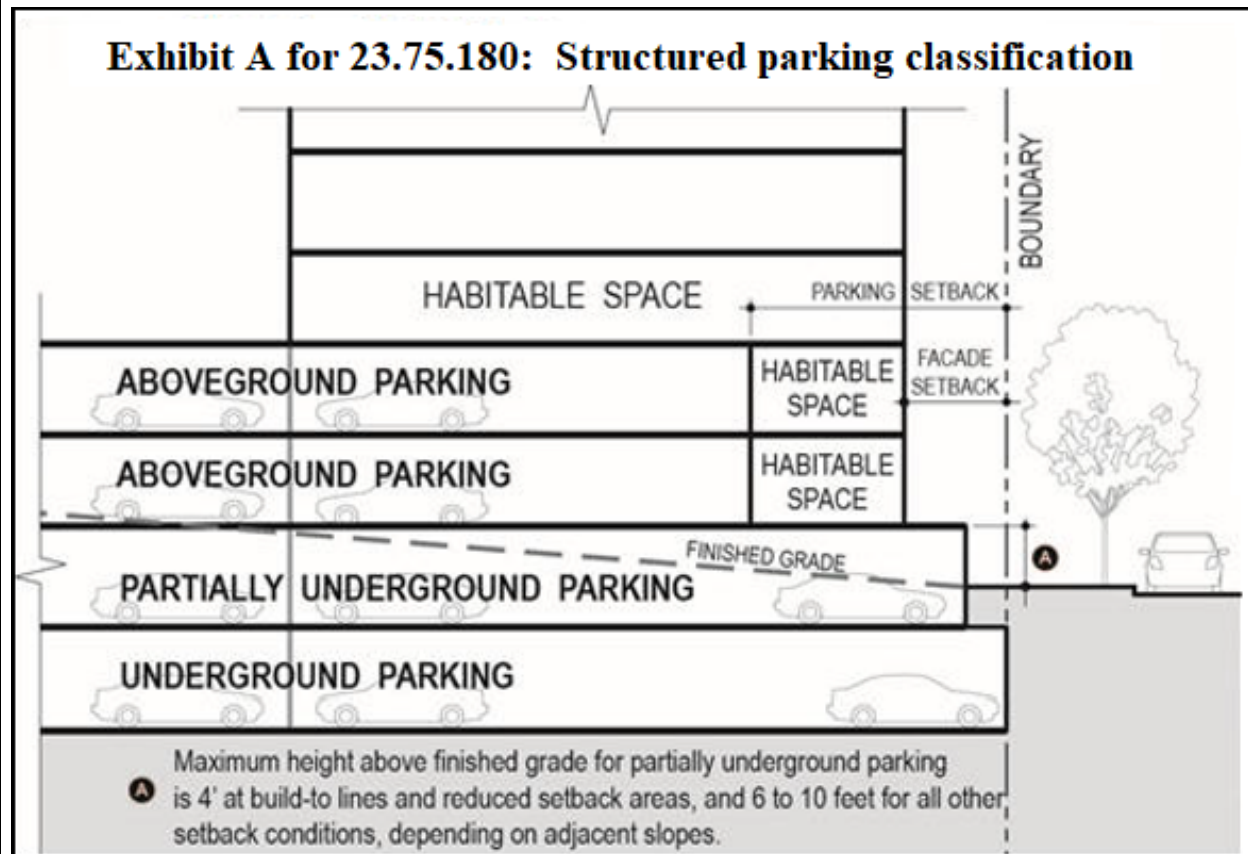
1 **Exhibit A for 23.75.180**

2 **Structured ((~~Parking Classification~~)) parking classification**



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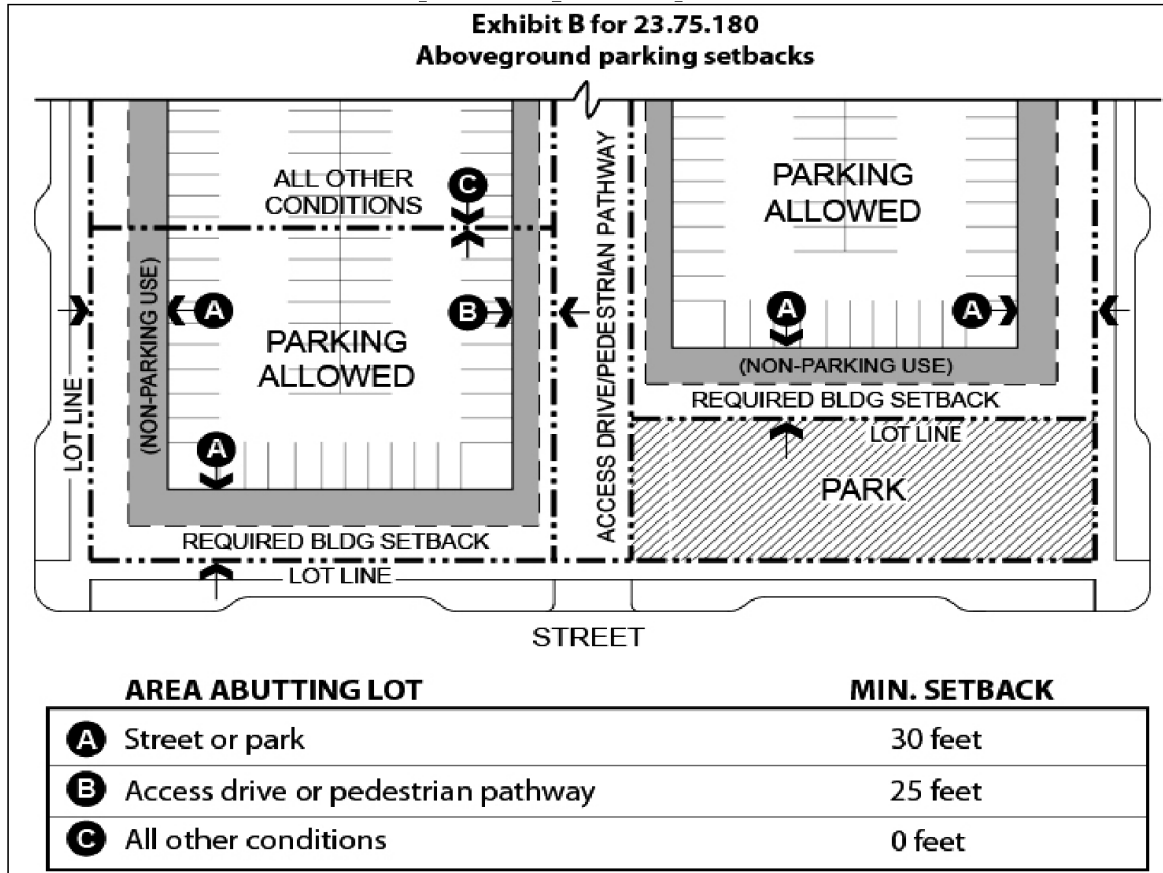
\* \* \*

F. Aboveground parking is subject to the following requirements((-)) :

1. Minimum setbacks for aboveground parking are established in Exhibit B for 23.75.180. No parking setbacks are required from lot lines abutting the Interstate 5 right-of-way.

1 **Exhibit B for 23.75.180**

2 **Aboveground parking setbacks**



3

4           2. Parking within 50 feet of a street, park that is open to the public, access drive,  
 5 or pedestrian pathway may not exceed three levels of aboveground parking.

6           3. Aboveground parking and loading areas shall be separated from each regulated  
 7 facade by a normally occupied use along at least 80 percent of the width of the regulated  
 8 facade, except where parking access and/or loading access occurs. The remaining part of the (~~façade~~)  
 9 facade shall include architectural detailing, artwork, vegetated walls, or other landscape features,  
 10 with an opaque screen at least 3.5 feet high on each story.

11           4. If aboveground parking or an aboveground loading area abuts any (~~façade~~)  
 12 facade other than a regulated (~~façade~~) facade, that (~~façade~~) facade shall be enhanced with

1 architectural detailing, artwork, vegetated walls, or other landscape features. Each story shall  
2 have an opaque screen at least 3.5 feet high.

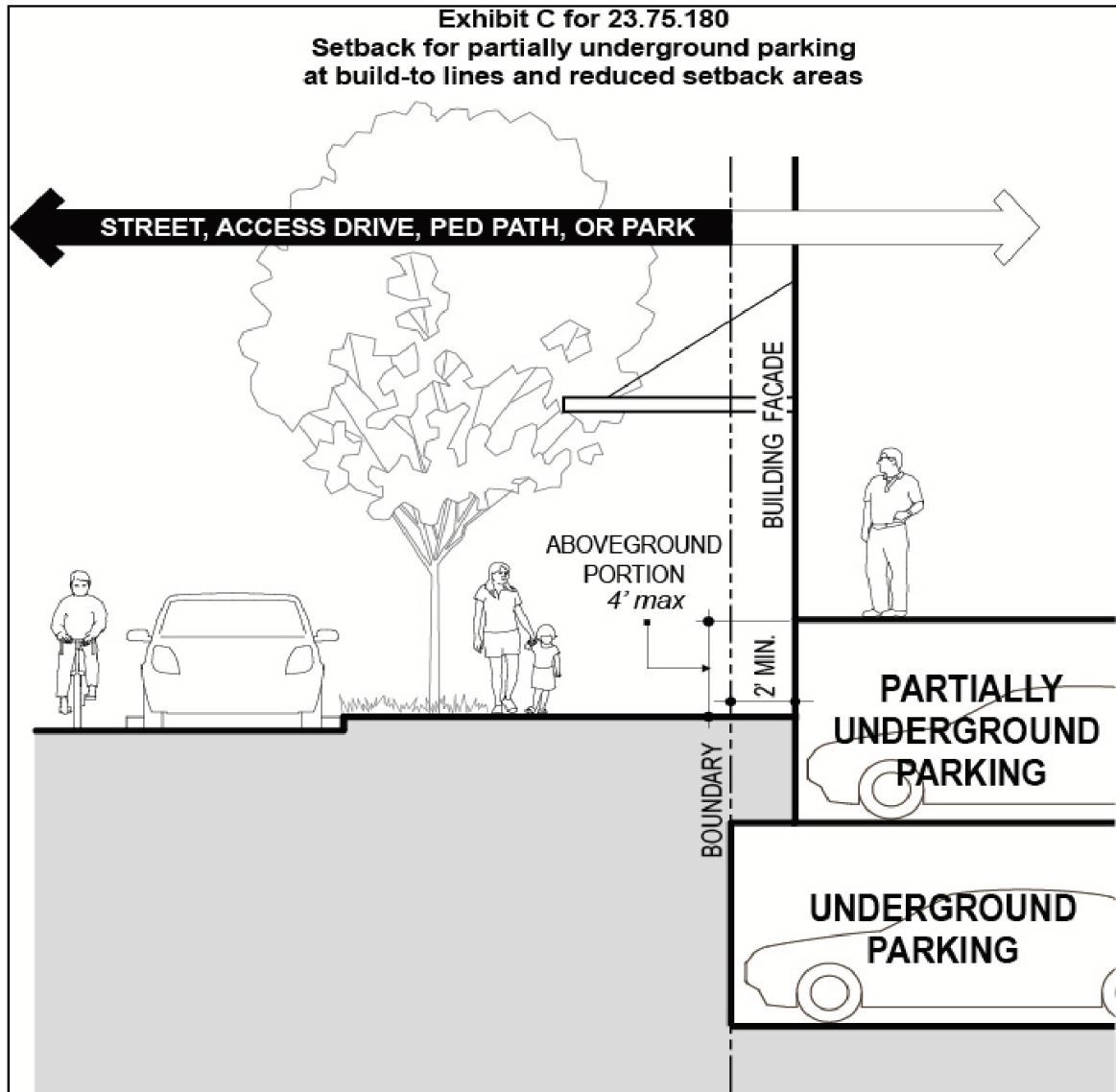
3 G. Partially underground parking is subject to the following requirements:

4 1. At build-to lines and in reduced setback areas as depicted in Exhibit C for  
5 23.75.140, partially underground parking is required to be set back at least 2 feet from the  
6 boundary, as shown in Exhibit C for 23.75.180. In these locations, the aboveground portion of  
7 the parking garage is not allowed to exceed 4 feet above finished grade.

8 2. Along boundaries that do not abut a street, park that is open to the public,  
9 pedestrian pathway, or access drive, no setback is required for partially underground parking.

1 **Exhibit C for 23.75.180**

2 **Setback for partially underground parking at build-to lines and reduced setback areas**



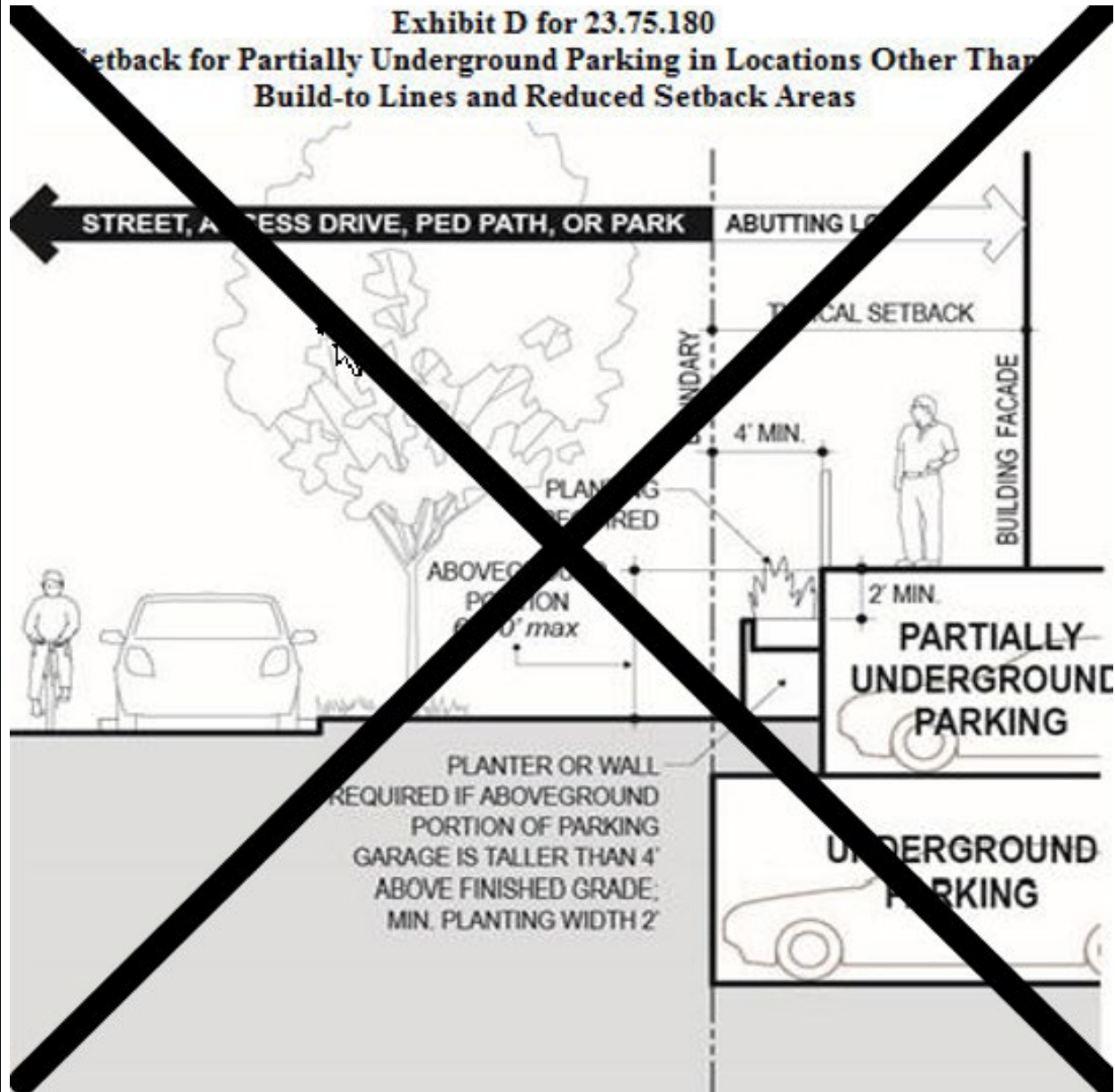
3

4 3. Along boundaries that abut a street, park that is open to the public, pedestrian  
5 pathway, or access drive and are not subject to a build-to line or reduced setback area, partially  
6 underground parking is required to be set back at least 4 feet from the boundary, as shown in  
7 Exhibit D for 23.75.180, and must meet the following standards:

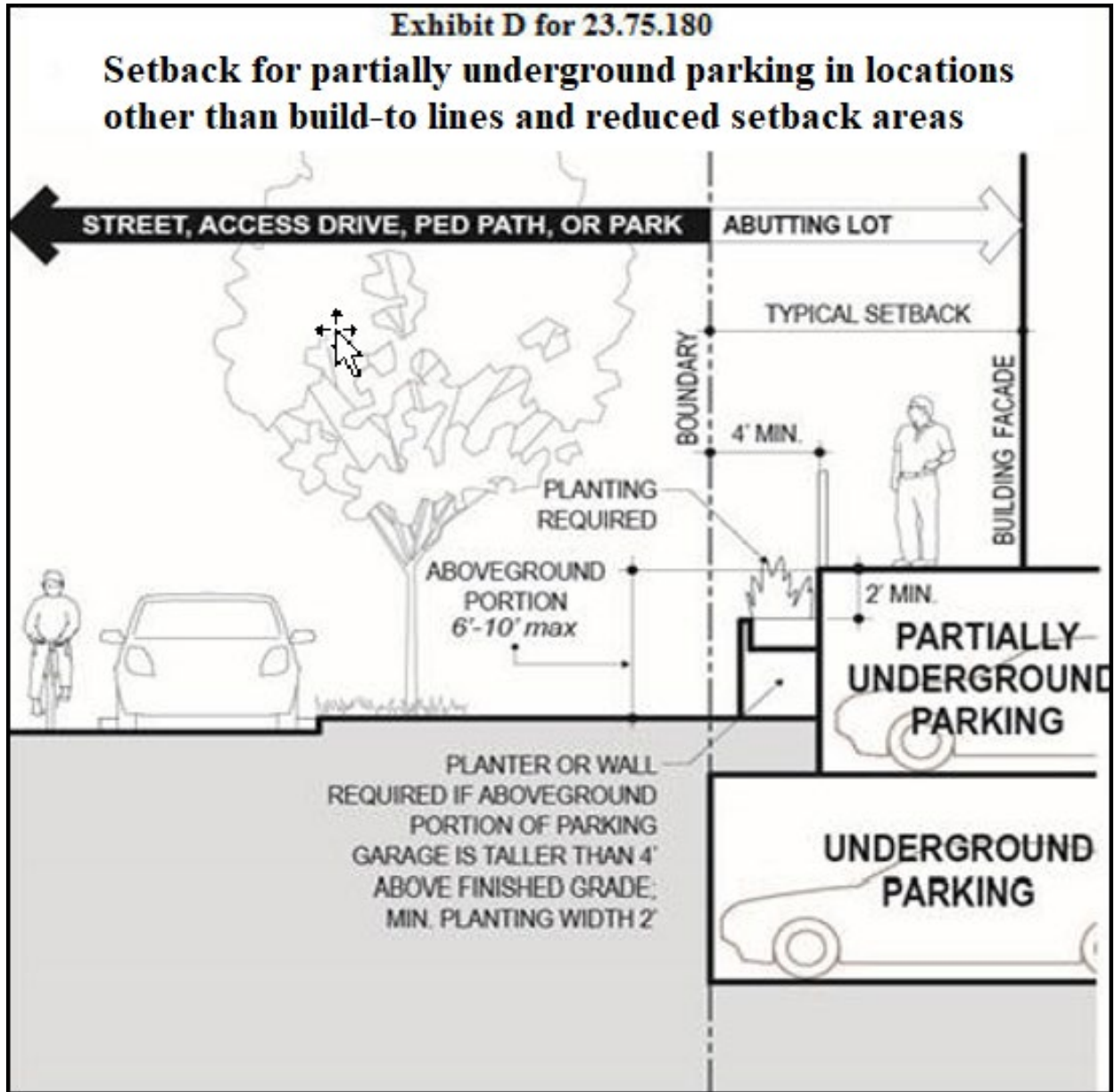
1                                   a. The aboveground portion is required to be no higher than 6 feet above  
2 the finished grade at the boundary.

3                                   b. If the aboveground portion of the parking garage is taller than 4 feet  
4 above finished grade, a wall or planter shall be provided between the parking garage and the  
5 boundary, as illustrated in Exhibit D for 23.75.180. The top of this wall or planter shall be at  
6 least (~~two~~) 2 feet below the top of the aboveground portion of the parking garage, and the  
7 planting area shall be at least 2 feet in width. Vegetation shall be provided at the top of this wall  
8 or planter.

- 1 **Exhibit D for 23.75.180**
- 2 **Setback ((for Partially Underground Parking in Locations Other Than Build-to-Lines and**
- 3 **Reduced Setback Areas)) for partially underground parking in locations other than build-**
- 4 **to lines and reduced setback areas**



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I. Parking and loading access

1. Access for parking and for loading is required to meet the following requirements:
  - a. Access is not allowed within 40 feet of the curb line of an intersection.
  - b. Access is not allowed within 20 feet of a structure corner that includes a regulated ((~~façade~~)) façade on one or both sides.

1                   2. Each access drive is required to include a dedicated pedestrian area along at  
2 least one side of the length of the drive. The dedicated pedestrian area is required to:

3                   a. ~~((include))~~ Include a walking surface at least 6 feet wide along the  
4 length of the access drive; and

5                   b. ~~((be))~~ Be separated from the access drive roadway by a raised curb,  
6 bollards, landscaping, or textured paving details.

7                   3. Curb cuts are required to meet the standards of ~~((subsections 23.54.030.F and~~  
8 ~~23.54.030.G))~~ Section 23.54.031.

9                   4. Driveways are required to meet the standards of subsection 23.54.030.D.

10                  Section 70. Section 23.76.064 of the Seattle Municipal Code, last amended by Ordinance  
11 118672, is amended as follows:

12 **23.76.064 Approval of City facilities~~((:))~~**

13                  A. Concept ~~((Approval))~~ approval for City ~~((Facilities:))~~ facilities

14                   1. In acting on the proposed siting or expansion of a City facility, the Council  
15 shall decide whether to approve in concept the facility. If concept approval is granted, the  
16 Council may impose terms and conditions, including but not limited to design criteria and  
17 conditions relating to the size and configuration of the proposed facility.

18                   2. Following Council approval, final plans for a City facility shall be submitted to  
19 the Director. If the Director determines that the project is consistent with the Council's concept  
20 approval, the Director shall issue the necessary permits for the facility.

21                   3. No further Council action is required for a City facility unless the Director  
22 determines that the final plans represent a major departure from the terms of the original Council



1 concept approval, in which case the final plan shall be submitted to the Council for approval in  
2 the same manner as the original application.

3 B. City (~~(Facilities Not Meeting Development Standards)~~) facilities not meeting  
4 development standards. The Council may waive or modify applicable development standards,  
5 accessory use requirements, special use requirements, or conditional use criteria for City  
6 facilities. If a waiver or modification of a development regulation is sought because the  
7 development regulation would otherwise preclude the siting of an essential public facility, then  
8 the decision to waive or modify shall be made pursuant to Chapter 23.80 and not this Section  
9 23.76.064.

10 Section 71. Subsection 23.80.004.B of the Seattle Municipal Code, which section was  
11 last amended by Ordinance ~~124105~~ 127228, is amended as follows:

12 **23.80.004 Review criteria((-))**

13 \* \* \*

14 B. ((H)) Except as provided in subsection 23.80.004.C, if the decisionmaker determines  
15 that attaching conditions to the permit approval will facilitate project siting in light of the  
16 considerations identified above, the decisionmaker may establish conditions for the project for  
17 that purpose. However, the decisionmaker may waive or modify development regulations only to  
18 the extent that a waiver or modification is approved pursuant to Section ((23.80.010)) 23.80.014.

19 \* \* \*

20 Section 72. Section 23.80.008 of the Seattle Municipal Code, enacted by Ordinance  
21 127228, is amended as follows:

22 **23.80.008 Development standards for light rail transit facilities**

23 \* \* \*

1           K. Access, street improvements, and motor vehicle parking((=))

2                   1. The Director shall consult with the Director of Transportation to determine the  
3 required location for motor vehicle access from a right-of-way to a light rail transit facility. The  
4 access location shall enhance pedestrian safety and comfort, facilitate transit operations and  
5 maintenance, facilitate the movement of vehicles, minimize the on-street queuing of vehicles,  
6 enhance vehicular safety, and minimize hazards.

7                   2. Light rail transit stations and ancillary facilities, including but not limited to  
8 venting structures and traction power substations, shall be subject to Chapter 23.53. Light rail  
9 transit stations and ancillary facilities may not utilize the street and alley improvement  
10 exceptions in Chapter 23.53 that are based on minimum gross floor area thresholds for non-  
11 residential uses and expansions of outdoor storage or parking supply.

12                  3. Light rail transit facilities, including motor vehicle, transit, pedestrian, bicycle,  
13 and shared micromobility facilities for operation of new light rail transit facilities, shall  
14 demonstrate a right-of-way design consistent with Chapter 23.53 and the Streets Illustrated  
15 Right-of-Way Improvements Manual or successor rule unless otherwise allowed by the  
16 Director of Transportation. Where such facilities cannot be accommodated in the right-of-way,  
17 they shall be provided on the station site. Site and right-of-way design shall be reviewed in  
18 consultation with the Director of Transportation.

19                  4. Pedestrian lighting shall be provided in the right-of-way adjacent to light rail  
20 transit facilities.

21                  5. Light rail transit facilities' vehicle and pedestrian access outside of the rights-  
22 of-way shall meet the following requirements unless the requirements are waived or modified by  
23 the Director to enhance pedestrian safety and comfort, facilitate transit operations and

1 maintenance, facilitate the movement of vehicles, minimize the on-street queuing of vehicles,  
2 enhance vehicular safety, or minimize hazards:

3 a. A maximum of two vehicle travel lanes may be provided to connect  
4 light rail transit facilities to the right-of-way. Vehicle travel lanes have a maximum width of 9  
5 feet, except vehicle travel lanes used by buses or freight vehicles have a maximum width of 11  
6 feet. Lanes for bus loading and unloading and bus layover are not considered travel lanes.

7 b. Curb cuts for one-way traffic shall be a minimum of 12 feet and a  
8 maximum of 15 feet, and curb cuts for two-way traffic shall be a minimum of 22 feet and a  
9 maximum of 25 feet.

10 c. Vehicle travel lanes shall meet sight triangle requirements of  
11 ~~((subsection 23.54.030-G))~~ Section 23.54.032.

12 d. Pedestrian walkways shall be provided adjacent to vehicle travel lanes  
13 and have a minimum unobstructed width of 8 feet except that the minimum pedestrian walkway  
14 width shall be 18 feet adjacent to station entries and the minimum unobstructed multiuse path  
15 width shall be 12 feet where the pedestrian walkway is shared with bicycles and other mobility  
16 devices. Where pedestrian walkways and paths for bicycles and other mobility devices are  
17 separated, the paths for bicycles and other mobility devices shall comply with the minimum  
18 requirements of the Streets Illustrated Right-of-Way Improvements Manual or successor rule.

19 e. Pedestrian walkways shall include a horizontal or vertical separation  
20 between the walkway and a vehicle travel lane.

21 f. Curb ramps are required where a pedestrian walkway crosses a vehicle  
22 travel lane or right-of-way.

1 g. Lighting shall be provided along all travel lanes, pedestrian walkways,  
2 multiuse pathways, and bicycle facilities.

3 6. Vehicle parking provided at light rail transit facilities shall comply with  
4 ~~((Section))~~ Sections 23.54.030, 23.54.031, 23.54.032, 23.54.033, and 23.54.034.

5 L. Bicycle parking and shared micromobility device parking for light rail transit  
6 stations~~((:))~~

7 1. Definitions. For the purposes of this subsection 23.80.008.L:

8 “Bicycles-on-board ratio” is the assumed proportion of bicycle riders that  
9 will take their bicycles with them on a train trip, which is 50 percent.

10 “Central stations” are stations located within the Downtown Urban Center  
11 with greater than 10,000 projected daily boardings.

12 “Daily total boardings” is the projected horizon year daily passenger  
13 boarding volume at a station, as defined in a final EIS for a link extension, or other subsequent  
14 documentation if prepared for a future system expansion.

15 “Horizon year” means the year used in projecting the highest analyzed  
16 level of future ridership.

17 “Local stations” are those stations located in intermediate vicinities that  
18 are not served by central stations, mid-center stations, or terminus stations.

19 “Mid-center stations” are those located within one-half mile of the  
20 Downtown Urban Center or stations within the Downtown Urban Center with less than 10,000  
21 projected daily boardings.

22 “Morning peak passenger ridership” is assumed as one-third of daily total  
23 boardings at a station projected for the horizon year, based on boarding volumes documented in a

1 final EIS for a link extension, or other subsequent documentation if prepared for a future system  
2 expansion. Daily boardings generated by riders transferring to and from trains on other light rail  
3 link segments shall not be included in the daily total boardings.

4 “Planned bicycle mode share” is defined as an estimated proportion of a  
5 station’s total boardings that will be made by persons using bicycles as their primary means of  
6 accessing a light rail station.

7 “Shared micromobility” refers to fleets of small, low-speed vehicles  
8 designed for personal transport, including but not limited to bicycles and scooters, and operated  
9 as a network by for-profit, non-profit, or government entity. They are available for membership  
10 to the general public on a pay-per-use or pass basis.

11 “Terminus stations” are those stations located at the end of a light rail  
12 system route in the City of Seattle.

13 2. Bicycle parking demand “D” is calculated as the morning peak passenger  
14 ridership multiplied by the planned bicycle mode share percentages in Table A for 23.80.008,  
15 which is then multiplied by 0.5 (the bicycles-on-board ratio).

16 3. To serve the bicycle parking demand “D” for opening day of service, the  
17 required minimum number of bicycle parking spaces shall be provided as follows:

18 a. The minimum bicycle parking amount required at opening day of  
19 service at a light rail station shall be calculated using the “day-of-opening” planned bicycle travel  
20 mode share percentages in Table A for 23.80.008;

21 b. Two-thirds of the minimum bicycle parking shall be long-term bicycle  
22 parking;

1 c. One-third of the minimum bicycle parking shall be short-term bicycle  
2 parking;

3 d. If the bicycle parking demand “D” is less than 54 total spaces, a  
4 minimum number of 54 bicycle parking spaces shall be provided, which shall be allocated two-  
5 thirds to long-term spaces and one-third to short-term spaces;

6 e. Bicycle parking to meet day-of-opening requirements shall be provided  
7 on the light rail transit station site, or may be located within the right-of-way if approved by the  
8 Director of Transportation.

<b>Station type</b>	<b>Day-of-opening</b>	<b>In-reserve</b>
Terminus	5.5%	1.5%
Local	4%	3%
Mid-center	2%	2%
Central	1%	1%

9 4. If average use of the bicycle parking at a light rail transit facility exceeds 85  
10 percent of capacity at a future date, measured using methods that the Director shall adopt by rule,  
11 additional bicycle parking shall be required. The amount of additional required bicycle parking,  
12 described as the “in-reserve requirement,” shall be calculated using the planned bicycle travel  
13 mode shares for the “in-reserve requirement” in Table A for 23.80.008. In-reserve required  
14 bicycle parking may be provided on the light rail transit station site, or within 200 feet of the site,  
15 or in right-of-way if approved by the Director of Transportation.

16 5. The Director may require more or fewer than the minimum number of bicycle  
17 parking spaces and micromobility space requirements based on the following: area topography;  
18 pattern and volume of expected bicycle users; nearby residential and employment density;  
19 proximity to the Urban Trails system and other existing and planned bicycle facilities; projected  
20 transit ridership and expected access to transit by bicycle; and other relevant transportation and

1 land use information. Prior to adjusting the minimum number of parking spaces for bicycles, the  
2 Director shall consult with the Director of Transportation.

3 6. The minimum space for shared micromobility device parking shall be: 240  
4 square feet for terminus stations and 120 square feet for other station types.

5 7. Bicycle and micromobility device parking locations shall be located as close to  
6 station entrances as feasible and may be located within the right-of-way if approved by the  
7 Director of Transportation.

8 8. Bicycle parking shall meet the following performance standards: subsections  
9 ((~~23.54.015.K.2.a, 23.54.015.K.2.c, 23.54.015.K.2.d, 23.54.015.K.2.e, 23.54.015.K.2.h, and~~  
10 ~~23.54.015.K.2.i~~)) 23.54.037.B.1, 23.54.037.B.3, 23.54.037.B.4, 23.54.037.B.5, 23.54.037.B.8,  
11 and 23.54.037.B.9.

12 9. Parking locations shall be provided with level-entry routes, and, if bicycle  
13 parking is located above or below the surface level, it shall be served by features such as  
14 elevators sized to accommodate bicycles and runnels on stairs to aid bicycle movement.

15 10. The applicant shall demonstrate bicycle parking design will accommodate a  
16 variety of bicycle types, including but not limited to, electric bikes and cargo bikes.

17 11. Shared micromobility device parking shall be clearly delineated, located at  
18 ground level, be without access obstructions and not encroach on pedestrian access paths,  
19 include adequate lighting, and include directional signage to promote easy wayfinding.

20 \* \* \*

21 Section ~~72-73~~. A new Section ~~23.80.006~~ 23.80.010 is added to the Seattle Municipal  
22 Code as follows:

23 ~~23.80.006~~ 23.80.010 **Identifying new types of essential public facilities**

1 The Director may, as a Type I decision, determine that a facility not otherwise listed in the  
2 definition of an essential public facility in Section 23.84A.010 is an essential public facility if:

3 A. The facility provides or is necessary to provide a public service; and

4 B. Any of the following conditions exist:

5 1. The public facility needs a specific type of site of such a size, location, or  
6 availability of public services for which there are few choices;

7 2. The public facility needs to be located near another public facility or is an  
8 expansion of an essential public facility at an existing location;

9 3. The public facility has significant adverse impacts that make it difficult to site;

10 4. Use of the normal development review process would effectively preclude the  
11 siting of an essential public facility; or

12 5. Development regulations require the proposed facility to use an essential public  
13 facility siting process.

14 Section ~~73-74~~. A new Section ~~23.80.008~~ 23.80.012 is added to the Seattle Municipal  
15 Code as follows:

16 ~~23.80.008~~ 23.80.012 **Review is supplementary**

17 Review of an essential public facility, except for light rail facilities, under this Chapter 23.80,

18 including a decision to condition approval of a project or to waive or modify a development

19 regulation as authorized by this Chapter 23.80, is part of the decision to approve or deny a permit

20 application and is not a separate or distinct regulatory decision. If the underlying decision is

21 subject to administrative appeal, then decisions made under this Chapter 23.80 are subject to

22 review on administrative appeal of the underlying decision. If the underlying decision is not



1 subject to administrative appeal, then decisions made under this Chapter 23.80 are not subject to  
2 review on administrative appeal of the underlying decision.

3 Section ~~74-75~~. A new Section ~~23.80.0010~~ 23.80.014 is added to the Seattle Municipal  
4 Code as follows:

5 ~~23.80.010~~ 23.80.014 **Waiver or modification of development regulation**

6 A. Application for waiver or modification. If the applicant for approval of an essential  
7 public facility seeks the waiver or modification of a development regulation, the applicant shall  
8 include in the application:

9 1. The specific identification of each development regulation sought to be waived  
10 or modified;

11 2. A detailed explanation of the manner in which each development regulation is  
12 believed to preclude the siting of the essential public facility; and

13 3. A detailed description of any mitigation measures the applicant proposes to  
14 take to avoid or mitigate the adverse effects that may result from the proposed waiver or  
15 modification of the development regulation.

16 B. Decision to waive or modify. If the decisionmaker determines that application of a  
17 development regulation will preclude the siting of an essential public facility, the decisionmaker  
18 shall waive or modify the application of the development regulation to the extent necessary to  
19 allow siting the facility. The decisionmaker shall consider the provisions of WAC 365-196-550  
20 when deciding whether a development regulation precludes the siting of the facility.

21 C. Mitigation. If the decisionmaker waives or modifies a development regulation, the  
22 decisionmaker may require the applicant to comply with conditions that avoid or mitigate  
23 adverse effects that the decisionmaker believes may result from waiver or modification of the

1 development regulation. If the development regulation to be waived or modified is contained in  
2 Chapter 23.60A or Chapter 25.09, and the waiver or modification would result in a net loss of  
3 ecological function, the decisionmaker shall impose mitigation conditions to achieve no net loss  
4 of ecological functions as a result of granting the waiver or modification.

5 D. Relationship to other provisions authorizing exceptions, variances, exemptions, and  
6 other forms of relief

7 1. Except as provided in subsection ~~23.80.010.D.2~~ 23.80.014.D.2, regardless of  
8 any other provision of this Title 23, Chapter 25.09, or Chapter 25.11, if an applicant seeks the  
9 waiver or modification of a development regulation under this Section ~~23.80.010~~ 23.80.014, the  
10 applicant is not required to also seek relief from the application of the development regulation  
11 pursuant to any other form of relief afforded by the Seattle Municipal Code, including  
12 procedures for exceptions, variances, exemptions, and similar procedures. However, an applicant  
13 is not precluded from seeking such other relief in addition to relief under this Section ~~23.80.010~~  
14 23.80.014.

15 2. When the waiver or modification sought under this Section ~~23.80.010~~  
16 23.80.014 is of a development regulation contained in Chapter 23.60A, the applicant must seek  
17 relief from the development regulation pursuant to the procedures set forth in Chapter 23.60A. In  
18 the event that relief cannot be granted under those procedures, the development regulation may  
19 be waived or modified under this Section ~~23.80.010~~ 23.80.014.

20 E. Exemption for light rail facilities. This Section ~~23.80.010~~ 23.80.014 does not apply to  
21 light rail facilities. Development standards for light rail facilities may be waived or modified  
22 pursuant to subsection 23.80.004.C.

1 Section ~~75~~ 76. Section 23.84A.002 of the Seattle Municipal Code, last amended by  
2 Ordinance 126855, is amended as follows:

3 **23.84A.002 “A”**

4 \* \* \*

5 “Adult family home((-))” ((~~See “Residential use.”~~)) means the occupation of a dwelling  
6 unit by an adult family home defined and licensed as such by the State of Washington under  
7 chapter 70.128 RCW.

8 \* \* \*

9 Section ~~76~~ 77. Section 23.84A.006 of the Seattle Municipal Code, last amended by  
10 Ordinance 127099, is amended as follows:

11 **23.84A.006 "C"**

12 \* \* \*

13 ((~~“Carriage House” See “Residential use.”~~  
14 ~~“Carriage House structure” See “Residential use.”~~))

15 \* \* \*

16 ((~~“Cottage, backyard.” See “detached accessory dwelling unit” under the definition of~~  
17 ~~“Residential use” in Section 23.84A.032.~~))

18 \* \* \*

19 Section ~~77~~ 78. Section 23.84A.008 of the Seattle Municipal Code, last amended by  
20 Ordinance 127211, is amended as follows:

21 **23.84A.008 “D”**

22 \* \* \*

1           ~~((“Duplex” means a single structure containing only two dwelling units, neither of which~~  
2 ~~is a legally established accessory dwelling unit.))~~

3           “Dwelling unit” means a room or rooms located within a structure that are configured to  
4 meet the standards of Section 23.42.048 ~~((and that are occupied or intended to be occupied by~~  
5 ~~not more than one household as living accommodations independent from any other household.))~~  
6 , providing independent living facilities for one household, including permanent provisions for  
7 sleeping, food preparation, and sanitation.

8           “Dwelling unit, accessory((:))” ~~((See “Residential use.”))~~ means a dwelling unit that:

9                   1. Is located within the same structure as a principal dwelling unit or within an  
10 accessory structure on the same lot as a principal dwelling unit; and

11                   2. Is designed and arranged to be occupied as living facilities independent from  
12 any other dwelling unit.

13           “Dwelling unit, attached” means a dwelling unit that:

14                   1. Occupies space from the ground to the roof of the structure in which it is  
15 located; and

16                   2. Is attached to another dwelling unit. Dwelling units are considered attached if  
17 they share a common or party wall or have walls containing floor area that are located within 2  
18 feet of each other.

19           “Dwelling unit, detached” means a dwelling unit that:

20                   1. Occupies space from the ground to the roof of the structure in which it is  
21 located; and

22                   2. Is not attached to any other dwelling unit.

1 ((~~“Dwelling unit, detached accessory.”~~ Also known as a backyard cottage. See ~~“detached~~  
2 ~~accessory dwelling unit”~~ under the definition of ~~“Residential use”~~ in Section 23.84A.032.))

3 “Dwelling unit, principal” means a dwelling unit that is not accessory to another dwelling  
4 unit.

5 “Dwelling unit((-), small efficiency” means a dwelling unit with an amount of square  
6 footage less than the minimum amounts specified for Efficiency Dwelling Units in the Seattle  
7 Building Code, and that meets the standards prescribed in Section 23.42.048.

8 “Dwelling unit, stacked” means dwelling units that are located above or below other  
9 dwelling units such as apartments or condominium buildings.

10 Section ~~78~~ 79. Section 23.84A.010 of the Seattle Municipal Code, last amended by  
11 Ordinance 126685, is amended as follows:

12 **23.84A.010 “E”**

13 \* \* \*

14 “Essential public facilities” (~~within the City of Seattle~~) means (~~airports,~~) sewage  
15 treatment plants, (~~jails,~~) light rail transit systems, (~~and~~) power plants, any facilities identified  
16 as an essential public facility in RCW 36.70A.200, and any facility determined to be an essential  
17 public facility pursuant to Section ~~23.80.006~~ 23.80.010.

18 “EV-ready” means a minimum 40-ampere dedicated 208- or 240-volt branch circuit (32-  
19 amp load) terminated at a junction box or receptacle outlet in close proximity to a parking space.

20 \* \* \*

21 Section ~~79~~ 80. Section 23.84A.016 of the Seattle Municipal Code, last amended by  
22 Ordinance 127099, is amended as follows:

23 **23.84A.016 "H"**

\* \* \*

"Housing, low-income" means a structure or structures for which:

1. An application for public funding for the capital costs of development or rehabilitation of the structure(s) has been or will be submitted; and

2. ~~((Public))~~ A written notice of public funding ~~((is awarded))~~ award, including terms, is received prior to issuance of the ~~((first))~~ building permit, which for development projects shall be the first building permit that includes the structural frame for each structure, and such funding is conditioned on one or more regulatory agreements, covenants, or other legal instruments, recorded on the title of the property and enforceable by The City of Seattle, King County, State of Washington, Washington State Housing Finance Commission, or other public agency, if approved by the Director of Housing, ((being executed and recorded on the title of the property that includes the low-income housing and such legal instruments either:

a. ~~For a minimum period of 40 years, require rental of at least 40 percent of the dwelling units, small efficiency dwelling units, or congregate residence sleeping rooms as restricted units with rent and income limits no higher than 60 percent of median income; or~~

b. ~~For a minimum period of 50 years, require at least 40 percent of the dwelling units as restricted units sold to buyers with incomes no higher than 80 percent of median income at prices (initial sale and resale) to allow modest growth in homeowner equity while maintaining long-term affordability for income-eligible buyers, all as determined by the Director of Housing))~~ that ensure at least 50 percent of total dwelling units shall be low-income units.

\* \* \*

1 Section ~~80~~ 81. Section 23.84A.018 of the Seattle Municipal Code, last amended by  
2 Ordinance 126862, is amended as follows:

3 **23.84A.018 "I"**

4 \* \* \*

5 "Institution" means ~~((structures(s)))~~ structures and related grounds used by organizations  
6 for the provision of educational, medical, cultural, social, and/or recreational services to the  
7 community, including but not limited to the following uses:

8 1. "Adult care center" means an institution that regularly provides care to a group  
9 of adults for less than 24 hours a day, whether for compensation or not.

10 2. "College" means a post-secondary educational institution, operated by a  
11 nonprofit organization, granting associate, bachelor, and/or graduate degrees.

12 3. "Community club or center" means an institution used for athletic, social, civic,  
13 cultural, artistic, or recreational purposes, operated by a nonprofit organization, and open to the  
14 general public on an equal basis. Activities in a community club or center may include, but are  
15 not limited to, classes and events sponsored by nonprofit organizations, community programs for  
16 the elderly, social gatherings, educational programming, gardens, and art exhibits~~((;))~~.

17 a. "Community center" means a community club or center use, providing  
18 direct services to people on the premises rather than carrying out only administrative functions,  
19 that is open to the general public without membership. Community centers may include  
20 accessory commercial uses including but not limited to commercial kitchens and food  
21 processing, craft work and maker spaces, cafes, galleries, co-working spaces, health clinics,  
22 office spaces, and retail sales of food and goods.

1                   b. "Community club" means a community club or center use((  
2 membership)) to which membership is open to the general public on an equal basis.

3                   4. "Child care center" means an institution that regularly provides care to a group  
4 of children for less than 24 hours a day, whether for compensation or not. Preschools,  
5 cooperative child care exchanges, and drop-in centers where children receive care by the day  
6 ((~~shall be~~)) are considered to be child care centers.

7                   5. "Community farm" means an institution, operated by a nonprofit organization,  
8 in which land and related structures are primarily used to grow or harvest plants for food,  
9 educational, cultural, or ecological restoration purposes, or to keep animals in accordance with  
10 Section 23.42.052. Additional activities may include but are not limited to indoor and outdoor  
11 classes and events, food processing and preparation, community programs and gatherings, and  
12 the sale of plants, harvested or prepared food, ornamental crops, and animal products such as  
13 eggs or honey but not including the slaughtering of animals or birds for meat.

14                   6. (~~"Family support center" means an institution that offers support services and  
15 instruction to families, such as parenting classes and family counseling, and is co-located with a  
16 Department of Parks and Recreation community center.~~

17                   7.)) "Hospital" means an institution other than a nursing home that provides  
18 accommodations, facilities, and services over a continuous period of 24 hours or more, for  
19 observation, diagnosis, and care of individuals who are suffering from illness, injury, deformity,  
20 or abnormality or from any condition requiring obstetrical, medical, or surgical services, or  
21 alcohol or drug detoxification.



1                    ~~((8-))~~ 7. "Institute for advanced study" means an institution operated by a  
2 nonprofit organization for the advancement of knowledge through research, including the  
3 offering of seminars and courses, and technological and/or scientific laboratory research.

4                    ~~((9-))~~ 8. "Library" means an institution where literary, musical, artistic, or  
5 reference materials are kept for use but not generally for sale.

6                    ~~((10-))~~ 9. "Museum" means an institution operated by a nonprofit organization as  
7 a repository of natural, scientific, historical, cultural, or literary objects of interest or works of  
8 art, and where the collection of such items is systematically managed for the purpose of  
9 exhibiting them to the public.

10                   ~~((11-))~~ 10. "Private club" means an institution used for athletic, social, or  
11 recreational purposes and operated by a private nonprofit organization, ~~((membership))~~ to which  
12 membership is by written invitation and election according to qualifications in the club's charter  
13 or bylaws and the use of which is generally restricted to members and their guests.

14                   ~~((12-))~~ 11. "Religious facility" means an institution, such as a church, temple,  
15 mosque, synagogue, or other structure, together with its accessory structures, used primarily for  
16 religious worship.

17                   ~~((13-))~~ 12. "School, elementary or secondary" means an institution operated by a  
18 public or nonprofit organization primarily used for systematic academic or vocational instruction  
19 through the twelfth grade.

20                   ~~((14-))~~ 13. "School, vocational or fine arts" means an institution that teaches  
21 trades, business courses, hairdressing, and similar skills on a post-secondary level, or that teaches  
22 fine arts such as music, dance, or painting to any age group, whether operated for nonprofit or  
23 profit-making purposes, except businesses that provide training, instruction, or lessons

1 exclusively on an individual basis, which are classified as general retail sales and service uses,  
2 and except those businesses accessory to an indoor participant sports use.

3 ((15.)) 14. "University." See "College."

4 Section ~~81~~ 82. Section 23.84A.024 of the Seattle Municipal Code, last amended by  
5 Ordinance 126855, is amended as follows:

6 **23.84A.024 “L”**

7 \* \* \*

8 "Lot line, front" means: ~~((, in the case of a lot with frontage on a single street, the lot line~~  
9 ~~separating the lot from the street, and in the case of a lot with frontage on more than one street~~  
10 ~~other than a through lot, the lot line separating the lot from any abutting street, provided the other~~  
11 ~~lot line(s) that abut streets are considered to be either side street lot line(s) or the rear lot line~~  
12 ~~according to the definitions of those terms. In the case of a through lot, the lot lines separating~~  
13 ~~the lot from the streets that are parallel or within 15 degrees of parallel to each other are both~~  
14 ~~front lines. For new development on a lot with no street frontage, the front lot line shall be the lot~~  
15 ~~line designated by the project applicant in accordance with Section 23.86.010. If the area of the~~  
16 ~~front yard based on a front lot line determined according to this definition is less than 20 percent~~  
17 ~~of the total lot area and is less than 1,000 square feet in area, the Director may designate a~~  
18 ~~different lot line as the front lot line in order to provide structural setbacks, building separations,~~  
19 ~~and open space that are more consistent with those of other lots that are within 100 feet of the~~  
20 ~~property.))~~

21 1. For a lot with frontage on a single street, the lot line separating the lot from the  
22 street;

1 2. For a through lot, all lot lines separating the lot from the streets that are parallel  
2 or within 15 degrees of parallel to each;

3 3. For a lot with frontage on more than one street other than a through lot, a lot  
4 line determined by the Director based on the existing pattern of lots and buildings on the block;

5 and

6 4. For a lot with no street frontage:

7 a. On a lot that has only one alley lot line, the alley lot line;

8 b. On a lot that has more than one alley lot line, one alley lot line  
9 determined by the Director based on existing pattern of lots and buildings on the alleys; and

10 c. On a lot that has no alley lot lines, a lot line chosen by the applicant,  
11 provided that the selected front lot line length is at least 50 percent of the width of the lot.

12 \* \* \*

13 Section ~~82~~ 83. Section 23.84A.025 of the Seattle Municipal Code, last amended by  
14 Ordinance 127099, is amended as follows:

15 **23.84A.025 “M”**

16 \* \* \*

17 “Major retail store” means a structure or portion of a structure that provides adequate  
18 space of at least ~~((eighty thousand (–)))~~ 80,000 ~~((+))~~ square feet to accommodate the merchandising  
19 needs of a major new retailer with an established reputation, and providing a range of  
20 merchandise and services, including both personal and household items, to anchor downtown  
21 shopping activity around the retail core, thereby supporting other retail uses and the area's  
22 vitality and regional draw for customers.

23 “Major transit service.” See “Transit service, major.”

1 “Major transit stop.” See “Transit stop, major.”

2 \* \* \*

3 ~~((“Multifamily residential structure” means a structure containing only multifamily~~  
4 ~~residential uses and permitted uses accessory to the multifamily residential uses.~~

5 ~~“Multifamily structure.” See “Residential use.”))~~

6 \* \* \*

7 Section ~~83~~ 84. Section 23.84A.030 of the Seattle Municipal Code, last amended by  
8 Ordinance 127099, is amended as follows:

9 **23.84A.030 “P”**

10 \* \* \*

11 ~~“Permanent supportive housing.” ((means low income housing that is paired with on or~~  
12 ~~off-site voluntary human services to support people living with complex and disabling behavioral~~  
13 ~~health or physical health conditions and experiencing homelessness or at imminent risk of~~  
14 ~~homelessness prior to moving into such housing.)) See “Residential use, permanent supportive~~  
15 ~~housing.”~~

16 \* \* \*

17 ~~((“Planned community development (PCD)” means a zoning process that authorizes~~  
18 ~~exceptions from certain development standards for structures on large tracts of land in certain~~  
19 ~~downtown zones. A PCD is developed as a single entity through a public process.~~

20 ~~“Planned residential development (PRD)” means a zoning mechanism that allows for~~  
21 ~~flexibility in the grouping, placement, size and use of structures on a fairly large tract of land. A~~  
22 ~~PRD is developed as a single entity, using a public process that incorporates design review.))~~

23 \* \* \*

1 Section ~~84~~ 85. Section 23.84A.032 of the Seattle Municipal Code, last amended by  
2 Ordinance 127211, is amended as follows:

3 **23.84A.032 “R”**

4 \* \* \*

5 "Residential use" means ~~((any one or more of))~~ a use in one or more structures, including  
6 interior and exterior accessory spaces, in which people primarily live including the following  
7 uses:

8 1. ~~("Accessory dwelling unit" means a dwelling unit that:~~

9 a. ~~Is located within or attached to a structure containing a principal~~  
10 ~~dwelling unit or within an accessory structure on the same lot as principal dwelling unit(s); and~~

11 b. ~~Is designed, arranged, and intended to be occupied as living facilities~~  
12 ~~independent from any other dwelling unit.~~

13 2. ~~"Attached accessory dwelling unit" means an accessory dwelling unit that is~~  
14 ~~within or attached to a structure containing a principal dwelling unit.~~

15 3. ~~"Adult family home" means an adult family home defined and licensed as such~~  
16 ~~by the State of Washington in a dwelling unit.~~

17 4. ~~"Apartment" means a multifamily residential use that is not a cottage housing~~  
18 ~~development, rowhouse development, or townhouse development.~~

19 5.)) "Artist's studio/dwelling" means a combination working studio and dwelling  
20 unit for artists, consisting of a room or suite of rooms occupied by not more than one household.

21 ~~((6.))~~ 2. "Assisted living facility" means a ~~((use licensed by the State of~~  
22 ~~Washington as a))~~ boarding home licensed by the State of Washington that contains at least two  
23 assisted living units for people who have either a need for assistance with activities of daily

1 living (which are defined as eating, toileting, ambulation, transfer (e.g., moving from bed to chair  
2 or chair to bath), and bathing) or some form of cognitive impairment but who do not need the  
3 skilled critical care provided by nursing homes. See "Assisted living unit."

4 ~~((7. "Carriage house" means a dwelling unit in a carriage house structure.~~

5 ~~8. "Carriage house structure" means a structure within a cottage housing  
6 development, in which one or more dwelling units are located on the story above an enclosed  
7 parking garage at ground level that either abuts an alley and has vehicle access from that alley, or  
8 is located on a corner lot and has access to the parking in the structure from a driveway that abuts  
9 and runs parallel to the rear lot line of the lot. See also "Carriage house."~~

10 ~~9.))~~ 3. "Caretaker's quarters" means a ~~((use accessory to a non-residential use  
11 consisting of a))~~ dwelling unit not exceeding 800 square feet of living area ~~((and))~~ that is  
12 occupied by a caretaker or watchperson and accessory to a nonresidential use.

13 ~~((10.))~~ 4. "Congregate residence" means a use in which sleeping rooms are  
14 independently rented and lockable and provide living and sleeping space, and residents share  
15 kitchen facilities and other common elements with other residents in a building.

16 ~~((11. "Cottage housing development" means a use consisting of cottages arranged  
17 on at least two sides of a common open space or a common amenity area. A cottage housing  
18 development may include a carriage house structure. See "Cottage," "Carriage house," and  
19 "Carriage house structure."~~

20 ~~12. "Detached accessory dwelling unit" means an accessory dwelling unit in an  
21 accessory structure.~~

1                   ~~13. "Domestic violence shelter" means a structure or portion of a structure~~  
2 ~~managed by a nonprofit organization, which unit provides housing at a confidential location and~~  
3 ~~support services for victims of domestic violence.~~

4                   ~~14. "Floating home" means a dwelling unit constructed on a float that is moored,~~  
5 ~~anchored, or otherwise secured in the water.~~

6                   ~~15. "Low income housing.")~~

7                   5. "Housing" means one or more dwelling units with permanent foundations or  
8 moorage at a marina that are not defined as another type of residential use in this definition.

9                   ~~((16.))~~ 6. "Mobile home" means a structure that is designed and constructed to be  
10 transportable in one or more sections and built on a permanent chassis, designed to be used as a  
11 dwelling unit without a permanent foundation, and connected to utilities that include plumbing,  
12 heating, and electrical systems. A structure that was transportable at the time of manufacture is  
13 still considered to meet this definition notwithstanding that it is no longer transportable.

14                   ~~((17. "Mobile home park" means a tract of land that is rented for the use of more~~  
15 ~~than one mobile home occupied as a dwelling unit.~~

16                   ~~18. "Multifamily residential use" means a use consisting of two or more dwelling~~  
17 ~~units in a structure or portion of a structure, excluding accessory dwelling units, or a congregate~~  
18 ~~residence.~~

19                   ~~19. "Nursing home" means a use licensed by the State of Washington as a nursing~~  
20 ~~home, that provides full-time convalescent and/or chronic care for individuals who, by reason of~~  
21 ~~chronic illness or infirmity, are unable to care for themselves, but that does not provide care for~~  
22 ~~the acutely ill or surgical or obstetrical services. This definition excludes hospitals or~~  
23 ~~sanitariums.~~

1                   ~~20.))~~ 7. "Permanent supportive housing((-))" means low-income housing that is  
2 paired with on- or off-site voluntary human services to support people living with complex and  
3 disabling behavioral health or physical health conditions and experiencing homelessness or at  
4 imminent risk of homelessness prior to moving into such housing.

5                   ~~((21. "Rowhouse development" means a multifamily residential use in which all~~  
6 ~~principal dwelling units on the lot meet the following conditions:~~

7                   ~~a. Each dwelling unit occupies the space from the ground to the roof of the~~  
8 ~~structure in which it is located;~~

9                   ~~b. No portion of a dwelling unit, except for an accessory dwelling unit or~~  
10 ~~shared parking garage, occupies space above or below another dwelling unit;~~

11                   ~~c. Each dwelling unit is attached along at least one common wall to at~~  
12 ~~least one other dwelling unit, with habitable interior space on both sides of the common wall, or~~  
13 ~~abuts another dwelling unit on a common lot line;~~

14                   ~~d. The front of each dwelling unit faces a street lot line;~~

15                   ~~e. Each dwelling unit provides pedestrian access directly to the street that~~  
16 ~~it faces; and~~

17                   ~~f. No portion of any other dwelling unit, except for an attached accessory~~  
18 ~~dwelling unit, is located between any dwelling unit and the street faced by the front of that unit.~~

19                   ~~22. "Single family dwelling unit" means a detached principal structure having a~~  
20 ~~permanent foundation, containing one dwelling unit, except that the structure may also contain~~  
21 ~~one or two attached accessory dwelling units where expressly authorized pursuant to this Title~~

22 ~~23. A detached accessory dwelling unit is not considered a single family dwelling unit for~~  
23 ~~purposes of this Chapter 23.84A.~~



1                   23. ~~"Townhouse development" means a multifamily residential use that is not a~~  
2 ~~rowhouse development, and in which:~~

3                             a. ~~Each dwelling unit occupies space from the ground to the roof of the~~  
4 ~~structure in which it is located;~~

5                             b. ~~No portion of a dwelling unit occupies space above or below another~~  
6 ~~dwelling unit, except for an attached accessory dwelling unit and except for dwelling units~~  
7 ~~constructed over a shared parking garage, including shared parking garages that project up to 4~~  
8 ~~feet above grade; and~~

9                             c. ~~Each dwelling unit is attached along at least one common wall to at~~  
10 ~~least one other dwelling unit, with habitable interior space on both sides of the common wall, or~~  
11 ~~abuts another dwelling unit on a common lot line.))~~

12   \* \* \*

13                   Section ~~85~~ 86. Section 23.84A.036 of the Seattle Municipal Code, last amended by  
14 Ordinance ~~126157~~ 127285, is amended as follows:

15 **23.84A.036 “S”**

16   \* \* \*

17                   "Short subdivision" means the division or redivision of land into nine ~~((9))~~ or fewer  
18 lots, tracts, parcels, sites, or divisions for the purpose of sale, lease, development, or financing.

19                   “Short subdivision, zero lot line” means a short subdivision that conforms to the unit lot  
20 subdivision standards in Section 23.24.045.

21   \* \* \*

22                   “Social housing” means housing that is publicly owned, publicly financed, mixed-income  
23 housing developed by a ((~~Public Development Authority~~)) public development authority

1 organized pursuant to RCW 35.21.660, RCW 35.21.670, and RCW 35.21.730~~((-))~~ through  
2 35.21.755.

3 "Soil, structural" means a soil mix or equivalent structure approved by the Director that is  
4 engineered to support pavement while allowing healthy root growth.

5 "Solar access" means the amount of unrestricted sunlight that reaches a structure, or  
6 portion thereof.

7 "Solar collector" means ~~((any))~~ a device used to collect direct sunlight for use in the  
8 heating or cooling of a structure, domestic hot water, ~~((or))~~ swimming pool, or the generation of  
9 electricity, including photovoltaic panels and solar thermal panels.

10 ~~(("Solar greenhouse" means a solar collector that is a structure or portion of a structure  
11 utilizing glass or similar glazing material to collect direct sunlight for space heating purposes.))~~

12 \* \* \*

13 "Structure, accessory." See "Accessory structure."

14 "Structure, attached" means a structure that shares a common or party wall with another  
15 structure or have walls containing floor area that are located within 2 feet of another structure.

16 "Structure, detached" means a structure ~~((having no common or party wall with another  
17 structure))~~ that is not attached to any other structure.

18 \* \* \*

19 "Subdivision" means the division or redivision of land into ten ~~((10))~~ or more lots,  
20 tracts, parcels, sites, or divisions for the purpose of sale, lease, or transfer of ownership.

21 "Subdivision, zero lot line" means a subdivision that conforms to the unit lot subdivision  
22 standards in Section 23.22.062.

23 \* \* \*

1 Section ~~86~~ 87. Section 23.84A.038 of the Seattle Municipal Code, last amended by  
2 Ordinance ~~127211~~ 127228, is amended as follows:

3 **23.84A.038 "T"**

4 \* \* \*

5 "Transit route, frequent" means a transit route or segment of a transit route providing  
6 frequent transit service in each direction. Segments of overlapping routes that are co-scheduled  
7 and together provide frequent transit service shall be considered to provide frequent transit  
8 service, and segments of these routes that do not overlap or do not meet these frequencies will  
9 not be considered to provide frequent transit service.

10 "Transit service, frequent" means transit service with scheduled service in a typical week  
11 meeting or exceeding the following scheduled frequencies:

12 1. On weekdays from 6 a.m. to 7 p.m., 15 minutes on average (i.e., 52 trips  
13 between 6 a.m. and 6:59 p.m., inclusive), and no individual hour with fewer than three scheduled  
14 trips in each direction;

15 2. On weekdays from 7 p.m. to 12 a.m., 30 minutes on average (i.e., ten trips  
16 between 7 p.m. and 11:59 p.m., inclusive), and no individual hour with fewer than one scheduled  
17 trip in each direction; and

18 3. On weekends from 6 a.m. to 12 a.m., 30 minutes on average (i.e., 36 trips  
19 between 6 a.m. and 11:59 p.m., inclusive), and no individual hour with fewer than one scheduled  
20 trip in each direction.

21 4. For the purposes of this definition, "individual hour" means the 60-minute  
22 period beginning at the top of each hour; e.g., 6 a.m. to 6:59 a.m., inclusive, or 3 p.m. to 3:59  
23 p.m., inclusive.

1           “Transit service, major” means the following transit services:

2                   1. Commuter rail;

3                   2. Light rail or street car systems; and

4                   3. Bus rapid transit routes that are in operation or are funded for development and  
5 projected for construction within an applicable six-year transit plan under RCW 35.58.2795.

6           "Transit service area, frequent" means an area within 1,320 feet walking distance of a bus  
7 stop served by a frequent transit route or an area within 2,640 feet walking distance of a rail  
8 transit station, as shown on a map adopted by Director's Rule.

9           “Transit service area, major” means an area within 2,640 feet walking distance of a stop  
10 served by a major transit service, as shown on a map adopted by Director's Rule.

11           "Transit station, light rail." See "Rail transit facility" under "Transportation facility."

12           "Transit station access easement" means an easement for a pedestrian route or connection  
13 to provide direct access from street level to transit tunnel stations and concourses and/or light rail  
14 transit facilities.

15           "Transit station access, grade-level" means a pedestrian connection that provides direct  
16 access from street level to transit tunnel stations or concourses and/or light rail transit facilities at  
17 approximately the same level as the station mezzanine.

18           "Transit station access, mechanical" means a pedestrian connection that incorporates a  
19 mechanical device, such as an escalator, to provide direct access from street level to transit  
20 tunnel stations and concourses and/or light rail transit facilities.

21           “Transit stop, major” means a stop on a major transit service.

22   \* \* \*

1            ~~((“Triplex” means a single structure containing three dwelling units, none of which is a~~  
2 ~~legally established accessory dwelling unit.))~~

3   \* \* \*

4            Section ~~87~~ 88. Section 23.84A.040 of the Seattle Municipal Code, last amended by  
5 Ordinance 126862, is amended as follows:

6 **23.84A.040 "U"**

7   \* \* \*

8            "Unit, low-income" means a ~~((dwelling))~~ restricted unit that, for a minimum period of at  
9 least 50 years, is ~~((a restricted unit))~~ affordable to and reserved solely for ~~((families))~~ households  
10 with annual incomes not to exceed 60 percent of median income for rental units or 80 percent of  
11 median income for ownership units ~~((according to one or more regulatory agreements,~~  
12 ~~covenants, or other legal instruments that, as a condition to issuance of the first building permit~~  
13 ~~that includes the structural frame for the structure that includes the low income unit, shall be~~  
14 ~~executed and recorded on the title of the property and are enforceable by The City of Seattle,~~  
15 ~~King County, State of Washington, Washington State Housing Finance Commission, or other~~  
16 ~~public agency if approved by the Director of Housing)).~~

17            "Unit, moderate-income" means a ~~((dwelling))~~ restricted unit that, for a minimum period  
18 of at least 50 years, is ~~((a restricted unit))~~ affordable to and reserved solely for ~~((families))~~  
19 households with annual incomes not to exceed 80 percent of median income for rental units or  
20 100 percent of median income for ownership units ~~((according to one or more regulatory~~  
21 ~~agreements, covenants, or other legal instruments that, as a condition to issuance of the first~~  
22 ~~building permit that includes the structural frame for the structure that includes the moderate-~~  
23 ~~income unit, shall be executed and recorded on the title of the property and are enforceable by~~

1 ~~The City of Seattle, King County, State of Washington, Washington State Housing Finance~~  
2 ~~Commission, or other public agency if approved by the Director of Housing)).~~

3 "Unit, restricted" means a dwelling unit (~~((on a property))~~) subject to (~~((a recorded~~  
4 ~~agreement with the))~~) one or more regulatory agreements, covenants, or other legal instruments  
5 recorded on the title of the property and enforceable by The City of Seattle, King County, State  
6 of Washington, Washington State Housing Finance Commission, or other public agency, if  
7 approved by the Director of Housing, that for a specified number of years limits (~~((both the unit's~~  
8 ~~rent or sale price, as applicable, and eligible residents' annual income at a specified percentage of~~  
9 ~~median income. For purposes of each restricted unit, eligible residents shall be a "family"~~  
10 ~~according to 24 CFR Section 5.403 or successor provision, and the family's "annual income"~~  
11 ~~shall be determined according to 24 CFR Section 5.609 or successor provision, unless otherwise~~  
12 ~~approved in writing by the Director of Housing))~~) housing costs for income-eligible households,  
13 specified as a percentage of median income, as follows:

14 1. For renter-occupied housing, rental housing costs for each restricted unit shall  
15 not exceed 30 percent of the income limit; and

16 2. For owner-occupied housing, the initial sale price of each restricted unit shall  
17 be affordable to income-eligible households and resale prices must allow modest growth in  
18 homeowner equity while maintaining long-term affordability for subsequent eligible  
19 homebuyers, all as determined by the Director of Housing, consistent with Council-adopted  
20 Housing Funding Policies if funded by the Office of Housing or subsections 23.58C.050.C.7.a  
21 and 23.58C.050.C.7.b if not funded by the Office of Housing.

22 \* \* \*

1 Section ~~88~~ 89. Section 23.84A.046 of the Seattle Municipal Code, last amended by  
2 Ordinance 125603, is amended as follows:

3 **23.84A.046 "Y"**

4 (~~"Yard." See "Yard, front," "Yard, side" and "Yard, rear."~~

5 ~~"Yard, front" means an area from the ground upward between the side lot lines of a lot,~~  
6 ~~extending from the front lot line to a line on the lot parallel to the front lot line, the horizontal~~  
7 ~~depth of which is specified for each zone. The front yard includes all portions of the lot that are~~  
8 ~~within the specified distance from the street along which the front lot line extends, even if~~  
9 ~~separated from the street by an intervening lot. In the case of an irregularly shaped lot, the front~~  
10 ~~yard shall be a portion of the property as determined according to Section 23.86.010.~~

11 ~~"Yard, rear" means an area from the ground upward between the side lot lines of a lot,~~  
12 ~~extending from the rear lot line to a line on the lot parallel to the rear lot line, the horizontal~~  
13 ~~depth of which is specified for each zone. In the case of an irregularly shaped lot, the rear yard~~  
14 ~~shall be a portion of the property adjacent to the rear lot line as determined according to~~  
15 ~~subsection 23.86.010.C.~~

16 ~~"Yard, side" means an area from the ground upward between the front yard (or front lot~~  
17 ~~line if no front yard is required); and the rear yard (or rear lot line if no rear yard is required); and~~  
18 ~~extending from a side lot line to a line on the lot, parallel to the side lot line, the horizontal depth~~  
19 ~~of which is specified for each zone.))~~

20 \* \* \*

21 Section ~~89~~ 90. Section 23.84A.048 of the Seattle Municipal Code, last amended by  
22 Ordinance 126685, is amended as follows:

23 **23.84A.048 "Z"**

\* \* \*

~~((“Zone, neighborhood residential” means a zone with a classification that includes any of the following: NR1, NR2, NR3, and RSL.))~~

\* \* \*

“Zone, residential” means a zone with a classification that includes any of the following: ~~((NR1, NR2, NR3, RSL))~~ NR, LR1, LR2, LR3, MR, HR, RC, DMR, IDR, SM/R, SM-SLU/R, and SM-U/R which classification also may include one or more suffixes ~~((, but not including any zone with an RC designation))~~.

~~((“Zone, single family” means a zone with a classification that includes any of the following: Neighborhood Residential 1 (NR1), Neighborhood Residential 2 (NR2), Neighborhood Residential 3 (NR3), and Residential Small Lot (RSL).))~~

Section ~~90~~ 91. Section 23.86.002 of the Seattle Municipal Code, last amended by Ordinance 125791, is amended as follows:

**23.86.002 General provisions**

\* \* \*

**B. Fractions**

1. Unless otherwise indicated, if any measurement technique for determining the number of items required or allowed, including but not limited to motor vehicle parking, or required trees or shrubs, results in fractional requirements, any fraction up to and including 0.5 of the applicable unit of measurement shall be disregarded and fractions over 0.5 shall require the next higher full unit of measurement.

2. If any measurement technique for determining required minimum or allowed maximum dimensions, including but not limited to height, ~~((yards,))~~ setbacks, lot coverage, open



1 space, building depth, parking space size, or curb cut width, results in fractional requirements,  
2 the dimension shall be measured to the nearest inch. Any fraction up to and including 0.5 of an  
3 inch shall be disregarded and fractions over 0.5 of an inch shall require the next higher unit.

4 3. (~~Except within Lowrise and RSL zones, if density calculations result in a~~  
5 ~~fraction of a unit, any fraction up to and including 0.5 constitutes zero additional units, and any~~  
6 ~~fraction over 0.5 constitutes one additional unit. Within Lowrise zones, the effect of a density~~  
7 ~~calculation that results in a fraction of a unit is as described in Section 23.45.512. Within RSL~~  
8 ~~zones, the effect of a density calculation that results in a fraction of a unit is as described in~~  
9 ~~Section 23.44.017. This provision may not be applied to density calculations that result in a~~  
10 ~~quotient less than one.)) When calculation of the number of dwelling units allowed results in a  
11 fraction of a unit, any fraction over 0.85 constitutes one additional unit.~~

12 \* \* \*

13 Section ~~91~~ 92. Section 23.86.006 of the Seattle Municipal Code, last amended by  
14 Ordinance 126685, is amended as follows:

15 **23.86.006 Structure height measurement**

16 \* \* \*

17 B. Within the South Lake Union Urban Center, at the applicant's option, structure height  
18 shall be measured either as provided for in subsection 23.86.006.A(~~(, 23.86.006.E))~~) or  
19 23.86.006.D, or under provisions of this subsection 23.86.006.B. Structure height shall be  
20 measured for all portions of the structure. All measurements shall be taken vertically from  
21 existing or finished grade, whichever is lower, to the highest point of the structure located  
22 directly above each point of measurement. Existing or finished grade shall be established by  
23 drawing straight lines between the corresponding elevations at the perimeter of the structure. The

1 straight lines will be existing or finished grade for the purpose of height measurement. When a  
2 contour line crosses a facade more than once, that contour line will be disregarded when  
3 establishing existing or finished grade.

4 ~~C. ((Height averaging for neighborhood residential zones. In a neighborhood residential~~  
5 ~~zone, when expanding an existing structure occupied by a nonconforming residential use per~~  
6 ~~Section 23.42.106, the following measurement shall be used to determine the average height of~~  
7 ~~the closest principal structures on either side:~~

8 ~~1. Each structure used for averaging shall be on the same block front as the lot for~~  
9 ~~which a height limit is being established. The structures used shall be the nearest single family~~  
10 ~~structure on each side of the lot, and shall be within 100 feet of the side lot lines of the lot.~~

11 ~~2. The height limit for the lot shall be established by averaging the elevations of~~  
12 ~~the structures on either side in the following manner:~~

13 ~~a. If the nearest structure on either side has a roof with at least a 4:12~~  
14 ~~pitch, the elevation to be used for averaging shall be the highest point of that structure's roof~~  
15 ~~minus 5 feet.~~

16 ~~b. If the nearest structure on either side has a flat roof, or a roof with a~~  
17 ~~pitch of less than 4:12, the elevation of the highest point of the structure's roof shall be used for~~  
18 ~~averaging.~~

19 ~~c. Rooftop features which are otherwise exempt from height limitations~~  
20 ~~according to subsection 23.44.012.C, shall not be included in elevation calculations.~~

21 ~~d. The two elevations obtained from subsection 23.86.006.B.2.a and/or~~  
22 ~~subsection 23.86.006.B.2.b shall be averaged to derive the height limit for the lot. This height~~  
23 ~~limit shall be the difference in elevation between the midpoint of a line parallel to the front lot~~

1 ~~line at the required front setback and the average elevation derived from subsection~~  
2 ~~23.86.006.B.2.a and/or subsection 23.86.006.B.2.b.~~

3 ~~e. The height measurement technique used for the lot shall then be the~~  
4 ~~City's standard measurement technique, subsection 23.86.006.A.~~

5 ~~3. If there is no single family structure within 100 feet of a side lot line, or if the~~  
6 ~~nearest single family structure within 100 feet of a side lot line is not on the same block front,~~  
7 ~~the elevation used for averaging on that side shall be 30 feet plus the elevation of the midpoint of~~  
8 ~~the front lot line of the abutting vacant lot.~~

9 ~~4. If the lot is a corner lot, the height limit may be the highest elevation of the~~  
10 ~~nearest structure on the same block front, provided that the structure is within 100 feet of the side~~  
11 ~~lot line of the lot and that both front yards face the same street.~~

12 ~~5. In no case shall the height limit established according to these height averaging~~  
13 ~~provisions be greater than 40 feet.~~

14 ~~6. Lots using height averaging to establish a height limit shall be eligible for the~~  
15 ~~pitched roof provisions of subsection 23.44.012.B.~~

16 ~~D.))~~ Stories or portions of stories of a structure that are underground are not analyzed for  
17 purposes of structure height measurement.

18 ~~((E.))~~ D. Height measurement techniques in downtown zones and in the South Lake  
19 Union Urban Center

20 1. Determine the major street lot line, which shall be the lot's longest street lot  
21 line. When the lot has two or more street lot lines of equal length, the applicant shall choose the  
22 major street lot line.

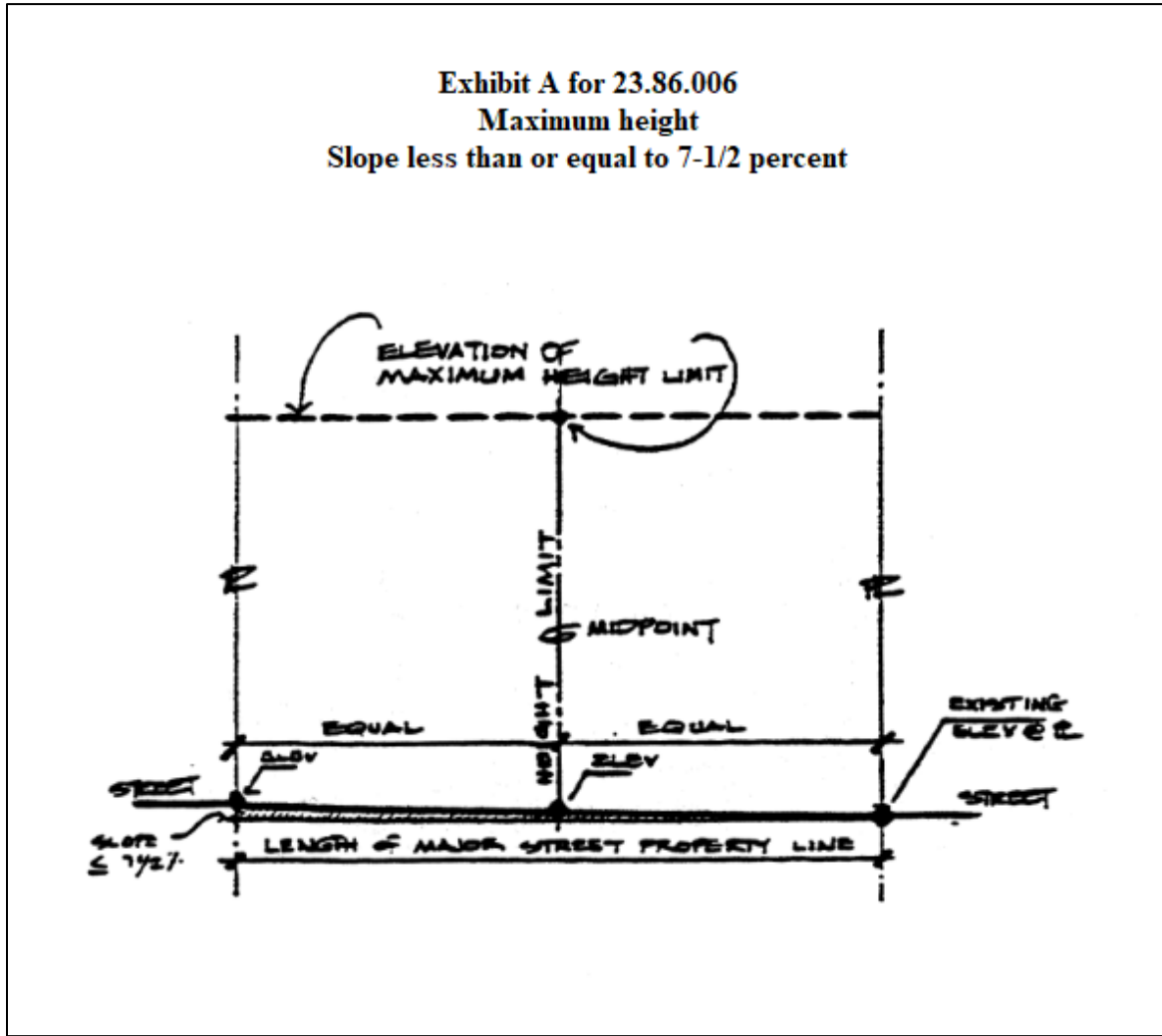
23 2. Determine the slope of the lot along the entire length of the major street lot line.

- 1                   3. ~~((The))~~ Measure the maximum height ~~((shall be measured))~~ as follows:
- 2                               a. When the slope of the major street lot line is less than or equal to 7.5
- 3 percent, the elevation of maximum height shall be determined by adding the maximum permitted
- 4 height to the existing grade elevation at the midpoint of the major street lot line. On a through-
- 5 lot, the elevation of maximum height shall apply only to the half of the lot nearest the major
- 6 street lot line. On the other half of a through-lot, the elevation of maximum height shall be
- 7 determined by the above method using the street lot line opposite and parallel to the major street
- 8 lot line as depicted in Exhibit ~~((B))~~ A for 23.86.006.

1 **Exhibit A for 23.86.006**

2 **Maximum height**

3 **Slope Less than or equal to 7-1/2 percent**



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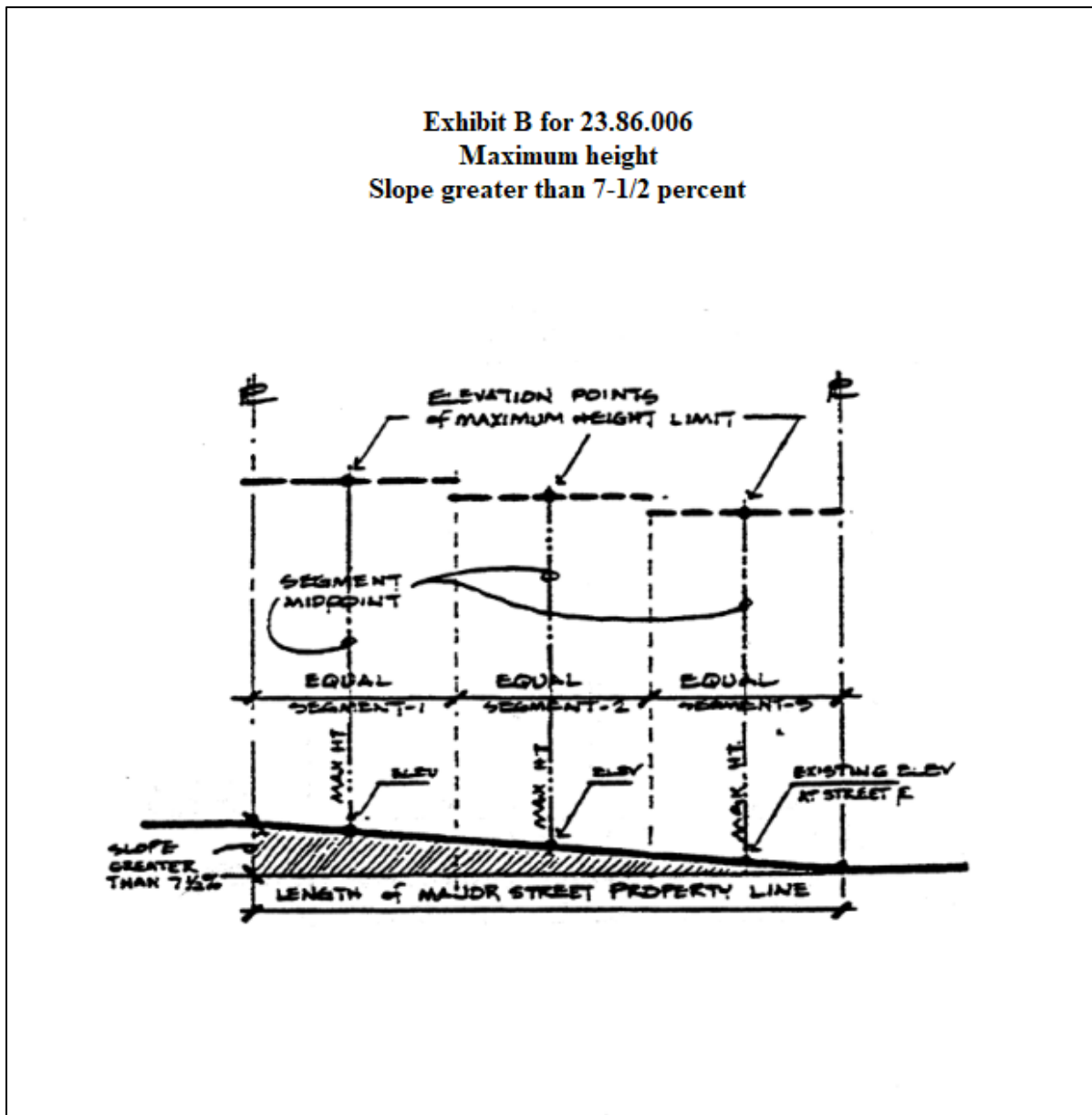
b. When the slope of the major street lot line exceeds 7.5 percent, the major street lot line shall be divided into four or fewer equal segments no longer than 120 feet in length. The elevation of maximum height shall be determined by adding the maximum permitted height to the existing grade elevation at the midpoint of each segment. On a through-lot, the elevation of maximum height shall apply only to the half of the lot nearest the major street lot

1 line. On the other half of a through-lot, the elevation of maximum height shall be determined by  
2 the above method using the street lot line opposite and parallel to the major street lot line, as  
3 depicted in Exhibit ((€)) B for 23.86.006.

4 **Exhibit B for 23.86.006**

5 **Maximum height**

6 **Slope greater than 7-1/2 percent**



7

1                                   c. For lots with more than one street frontage, where there is no street lot  
2 line that is essentially parallel to the major street lot line, when a measurement has been made for  
3 the portion of the block containing the major street lot line, the next measurement shall be taken  
4 from the remaining street lot line that is opposite and most distant from the major street lot line.

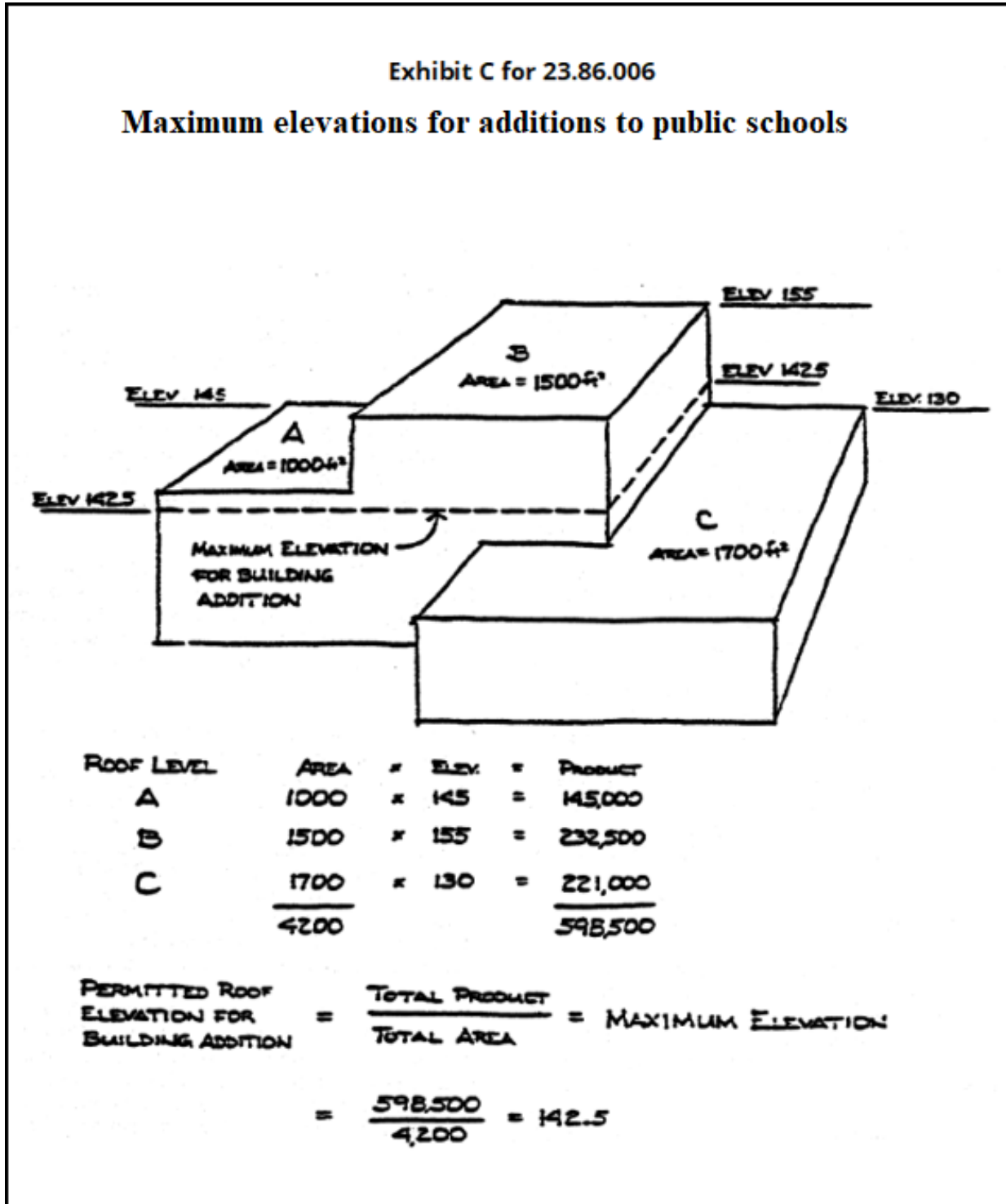
5                    (~~(F.)~~) E. Determining the height of existing public school structures. When the height of  
6 the existing public school structure is measured for purposes of determining the permitted height  
7 or lot coverage of a public school structure, either of the following measurement methods may be  
8 used:

9                                   1. If all parts of the new roof are pitched at a rate of not less than 4:12, the ridge  
10 of the new roof may extend to the highest point of the existing roof. A shed roof does not qualify  
11 for this option; or

12                                   2. If all parts of the new roof are not pitched at a rate of not less than 4:12, then  
13 the elevation of the new construction may extend to the average height of the existing structure.  
14 The average height shall be determined by measuring the area of each portion of the building at  
15 each height and averaging those areas, as depicted in Exhibit (~~(D)~~) C for 23.86.006.

1 Exhibit C for 23.86.006

2 Maximum elevations for additions to public schools

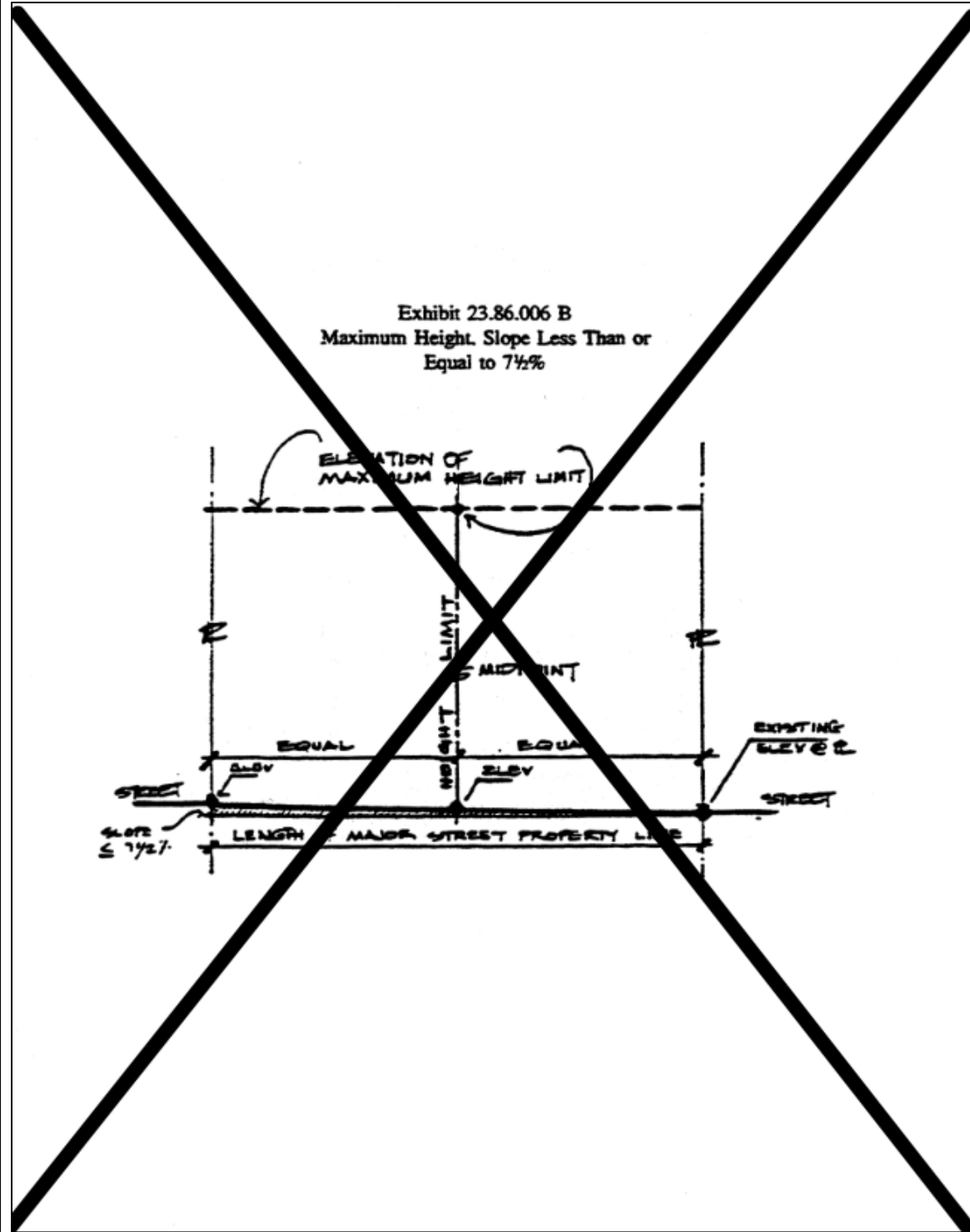


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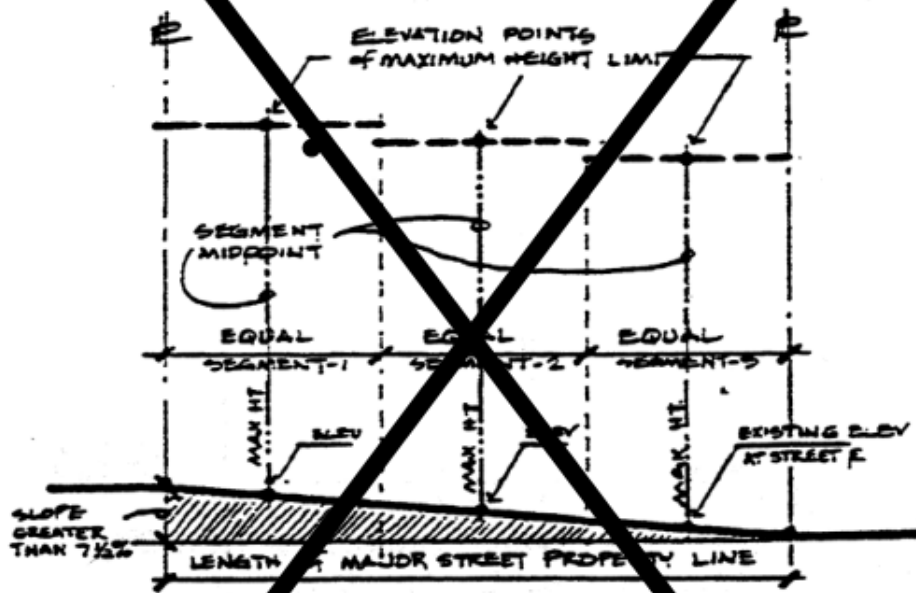
1            ~~((G.))~~ F. Height measurement technique for structures located partially within the  
2 Shoreline District. When any portion of the structure falls within the Shoreline District, structure  
3 height for the entire structure shall be measured according to Section 23.60A.952(~~(,Height)~~).

4            ~~((H.))~~ G. For projects accepted into the Living Building Pilot Program authorized  
5 pursuant to Section 23.40.060, the applicant may choose either the height definition of Chapter 2  
6 of the Seattle Building Code or the height measurement method described in this Section  
7 23.86.006.



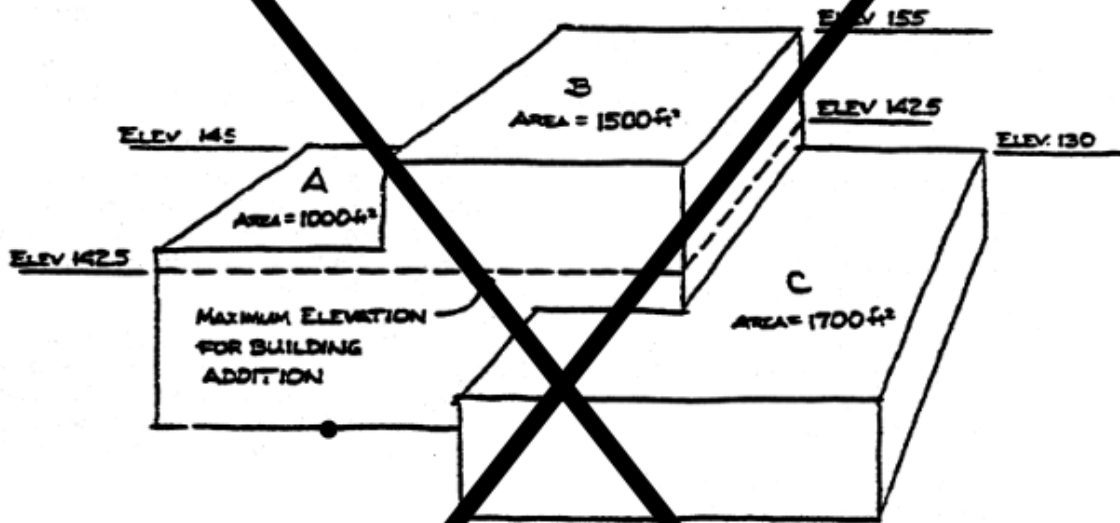
1

Exhibit 23.86.006 C  
Maximum Height, Slope Greater Than 7-1/2%



1  
2

Exhibit 23.86.006 D



ROOF LEVEL	AREA	=	ELEV.	=	PRODUCT
A	1000	x	145	=	145,000
B	1500	x	155	=	232,500
C	1700	x	130	=	221,000
	<u>4200</u>				<u>598,500</u>

$$\begin{aligned}
 \text{PERMITTED ROOF ELEVATION FOR BUILDING ADDITION} &= \frac{\text{TOTAL PRODUCT}}{\text{TOTAL AREA}} = \text{MAXIMUM ELEVATION} \\
 &= \frac{598,500}{4,200} = 142.5
 \end{aligned}$$

1

1 Section ~~92~~ 93. Section 23.86.007 of the Seattle Municipal Code, last amended by  
2 Ordinance 126855, is amended as follows:

3 **23.86.007 Floor area and floor area ratio (FAR) measurement**

4 A. Gross floor area. Except where otherwise expressly provided in this Title 23, gross  
5 floor area shall be as defined in Chapter 23.84A and as measured in this Section 23.86.007. The  
6 following are included in the measurement of gross floor area in all zones:

- 7 1. Floor area contained in stories above and below grade;
- 8 2. The area of stair penthouses, elevator penthouses, and other enclosed rooftop  
9 features;
- 10 3. The area of motor vehicle and bicycle parking that is enclosed; and
- 11 4. The area of motor vehicle parking that is covered by a structure or portion of a  
12 structure containing enclosed floor area, excluding motor vehicle parking in (~~neighborhood~~  
13 ~~residential~~) Neighborhood Residential and multifamily zones that is only covered by one of the  
14 following:

- 15 a. Projections containing enclosed floor area of up to 4 feet; or
- 16 b. Projections containing enclosed floor area of up to 6 feet for the area of  
17 parking accessed from an alley and located directly adjacent to an alley.

18 \* \* \*

19 D. Pursuant to subsections (~~23.44.011.C, 23.44.018.A,~~) 23.44.050.C, 23.45.510.D, and  
20 23.47A.013.B, and Section 23.48.020, for certain structures in (~~neighborhood residential~~)  
21 Neighborhood Residential, multifamily, commercial, and Seattle Mixed zones, portions of a  
22 story that extend no more than 4 feet above existing or finished grade, whichever is lower, are  
23 exempt from calculation of gross floor area. The exempt gross floor area of such partially below-  
24 grade stories is measured as follows:

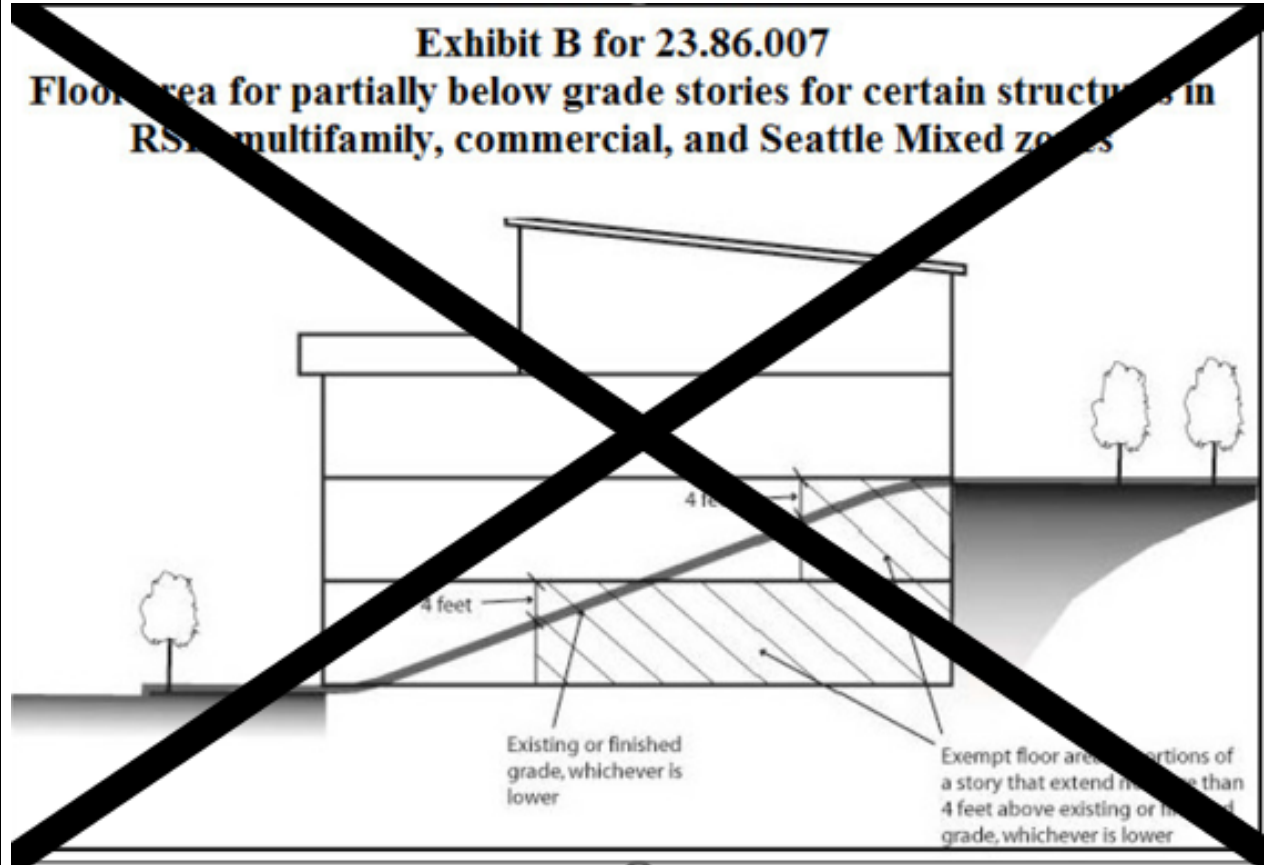
1                   1. Determine the elevation 4 feet below the ceiling of the partially below-grade  
2 story, or 4 feet below the roof surface if there is no next floor above the partially below-grade  
3 story;

4                   2. Determine the points along the exterior wall of the story where the elevation  
5 determined in subsection 23.86.007.D.1 intersects the abutting corresponding existing or finished  
6 grade elevation, whichever is lower;

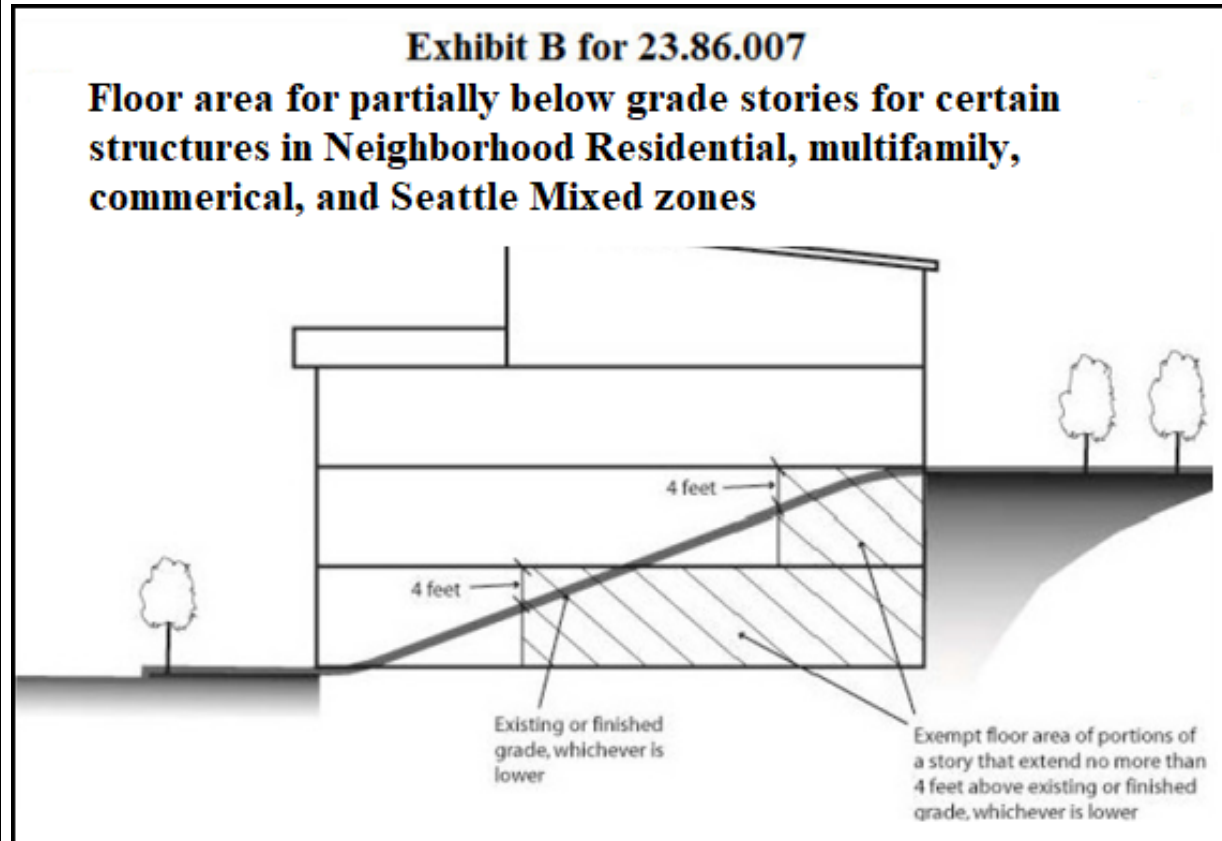
7                   3. Draw a straight line across the story connecting the two points on the exterior  
8 walls; and

9                   4. The gross floor area of the partially below-grade story or portion of a partially  
10 below-grade story is the area of the story that is at or below the straight line drawn in subsection  
11 23.86.007.D.3, excluding openings required by the Building Code for egress. (See Exhibit B for  
12 23.86.007.)

- 1 **Exhibit B for 23.86.007**
- 2 **Floor area for partially below grade stories for certain structures in ((RSL)) Neighborhood**
- 3 **Residential, multifamily, commercial, and Seattle Mixed zones**



- 4
- 5



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\* \* \*

Section ~~93~~ 94. Section 23.86.008 of the Seattle Municipal Code, last amended by Ordinance 126509, is amended as follows:

**23.86.008 Lot (~~coverage,~~) width (~~and depth,~~) in Neighborhood Residential zones**

~~((A. Lot coverage shall be calculated in accordance with Exhibit 23.86.008 A.~~

~~B. In neighborhood residential zones, lot depth shall be the length of the line extending between the front lot line or front lot line extended, and the rear lot line or lines, or in the case of a through lot, between the two (2) front lot lines or lines extended. This line shall be perpendicular to the front lot line or front lot line extended. Where an alley abuts the rear of the property, one half (1/2) of the width of the alley shall be included as a portion of the lot for determining lot depth.~~

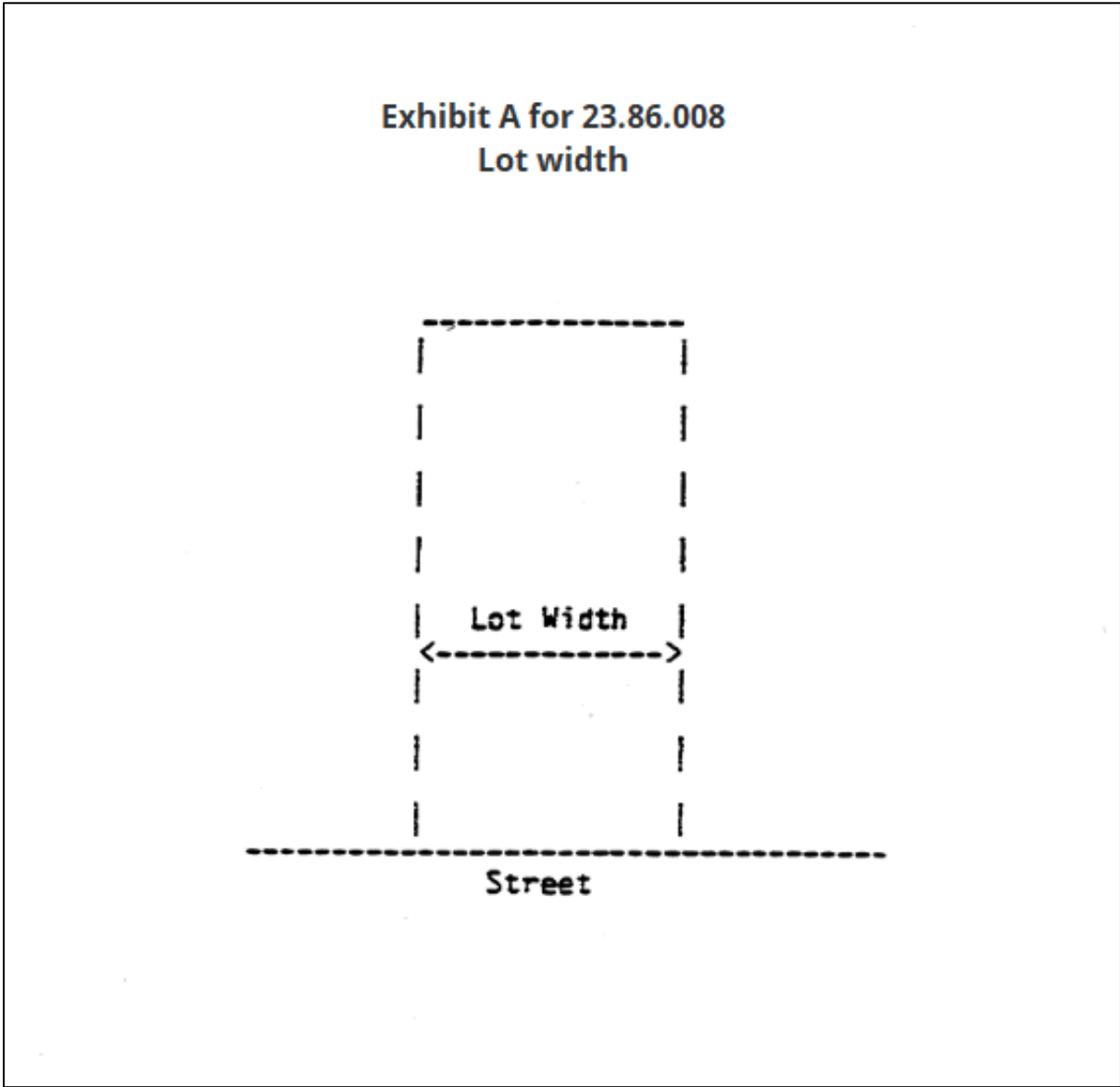
~~C. Lot Width in Neighborhood Residential Zones:~~



1            1.) A. When a lot is essentially rectangular, the lot width (~~shall be~~) is the mean  
2 horizontal distance between side lot lines measured at right angles to lot depth (~~Exhibit~~  
3 ~~23.86.008 B~~)) Exhibit A for 23.86.008.

4 **Exhibit A for 23.86.008**

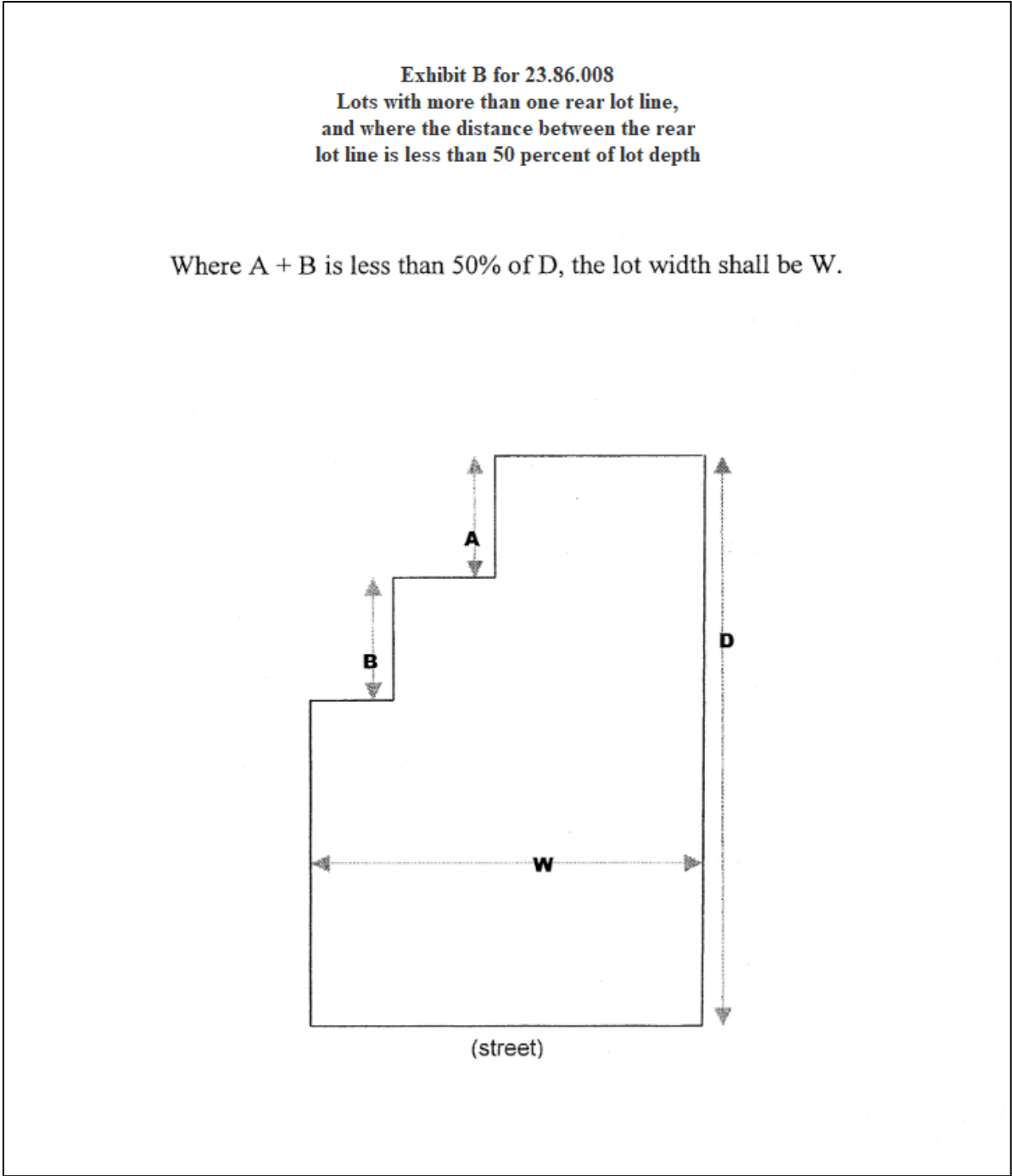
5 **Lot width**



6

1            ~~((2. In the case of))~~ B. For a lot with more than one ~~((1))~~ rear lot line ~~((Exhibits~~  
2 ~~23.86.008 C and 23.86.008 D))~~ (Exhibit B for 23.86.008 and Exhibit C for 23.86.008), the lot  
3 width shall be measured according to the following:

- 1 **Exhibit B for 23.86.008**
- 2 **Lots with more than one rear lot line, and where the distance between the rear lot line is**
- 3 **less than 50 percent of lot depth**



4

- 1 **Exhibit C for 23.86.008**
- 2 **Lots with more than one rear lot line, and where the distance between the rear lot line is**
- 3 **greater than 50 percent of lot depth**

**Exhibit C for 23.86.008**

**Lots with more than one rear lot line, and where the distance between the rear lot line is greater than 50 percent of lot depth**

Where A + B is greater than 50% of D:

Width of lot shall be:  $\frac{(A \times X) + (B \times Y) + (C \times Z)}{D}$

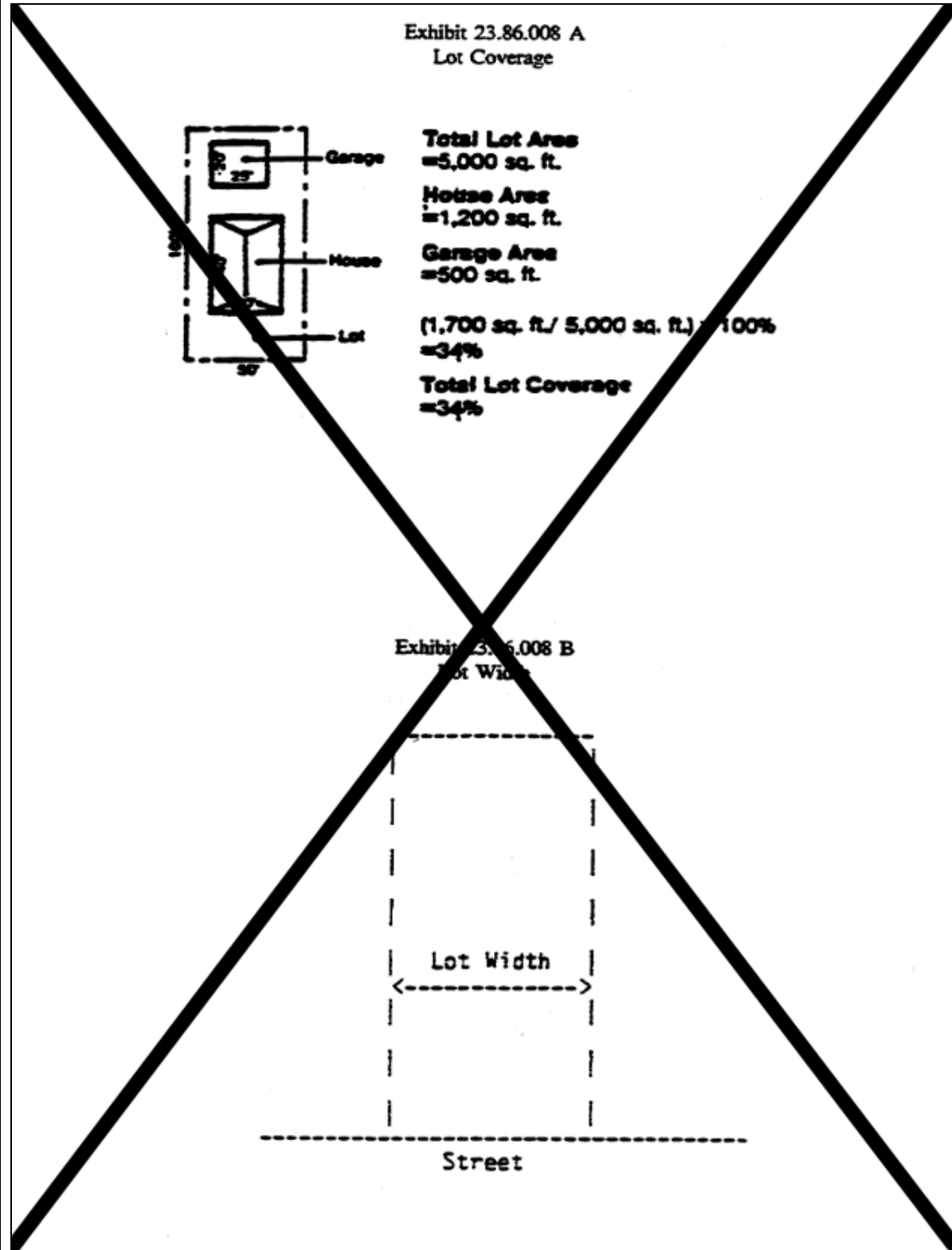
(Street)

4

1                                    ~~((a.))~~ 1. If the distance between the rear lot lines is ~~((fifty-))~~ 50 ~~((+))~~  
2 percent or less of the lot depth, the lot width shall be measured parallel to the front lot line and  
3 shall be the greatest distance between the side lot lines ~~((Exhibit 23.86.008 C))~~ Exhibit B for  
4 23.86.008; or

5                                    ~~((b.))~~ 2. If the distance between the rear lot lines is greater than ~~((fifty-))~~  
6 50 ~~((+))~~ percent of the lot depth, the lot width shall be determined by measuring average lot width  
7 according to ~~((Exhibit 23.86.008 D))~~ Exhibit C for 23.86.008.

8                                    ~~((3.))~~ C. For irregular lots not meeting the conditions of subsections ~~((C1 or C2))~~  
9 23.86.008.A or 23.86.008.B, the Director shall determine the measurement of lot width.



1

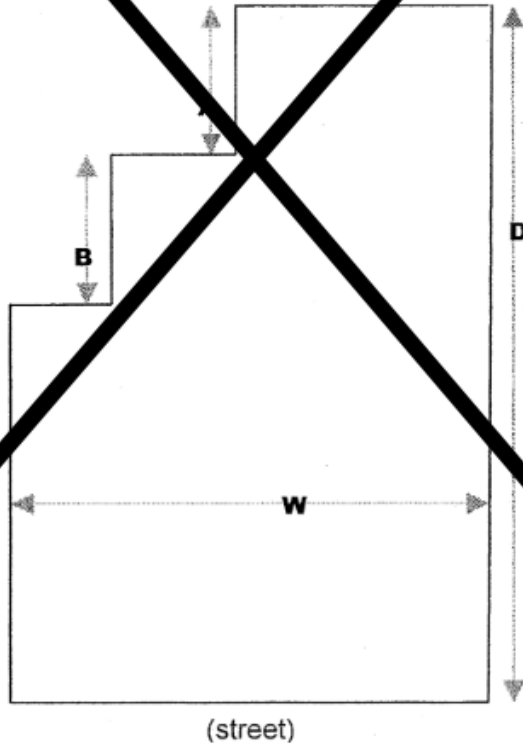
2

((Exhibits 23.86.008A, 23.86.008B))

**Exhibit 23.86.008 C**

Lots With More Than One Rear Lot Line,  
And Where The Distance Between The Rear  
Lot Line Is Less Than 50% Of Lot Depth

Where  $A + B$  is less than 50% of  $D$ , the lot width shall be  $W$ .



1  
2

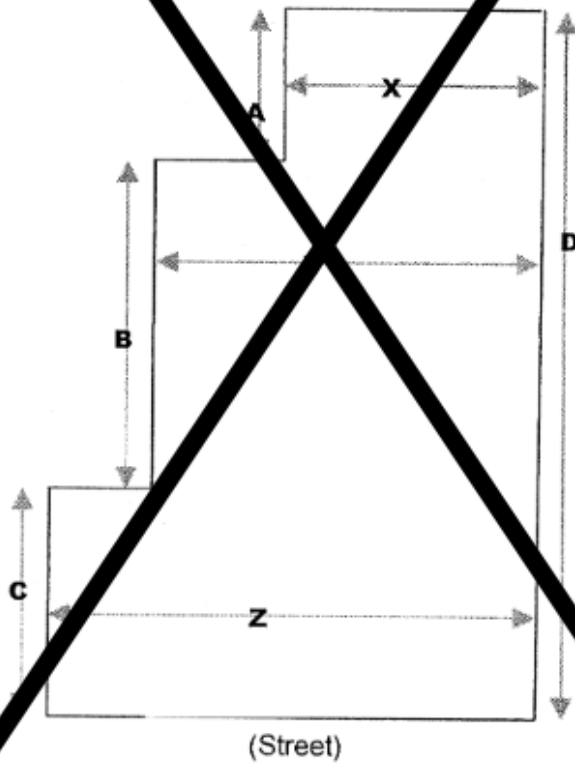
((Exhibit 23.86.008C))

**Exhibit 23.86.008 D**

Lots with More Than One Rear Lot Line, And Where  
The Distance Between the Rear Lot Line  
Is Greater than 50% Of Lot Depth

Where A + B is greater than 50% of D

Width of lot shall be:  $\frac{(A \times X) + (B \times Y) + (C \times Z)}{D}$





1 Section ~~94~~ 95. Section 23.86.010 of the Seattle Municipal Code, last amended by  
2 Ordinance 126509, is repealed:

3 ~~((23.86.010 Yards~~

4 ~~A. Measuring required yards. Required yard dimensions shall be horizontal distances,  
5 measured perpendicular to the appropriate lot lines (Exhibit A for 23.86.010). For lots with no  
6 street frontage, the applicant may designate the front lot line, provided that under the resulting  
7 orientation, the area of the front yard is at least 20 percent of the area of the lot or 1,000 square  
8 feet whichever is less. If a lot with frontage on more than one street is developed with an existing  
9 principal structure, the orientation of the lot for the purpose of current yard requirements shall be  
10 the orientation under which the existing structure is most conforming to current yard standards.~~

11 ~~B. Front Yards.~~

12 ~~1. Determining Front Yard Requirements. Front yard requirements are presented  
13 in the development standards for each zone. Where the minimum required front yard is to be  
14 determined by averaging the setbacks of structures on either side of a lot, the following  
15 provisions apply:~~

16 ~~a. The required depth of the front yard shall be the average of the distance  
17 between single family structures and front lot lines of the nearest single family structures on  
18 each side of the lot (Exhibit B for 23.86.010). If the front facade of the single family structure is  
19 not parallel to the front lot line, the shortest distance from the front lot line to the structure shall  
20 be used for averaging purposes (Exhibit C for 23.86.010).~~

21 ~~b. The yards used for front yard averaging shall be on the same block front  
22 as the lot, and shall be the front yards of the nearest single family structures within 100 feet of  
23 the side lot lines of the lot.~~

1                   e. ~~For averaging purposes, front yard depth shall be measured from the~~  
2 ~~front lot lines to the wall nearest to the street or, where there is no wall, the plane between~~  
3 ~~supports, which comprises 20 percent or more of the width of the front facade of the single-~~  
4 ~~family structure. Enclosed porches shall be considered part of the single family structure for~~  
5 ~~measurement purposes. Attached garages or carports permitted in front yards under 23.44.016.D,~~  
6 ~~decks, uncovered porches, eaves, attached solar collectors, and other similar parts of the structure~~  
7 ~~shall not be considered part of the structure for measurement purposes.~~

8                   d. ~~If there is a dedication of street right of way to bring the street abutting~~  
9 ~~the lot closer to the minimum widths established in Section 23.53.015, for averaging purposes~~  
10 ~~the amount of the dedication shall be subtracted from the front yard depth of the structures on~~  
11 ~~either side.~~

12                   e. ~~If the first single family structure within 100 feet of a side lot line of the~~  
13 ~~lot is not on the same block front, or does not provide its front yard on the same street, or if there~~  
14 ~~is no single family structure within 100 feet of the side lot line, the yard depth used for averaging~~  
15 ~~purposes on that side shall be 20 feet (Exhibits D and E for 23.86.010).~~

16                   f. ~~If the front yard of the first single family structure within 100 feet of the~~  
17 ~~side lot line of the lot exceeds 20 feet, the yard depth used for averaging purposes on that side~~  
18 ~~shall be 20 feet (Exhibit F for 23.86.010).~~

19                   g. ~~In cases where the street is very steep or winding, the Director shall~~  
20 ~~determine which adjacent single family structures should be used for averaging purposes.~~

21                   2. ~~Sloped Lots in Neighborhood Residential Zones. For a lot in a neighborhood~~  
22 ~~residential zone, reduction of the required front yard is permitted at a rate of 1 foot for every~~  
23 ~~percent of slope in excess of 35 percent. For the purpose of this provision the slope shall be~~

1 ~~measured along the centerline of the lot. In the case of irregularly shaped lots, the Director shall~~  
2 ~~determine the line along which slope is calculated.~~

3 ~~C. Rear yards. Rear yard requirements are presented in the standard development~~  
4 ~~requirements for each zone. In determining how to apply these requirements, the following~~  
5 ~~provisions shall apply:~~

6 ~~1. The rear yard shall be measured horizontally from the rear lot line if the lot has~~  
7 ~~a rear lot line that is essentially parallel to the front lot line for its entire length.~~

8 ~~2. If the front lot line is essentially parallel to portions of the rear property line, as~~  
9 ~~with a stepped rear property line, each portion of the rear property line that is opposite and~~  
10 ~~essentially parallel to the front lot line is considered to be a rear lot line for the purpose of~~  
11 ~~establishing a rear yard.~~

12 ~~3. On a lot with a rear property line, part of which is not essentially parallel to any~~  
13 ~~part of the front lot line, the rear yard is measured from a line or lines drawn from side lot line(s)~~  
14 ~~to side lot line(s), at least 10 feet in length, parallel to and at a maximum distance from the front~~  
15 ~~lot line. If an alley abuts the rear of the property, 1/2 the width of the alley, between the side lot~~  
16 ~~lines extended, is considered to be part of the lot for drawing this line. For those portions of the~~  
17 ~~rear lot line that are essentially parallel to the front lot line, subsection 23.86.010.C.2 above shall~~  
18 ~~apply. The lot depth is then measured perpendicularly from this 10 foot long line extended as~~  
19 ~~needed to the point on the actual front lot line that is the furthest distance away. This establishes~~  
20 ~~lot depth, which then may be used to determine the required rear yard depth.~~

21 ~~4. For a lot with a curved front lot line, the rear yard is measured from a line at~~  
22 ~~least 10 feet in length, parallel to and at a maximum distance from a line drawn between the~~  
23 ~~endpoints of the curve. The lot depth is then measured perpendicularly from this 10 foot long~~

1 ~~line extended as needed to the point on the actual front lot line that is the furthest distance away.~~

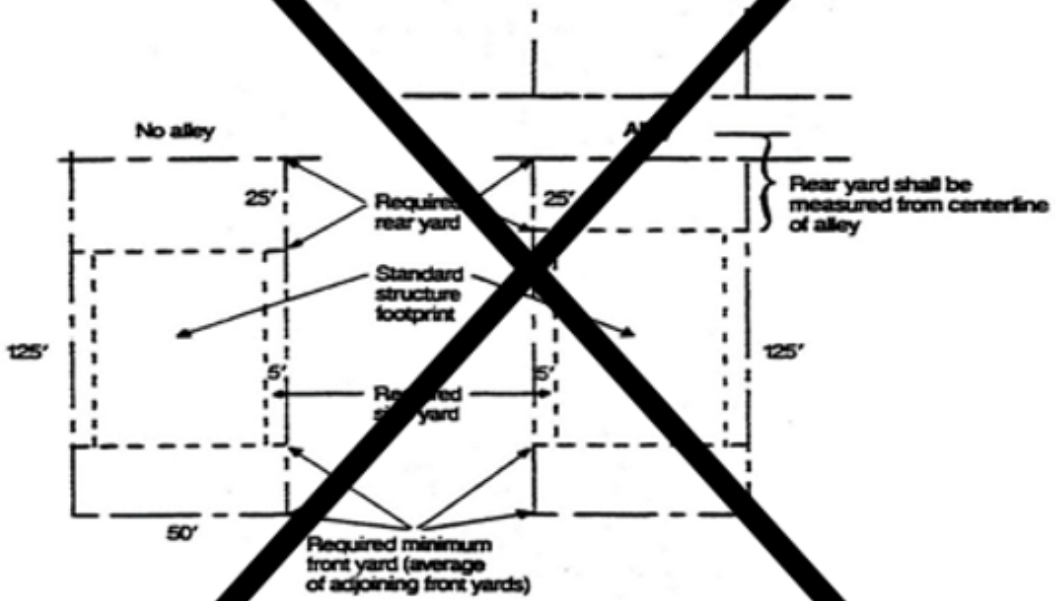
2 ~~This establishes lot depth, which then may be used to determine the required rear yard depth.~~

3 ~~5. For a lot with an irregular shape or with an irregular front lot line not meeting~~  
4 ~~conditions of subsections 23.86.010.C.1 through 23.86.010.C.4, the Director shall determine the~~  
5 ~~measurement of the rear yard.~~

6 ~~D. Side Yards.~~

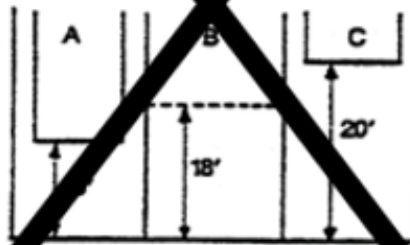
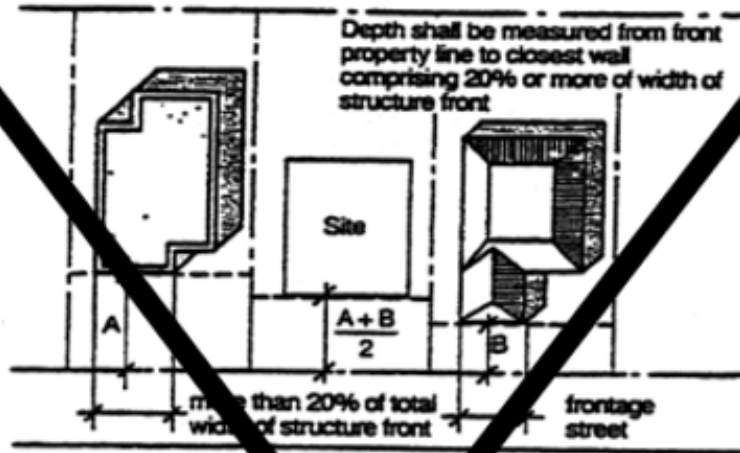
7 ~~1. Side Yard Averaging. Side yard requirements are presented in the standard~~  
8 ~~development requirements for each zone. In certain cases where specifically permitted, the side~~  
9 ~~yard requirement may be satisfied by averaging the distance from side lot line to structure facade~~  
10 ~~for the length of the structure. In those cases the side yard shall be measured horizontally from~~  
11 ~~side lot line to the side facade of the structure.))~~

**Exhibit 23.86.010 A  
Standard Required Yards  
(NR Zone Example)**



1

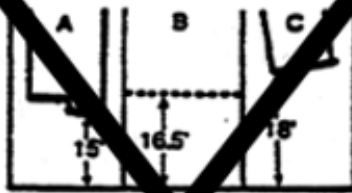
Exhibit B for 23.86.010  
Determination of Front Yard Setback



- Required minimum front setback for Lot B determined as follows:
1. Front setback, Lot A = 16'.
  2. Front setback, Lot C = 20'.
  3. Average front setback = 18'.
  4. Required minimum front setback for Lot B = 18'.

Exhibit C for 23.86.010

Calculating Minimum Required Front Yard  
Unusual Front Walls



Minimum required front yard for Lot B:

1. Front yard, Lot A = 15'.
2. Front yard, Lot C = 18'.
3. Average front yard = 16.5'.
4. Required minimum front yard for lot B = 16.5'

Exhibit D for 23.86.010

1. Front yard, Lot D = 16'.
2. Lot B unimproved.
3. Lot A not on same block front.
4. Use 20' for averaging purposes on west side.
5. Minimum required front yard,  
 $Lot C = (20 + 16)/2 = 18'$ .





Exhibit E for 23.86.010

**Minimum Required Front Yards, Adjoining Lots Unimproved**

1. Front yard, Lot F = 18'.
2. Lots B, C, D unimproved.
3. Use 20' for averaging purposes on west side.
4. Minimum required front yard,  
Lot E =  $(20 + 18)/2 = 19'$ .

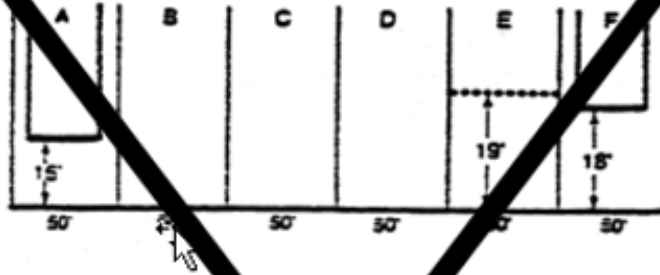
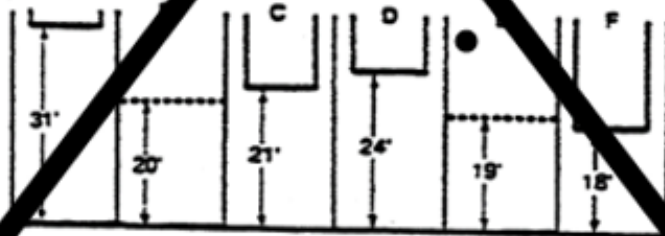


Exhibit F for 23.86.010

1. Minimum required front yard,  
Lot B =  $(20 + 20)/2 = 20'$ .
2. Minimum required front yard,  
Lot E =  $(20 + 18)/2 = 19'$ .



1 Section ~~95~~ 96. Section 23.86.012 of the Seattle Municipal Code, last amended by  
2 Ordinance 125791, is amended as follows:

3 **23.86.012 ((Multifamily and commercial zone setback)) Setback and separations**  
4 **measurement**

5 A. For purposes of setback and separation standards, measurement shall be taken to the  
6 outside of building foundations and exterior walls rather than to exterior finishing provided that  
7 exterior finishes extend no more than 8 inches into a required setback.

8 B. Setback averaging. In multifamily and commercial zones, certain required setbacks  
9 may be averaged. In such cases ~~((the following provisions apply))~~:

10 1. The average front and rear setbacks are calculated based on the entire width of  
11 the structure;

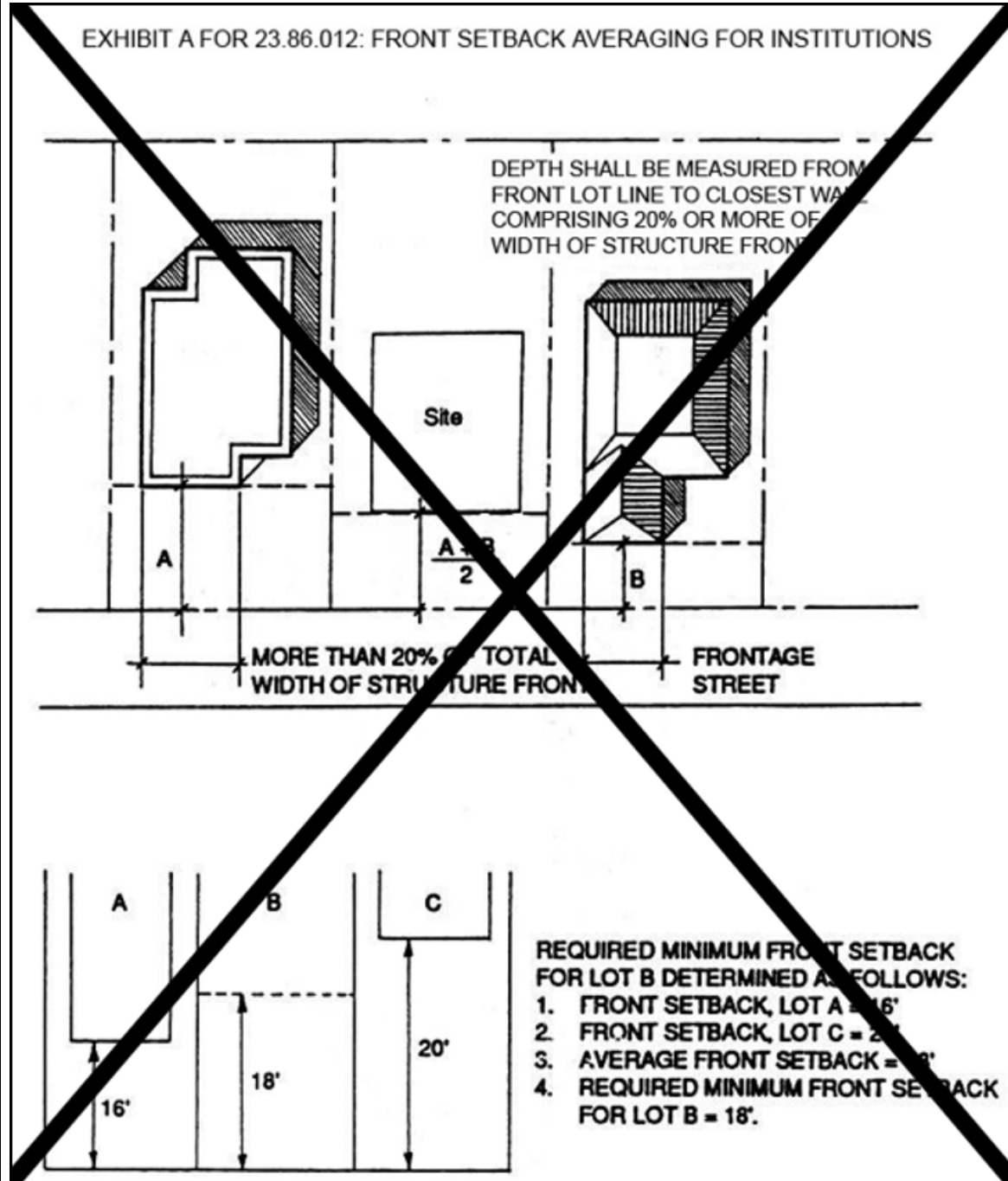
12 2. The average side setbacks are calculated based on the entire depth of the  
13 structure;

14 3. Setbacks are measured horizontally from the lot line to the facade of the  
15 structure. The facade(s) used in calculating the average and minimum setback requirements shall  
16 be those facades that are nearest to that lot line except that any features allowed to project into  
17 the setback are excluded.

18 ~~((B. Determining front setbacks for institutions. In LR zones, the minimum required front~~  
19 ~~setback for institutions is determined by averaging the setbacks of structures on either side of the~~  
20 ~~subject lot, as follows:~~

21 1. ~~The required front setback is the average of the distances between principal~~  
22 ~~structures and front lot lines of the nearest principal structures on each side of the subject lot if~~

- 1 each of those structures is on the same block front as the subject lot and is within 100 feet of the  
2 side lot lines of the subject lot (Exhibit A for 23.86.012).



3

1                   ~~2. If the first principal structure within 100 feet of a side lot line of the subject lot~~  
2 ~~is not on the same block front or there is no principal structure within 100 feet of the side lot line,~~  
3 ~~the setback depth used for averaging purposes on that side is 7 feet.~~

4                   ~~3. For averaging purposes, the front setback is the shortest distance from the front~~  
5 ~~lot line to the nearest wall or, where there is no wall, the plane between supports that span 20~~  
6 ~~percent or more of the width of the front facade of the principal structure. Attached garages and~~  
7 ~~enclosed porches are considered part of the principal structure for measurement purposes. Decks~~  
8 ~~less than 18 inches above existing grade, uncovered porches, eaves, attached solar collectors and~~  
9 ~~other similar parts of the structure are not considered part of the principal structure.~~

10                   ~~4. If there is a dedication of street right of way to bring the street abutting the lot~~  
11 ~~closer to the minimum widths established in Section 23.53.015, for averaging purposes the~~  
12 ~~amount of dedication is subtracted from the front setbacks of the structures on either side.~~

13                   ~~5. If the front setback of the first principal structure within 100 feet of the side lot~~  
14 ~~line of the subject lot exceeds 20 feet, the setback depth used for averaging purposes on that side~~  
15 ~~is 20 feet.~~

16                   ~~6. In cases where the street is very steep or winding, the Director will determine~~  
17 ~~which adjacent structures should be used for averaging purposes.~~

18                   ~~7. In the case of a through lot, the front setback is determined independently for~~  
19 ~~each street frontage. The measurement techniques of this section 23.86.012 apply to each street~~  
20 ~~frontage separately.~~

21                   ~~8. For multiple structures on the same lot, the front setback of a principal structure~~  
22 ~~on the same lot may be used for averaging purposes.))~~

23                                   \* \* \*

1 Section ~~96~~ 97. Section 23.86.017 of the Seattle Municipal Code, enacted by Ordinance  
2 123495, is amended as follows:

3 **23.86.017 Amenity area measurement**

4 ~~((Certain zones require a minimum amount of amenity area to be provided on the lot.))~~ If

5 amenity area is required, the following provisions shall apply:

6 A. If the applicable development standards specify a minimum contiguous amenity area,  
7 areas smaller than the minimum contiguous area are not to be counted toward fulfilling amenity  
8 area requirements.

9 1. Driveways and vehicular access easements, whether paved or unpaved, shall be  
10 considered to separate the amenity areas they bisect(~~(, except for woonerfs permitted to qualify~~  
11 ~~as required amenity area)).~~

12 2. Pedestrian access areas shall not be considered to break the contiguity of  
13 amenity area on each side.

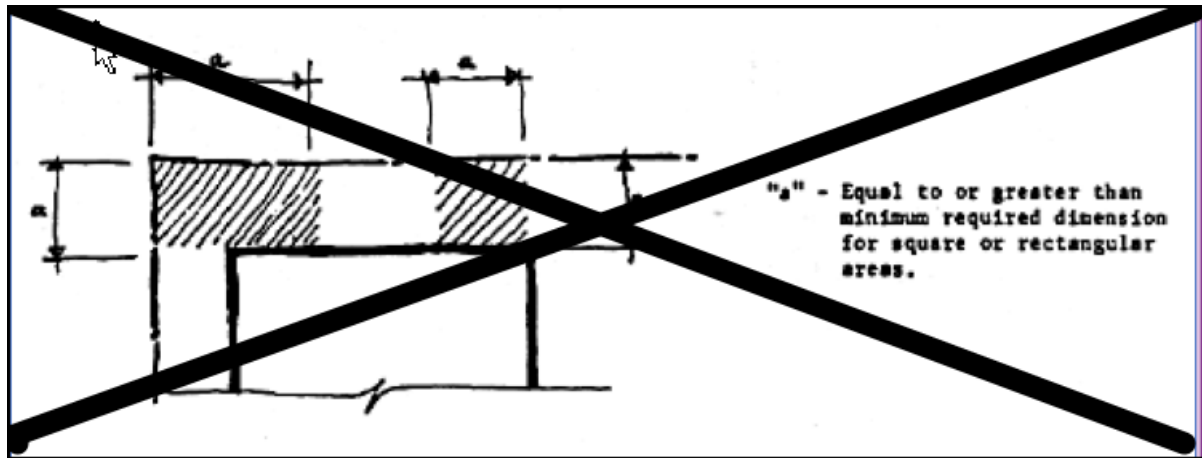
14 B. In shoreline areas, when determining the amount of amenity area required or provided,  
15 no land waterward of the ordinary high water mark shall be included in the calculation.

16 C. In cases where the shape or configuration of the amenity area is irregular or unusual,  
17 the Director shall determine whether amenity area requirements have been met, notwithstanding  
18 the following provisions, based on whether the proposed configuration would result in amenity  
19 area that is truly usable for normal residential recreational purposes. For the purpose of  
20 measuring the minimum horizontal dimension of the amenity area, if one is specified, the  
21 following provisions shall apply:

22 1. For rectangular or square areas, each exterior dimension of the area shall meet  
23 the minimum dimension (Exhibit A for 23.86.017).

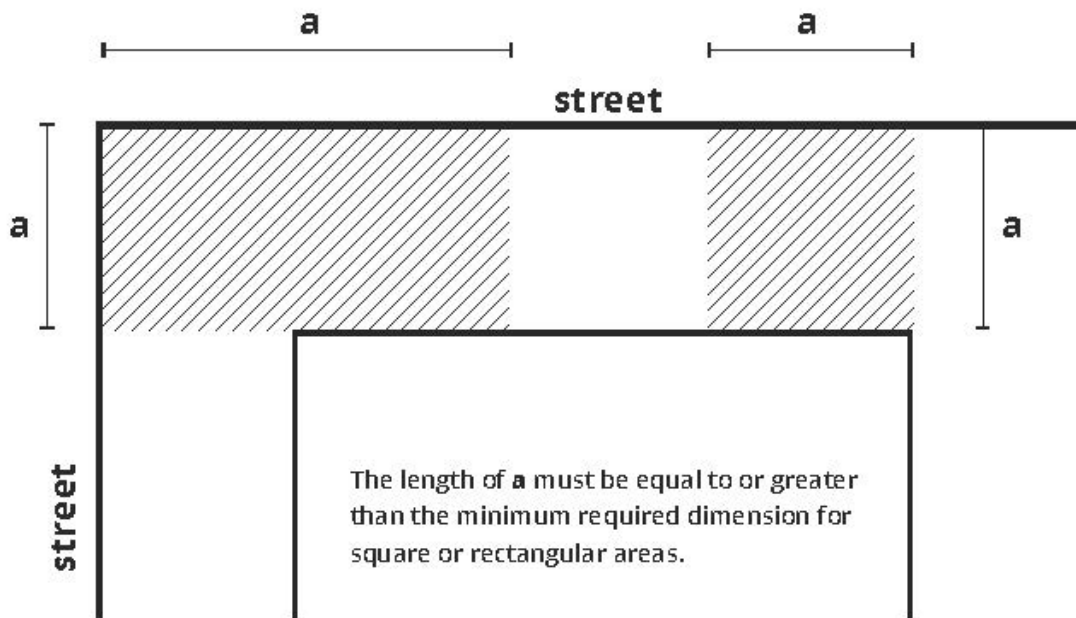
1 **Exhibit A for ((Section)) 23.86.017((= Measurement of Regular Amenity Area))**

2 **Measurement of amenity area**



3

**Exhibit A for 23.86.017  
Measurement of amenity area**

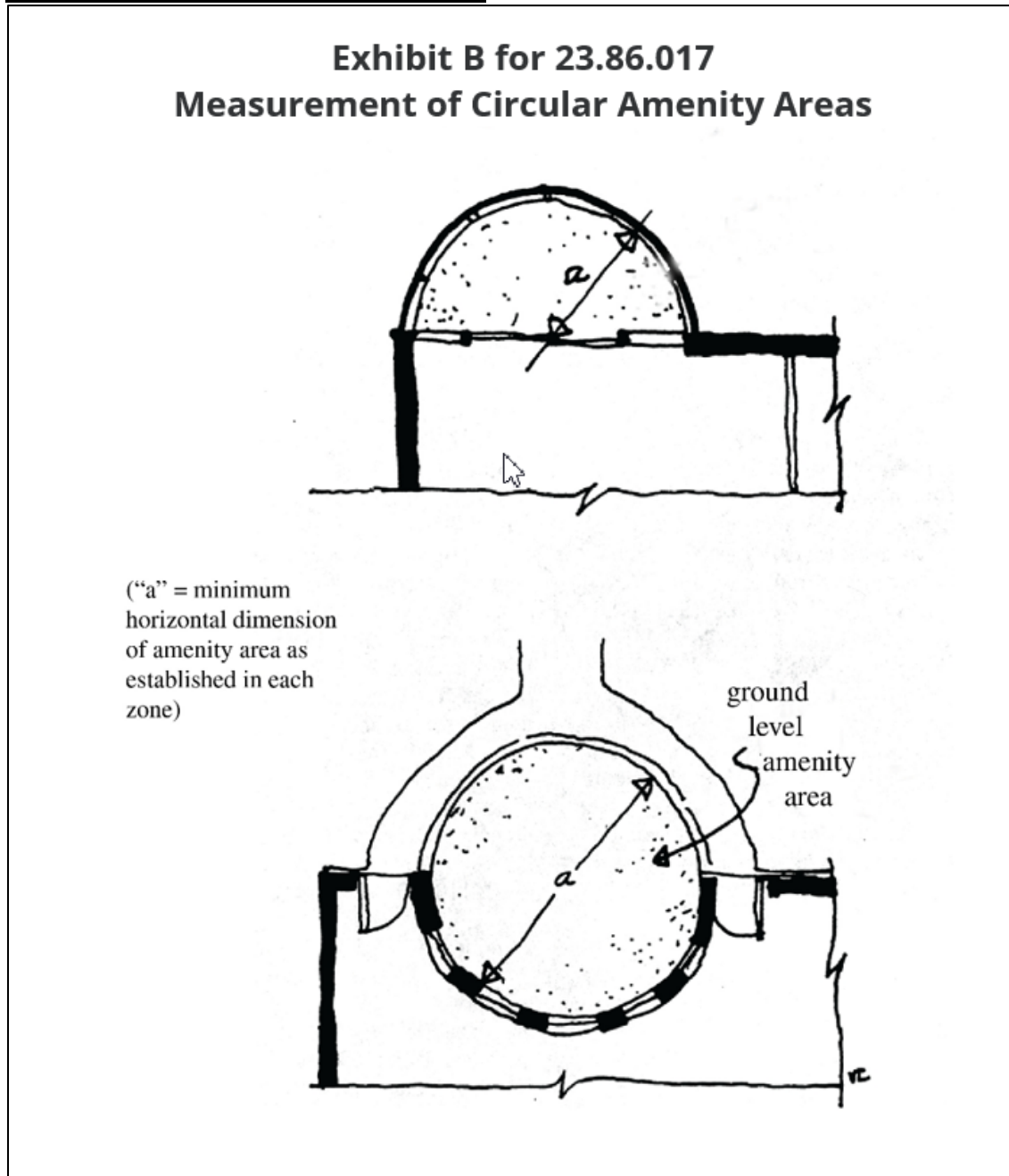


4

1                    2. For circular areas, the diameter of the circle shall meet the minimum  
2 dimension((;f0f)) . For semicircular areas, the radius of the area shall meet the minimum  
3 dimension (Exhibit B for 23.86.017).

4 **Exhibit B for 23.86.017((; Measurement of Circular Amenity Areas))**

5 **Measurement of circular amenity areas**



6  
7

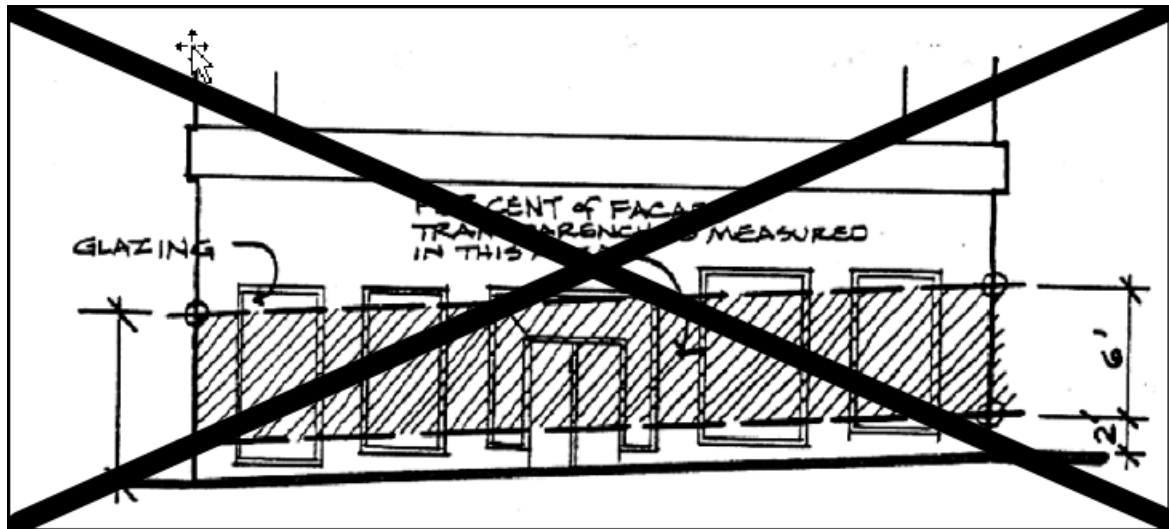
1 Section ~~97~~ 98. Section 23.86.026 of the Seattle Municipal Code, last amended by  
2 Ordinance 124503, is amended as follows:

3 **23.86.026 Facade transparency**

4 A. In zones, other than Neighborhood Residential or Lowrise zones, where a certain  
5 percentage of the street-facing facade is required to be transparent, transparency shall be  
6 measured in an area between 2 feet and 8 feet above the elevation of the lot line at the sidewalk,  
7 as depicted in Exhibit A for 23.86.026, unless a different area is specified in the development  
8 standards applicable to the lot. Areaways, stairways, and other excavations at the lot line shall  
9 not be considered in measuring the elevation of the street lot line. When sidewalk widening is  
10 required according to Section 23.49.022, the elevation of the lines establishing the new sidewalk  
11 width shall be used rather than the street lot line.

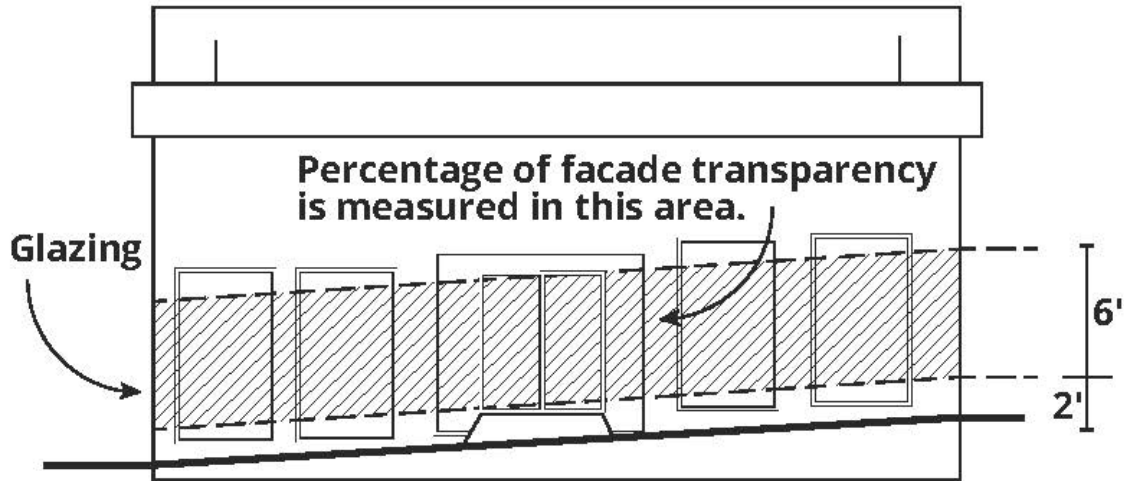
12 **Exhibit A for 23.86.026**

13 **Street ((~~Facade Transparency~~)) facade transparency**





## Exhibit A for 23.86.026 Street facade transparency



1  
2 B. When transparency is required for facades that abut bonused public open spaces, the  
3 measurement of facade transparency shall be from the elevation of the public open space.

4 C. The full length of ~~((landmark))~~ Landmark designated structures, and character  
5 structures retained according to Section 23.73.015, shall not be counted in determining the  
6 required transparency.

7 Section ~~98~~ 99. Section 23.90.019 of the Seattle Municipal Code, last amended by  
8 Ordinance 127211, is amended as follows:

9 **23.90.019 Civil penalty for unauthorized dwelling units ~~((in neighborhood residential~~  
10 ~~zones))~~**

11 In addition to any other sanction or remedial procedure that may be available, the following  
12 penalties apply to unauthorized dwelling units ~~((in neighborhood residential zones in violation of~~  
13 ~~Section 23.44.006))~~. An owner of a ~~((neighborhood residential zoned))~~ lot ~~((that has more than~~

1 ~~one single-family dwelling unit and~~) who is issued a notice of violation for an unauthorized  
2 dwelling unit((;)) is subject to a civil penalty of \$5,000 for each ~~((additional))~~ unauthorized  
3 dwelling unit(~~(, unless the additional unit is an authorized dwelling unit in compliance with~~  
4 ~~Section 23.42.022, is a legal non-conforming use, or is approved as part of an administrative~~  
5 ~~conditional use permit pursuant to Section 25.09.260)).~~ Penalties for ~~((violation of Sections~~  
6 ~~23.44.006 and 23.44.022 except for those violations subject to subsection 23.90.018.B,))~~  
7 unauthorized dwelling units in this Section 23.90.019 shall be reduced from \$5,000 to \$500 if,  
8 prior to the compliance date stated on the notice of violation for an unauthorized dwelling unit,  
9 the dwelling unit is removed or authorized ~~((, is a legal non-conforming use, or is approved as~~  
10 ~~part of an administrative conditional use permit pursuant to Section 25.09.260)).~~

11 Section ~~99~~ 100. Section 23.91.002 of the Seattle Municipal Code, last amended by  
12 Ordinance 126509, is amended as follows:

13 **23.91.002 Scope of this Chapter 23.91**

14 A. Violations of the following provisions of this Title 23 shall be enforced under the  
15 citation or criminal provisions set forth in this Chapter 23.91:

16 1. Junk storage in residential zones ~~((Chapter 23.44, Chapter 23.45, Chapter~~  
17 ~~23.46, Chapter 23.49 Subchapter IV, and Chapter 23.49 Subchapter VII))~~), unless the lot  
18 contains a vacant structure subject to the vacant building maintenance standards contained in  
19 subsection 22.206.200.A and a notice of violation has been issued requiring compliance with  
20 subsection 22.206.200.F;

21 2. Construction or maintenance of structures in required ~~((yards or))~~ setbacks in  
22 residential zones ~~((Chapter 23.44, Chapter 23.45, Chapter 23.46, Chapter 23.49 Subchapter IV,~~  
23 ~~and Chapter 23.49 Subchapter VII))~~);

1                   3. Parking of vehicles in a (~~neighborhood residential~~) Neighborhood Residential  
2 zone (Section (~~23.44.016~~) 23.44.160), unless the lot contains a vacant structure subject to the  
3 vacant building maintenance standards contained in subsection 22.206.200.A;

4                   4. Keeping of animals (Section 23.42.052); and

5                   (~~5. Reserved.~~)

6                   ~~6.~~) 5. The following violations of (~~the Shoreline District,~~) Chapter 23.60A:

7                   a. Discharging, leaking, or releasing solid or liquid waste and untreated  
8 effluent, oil, chemicals, or hazardous materials into the water (subsection 23.60A.152.R);

9                   b. Releasing debris and other waste materials from construction,  
10 maintenance, repair, or in operation or management of a property, into any water body  
11 (subsections 23.60A.152.H, 23.60A.152.I, 23.60A.152.T, and 23.60A.152.U);

12                   c. Conducting activity in or over water outside the allowed work windows  
13 (subsection 23.60A.152.J); and

14                   d. Closing required public access (Section 23.60A.164).

15                   B. Any enforcement action or proceeding pursuant to this Chapter 23.91 shall not affect,  
16 limit, or preclude any previous, pending, or subsequent enforcement action or proceeding taken  
17 pursuant to Chapter 23.90.

18                   Section ~~100~~ 101. Section 25.09.052 of the Seattle Municipal Code, last amended by  
19 Ordinance 126685, is amended as follows:

20 **25.09.052 Replacing structures in environmentally critical areas and buffers**

21                   \* \* \*

22                   B. Replacing a (~~single-family residence~~) detached dwelling unit voluntarily in wetlands,  
23 wetland buffers, and fish and wildlife habitat conservation areas

1                   1. Replacing a (~~single-family residence~~) detached dwelling unit and its  
2 appurtenant structures and access is allowed in wetlands, wetland buffers, and fish and wildlife  
3 habitat conservation areas if the replacement complies with the following:

4                   a. The replacement is in substantially the same location as the original  
5 development;

6                   b. The area of the footprint of the replacement does not exceed that of the  
7 original development;

8                   c. The proposed access does not exceed the width and length of necessary  
9 access;

10                  d. Lot size

11                         1) Riparian watercourse and wetlands. For a (~~single-family~~  
12 ~~residence~~) detached dwelling unit located over a riparian watercourse or built in a wetland, the  
13 replaced (~~residence~~) dwelling unit and necessary access meets wetland buffer or riparian  
14 management area requirements to the maximum extent feasible; or

15                         2) For all other property, the lot does not have sufficient area to  
16 site a (~~residence~~) dwelling unit with the same area of footprint as existed on May 14, 2017, plus  
17 necessary access, consistent with the regulations for the applicable environmentally critical area  
18 and buffer, including reducing the (~~yard and~~) front and/or rear setback requirements (~~for front~~  
19 ~~and rear yards in Title 23~~) allowed under Section 25.09.280, except subsection 25.09.280.B.2, to  
20 the minimum necessary to accommodate the (~~residence~~) dwelling unit and necessary access;  
21 and

22                   e. The site for the (~~residence~~) dwelling unit, necessary access, and  
23 utilities has the least impact on the functions and values of the environmentally critical area.

1 2. A structure that is replaced and activities related to replacing the structure shall:

2 a. Comply with restrictions on flood hazard areas reconstruction, if the  
3 structure is located in a flood-prone area; and

4 b. Comply with the development standards for the environmentally critical  
5 area and buffer in which it is located to the maximum extent feasible, including requirements for  
6 access and shall comply with the standards in Sections 25.09.060, 25.09.065, and 25.09.070; and

7 c. Mitigate impacts to the functions and values of the environmentally  
8 critical area and buffers, in compliance with Section 25.09.065, including any impacts caused by  
9 removing the ((~~residence~~)) dwelling unit from its original location, runoff from impervious  
10 surfaces, and/or replacing any portion of the ((~~residence~~)) dwelling unit within the  
11 environmentally critical area or buffer.

12 Section ~~101~~ 102. A new Section 25.09.055 is added to the Seattle Municipal Code as  
13 follows:

14 **25.09.055 Essential public facilities**

15 If an essential public facility as defined in Section 23.84A.010 is proposed within an  
16 environmentally critical area as defined in Section 25.09.020, review of the proposed facility is  
17 subject to the provisions of Chapter 23.80.

18 Section ~~102~~ 103. Section 25.09.240 of the Seattle Municipal Code, last amended by  
19 Ordinance 126509, is amended as follows:

20 **25.09.240 Short subdivisions and subdivisions**

21 \* \* \*

22 C. Application submittal requirements. All short subdivision and subdivision applications,  
23 in addition to the application submittal requirements included in Title 23 and this Chapter 25.09,

1 shall include on the surveyed site plan the information required by this Section 25.09.240 (~~and~~  
2 ~~Section 25.09.260~~)), as applicable.

3 ~~((D. Development standards for new lots in neighborhood residential zones. If new lots~~  
4 ~~are created in neighborhood residential zones by short subdivision or subdivision, the following~~  
5 ~~development standards apply based on the area of each new lot that is outside the~~  
6 ~~environmentally critical areas listed in subsection 25.09.240.A, plus environmentally critical~~  
7 ~~areas in which development is allowed pursuant to subsections 25.09.240.B.1, 25.09.240.B.2,~~  
8 ~~and 25.09.240.B.3:~~

9 1. ~~Lot coverage and lot coverage exceptions according to subsections 23.44.010.C~~  
10 ~~and 23.44.010.D.~~

11 2. ~~Height limits according to Section 23.44.012, including the requirements of~~  
12 ~~subsection 23.44.012.A.3 if the area of the largest rectangle or other quadrilateral that can be~~  
13 ~~drawn within the lot lines of the new lot outside the environmentally critical areas is less than~~  
14 ~~3,200 square feet.~~

15 ~~E.))~~ D. Lots shall be configured to preserve the environmentally critical areas and buffers  
16 identified in subsection 25.09.240.A by:

17 1. Establishing a separate buffer tract or lot with each owner having an undivided  
18 interest; or

19 2. Establishing non-disturbance areas on individual lots.

20 ~~((F.))~~ E. The environmentally critical areas and buffers identified in subsection  
21 25.09.240.A, except for areas qualifying for development under subsections 25.09.240.B.1,  
22 25.09.240.B.2, and 25.09.240.B.3, shall be designated non-disturbance areas on the final plat. A  
23 statement that these non-disturbance areas are located on the lots and the definition of "non-

1 disturbance area" shall be recorded in the King County Recorder's Office along with the final  
2 plat in a form approved by the Director. At the same time, a covenant protecting non-disturbance  
3 areas shall be recorded as set out in Section 25.09.335.

4 ~~((G. In computing the number of lots a parcel in a neighborhood residential zone may  
5 contain, the Director shall exclude the following areas:~~

6 ~~1. The environmentally critical areas and buffers identified in subsection  
7 25.09.240.A, unless:~~

8 ~~a. The environmentally critical areas and buffers are on a lot that meets the  
9 provisions of subsection 25.09.240.B; or~~

10 ~~b. The applicant obtains an administrative conditional use under Section  
11 25.09.260, if it is not practicable to meet the requirements of subsection 25.09.240.B considering  
12 the parcel as a whole.))~~

13 Section ~~103~~ 104. Section 25.09.260 of the Seattle Municipal Code, last amended by  
14 Ordinance 126509, is repealed:

15 ~~((25.09.260 Environmentally critical areas administrative conditional use~~

16 ~~A. Administrative conditional use~~

17 ~~1. In neighborhood residential zones the Director is authorized to approve an  
18 environmentally critical areas administrative conditional use pursuant to Section 23.42.042 and  
19 this Section 25.09.260 for one or both of the following purposes:~~

20 ~~a. In calculating the maximum number of lots and units allowed on the  
21 entire parcel under subsection 25.09.240.G, the Director may count environmentally critical  
22 areas and/or buffers, except the open water area of a wetland or riparian corridor, that would~~

1 ~~otherwise be excluded, if an applicant is unable to demonstrate compliance with the requirements~~  
2 ~~of subsection 25.09.240.B for the entire parcel proposed to be subdivided.~~

3 ~~b. For the entire parcel proposed to be subdivided, the Director may~~  
4 ~~approve development of single family residences that meet the development standards of~~  
5 ~~subsection 25.09.260.B.3 and the platting conditions in subsections 25.09.260.B.1 and~~  
6 ~~25.09.260.C.2.b. Except as specifically superseded by the development standards of subsection~~  
7 ~~25.09.260.B.3 and the platting conditions of subsection 25.09.260.C.2.b, all applicable~~  
8 ~~regulations of Title 23 shall also apply to the entire parcel. The entire parcel is designated as the~~  
9 ~~site.~~

10 ~~2. Process. If an administrative conditional use application includes an application~~  
11 ~~to authorize development in a steep slope erosion hazard area or buffer, the application is not~~  
12 ~~required to include an application for the variances allowed under Sections 25.09.280 or~~  
13 ~~25.09.290, but the application must address the criteria listed in subsection 25.09.260.B.1.c.~~

14 ~~B. Criteria. An application under this Section 25.09.260 shall provide information~~  
15 ~~sufficient to demonstrate that the proposal meets the following criteria:~~

16 ~~1. Environmental impacts on environmentally critical areas and buffers~~

17 ~~a. No development is allowed in a biodiversity area or corridor, riparian~~  
18 ~~corridor, wetland, or wetland buffer.~~

19 ~~b. No riparian management area or wetland buffer is reduced.~~

20 ~~c. No development is on a steep slope erosion hazard area or its buffer unless~~  
21 ~~either the proposed development meets the criteria of subsections 25.09.090.B.2.a,~~  
22 ~~25.09.090.B.2.b, or 25.09.090.B.2.c or the property is a lot in existence as a legal building site~~



1 ~~prior to October 31, 1992, is predominantly characterized by steep slope erosion hazard areas,~~  
2 ~~and the following criteria are met:~~

3 ~~1) The proposed development shall be located away from steep~~  
4 ~~slope erosion hazard areas and buffers to the extent practicable.~~

5 ~~2) The Director shall require clear and convincing evidence that~~  
6 ~~the provisions of this subsection 25.09.260.B are met if development is located on steep slope~~  
7 ~~erosion hazard areas and buffers with these characteristics:~~

8 ~~a) A wetland over 1,500 square feet in size or a watercourse~~  
9 ~~designated part of a riparian corridor;~~

10 ~~b) An undeveloped area over 5 acres characterized by steep~~  
11 ~~slope erosion hazard areas; or~~

12 ~~c) Areas designated by the Washington Department of Fish~~  
13 ~~and Wildlife (WDFW) as biodiversity areas and corridors, or areas identified by the Director~~  
14 ~~with significant tree and vegetation cover providing wildlife habitat.~~

15 ~~3) If the application includes a proposal to develop in a steep slope~~  
16 ~~erosion hazard area or buffer, the development in the steep slope erosion hazard area or buffer~~  
17 ~~shall be the minimum necessary to achieve the number of single family dwelling units that would~~  
18 ~~be allowed on the original entire parcel according to the calculation for subdivision required~~  
19 ~~under subsection 25.09.240.G in the following order of priority:~~

20 ~~a) The proposal reduces the front and/or rear yards pursuant~~  
21 ~~to subsection 25.09.260.B.3.b.1 and complies with the building separation standards of~~  
22 ~~subsections 25.09.260.B.3.b.2 and 25.09.260.B.3.b.3;~~

1 ~~b) The proposal reduces the steep slope erosion hazard area~~  
2 ~~buffer; and~~

3 ~~c) The proposal intrudes into not more than 30 percent of~~  
4 ~~the steep slope erosion hazard area.~~

5 ~~d. The proposal protects WDFW priority species and maintains wildlife~~  
6 ~~habitat.~~

7 ~~e. The proposal does not result in unmitigated negative environmental~~  
8 ~~impacts pursuant to Section 25.09.065, including drainage and water quality, erosion, loss of~~  
9 ~~trees and vegetation, and slope stability on the identified environmentally critical area and buffer.~~

10 ~~f. The proposal promotes expansion, restoration, or enhancement of the~~  
11 ~~identified environmentally critical area and buffer.~~

12 ~~2. General environmental impacts and site characteristics~~

13 ~~a. The proposal minimizes potential negative effects of the development~~  
14 ~~on the undeveloped portion of the site and preserves topographic features.~~

15 ~~b. The proposal retains and protects trees and vegetation on designated~~  
16 ~~non-disturbance areas, protects stands of mature trees, minimizes tree removal, removes noxious~~  
17 ~~weeds and non-native vegetation and replaces this vegetation with native trees and vegetation,~~  
18 ~~and protects the visual continuity of treed and vegetated areas and tree canopy.~~

19 ~~3. Development standards~~

20 ~~a. The total number of single-family dwelling units permitted through the~~  
21 ~~environmentally critical areas conditional use regulations shall not exceed the number that would~~  
22 ~~be allowed based on compliance with the use regulations of Section 23.44.008, and the minimum~~  
23 ~~lot area standards of the underlying neighborhood residential zone, and shall be established only~~

1 ~~on the site comprised of the original entire parcel, with subdivision of the original entire parcel~~  
2 ~~allowed only as unit lots approved through the unit lot subdivision process in Section~~  
3 ~~25.09.260.C.2.b.2.~~

4 ~~b. Single family dwelling units shall be the sole type of principal use~~  
5 ~~permitted through the environmentally critical areas conditional use regulations and shall meet~~  
6 ~~the development standards of Chapter 23.44, except that the following standards apply instead of~~  
7 ~~the standards in Chapter 23.44, as applicable:~~

8 ~~1) Front and rear yards required by subsections 23.44.014.A and~~  
9 ~~23.44.014.B may be reduced to no less than 10 feet each and 30 feet for the sum of both yards if~~  
10 ~~the reduction would minimize or eliminate any intrusion into the steep slope erosion hazard area~~  
11 ~~or required buffer;~~

12 ~~2) Front and rear building separations between proposed single~~  
13 ~~family residences shall be a minimum of 25 feet;~~

14 ~~3) Side building separations shall be a minimum of 10 feet;~~

15 ~~4) The maximum lot coverage shall be calculated by deducting~~  
16 ~~required non-disturbance areas from total lot size; and~~

17 ~~5) Front, rear, and side separations shall be determined by the~~  
18 ~~Director, based on location of the building in relation to other buildings and the front lot line.))~~

19 **C. Conditions**

20 ~~1. In authorizing an administrative conditional use, mitigation pursuant to Section~~  
21 ~~25.09.065 shall apply to protect and mitigate negative impacts to biodiversity areas and~~  
22 ~~corridors, priority habitat and setbacks, riparian corridors, wetlands, wetland buffers, and steep~~  
23 ~~slope erosion hazard areas and buffers, and the Director may impose additional conditions to~~

1 ~~protect other properties that could be adversely affected in the zone or vicinity in which the~~  
2 ~~property is located.~~

3 ~~2. In addition to any conditions imposed under subsection 25.09.260.C.1, the~~  
4 ~~following conditions apply to all administrative conditional uses approved under this Section~~  
5 ~~25.09.260:~~

6 ~~a. Replacement and establishment of native trees and vegetation shall be~~  
7 ~~required where it is not possible to save trees and vegetation and shall comply with Section~~  
8 ~~25.09.070.~~

9 ~~b. If a subdivision or short subdivision is proposed, the following~~  
10 ~~standards apply:~~

11 ~~1) The development as a whole shall meet development standards~~  
12 ~~under Title 23 and this Chapter 25.09 applicable at the time the application is vested.~~

13 ~~2) A unit lot short subdivision or unit lot subdivision proposal shall~~  
14 ~~be required to ensure that the development standards of subsection 25.09.260.B.3 are~~  
15 ~~implemented for development. New unit lots created under this Section 25.09.260 shall be~~  
16 ~~approved through the unit lot subdivision regulations of Sections 23.22.062 and 23.24.045 and~~  
17 ~~by compliance with this Section 25.09.260. Development on individual unit lots, except as~~  
18 ~~otherwise set forth in this Section 25.09.260, may be nonconforming as to some or all of the~~  
19 ~~development standards.~~

20 ~~3) Subsequent platting actions or additions or modifications to~~  
21 ~~structures may not create or increase any nonconformity of the development as a whole to this~~  
22 ~~Chapter 25.09, and this shall be noted on the document creating the new unit lots that is recorded~~  
23 ~~with the King County Recorder's Office.~~



1 a. No reasonable alternative location will accommodate the facility or  
2 utility, as demonstrated by an analysis of appropriate alternative locations provided by the  
3 applicant or the Director;

4 b. Mitigation sequencing under Section 25.09.065 is applied to the siting,  
5 design, and construction of the facility or utility; ~~and~~

6 c. All requirements of subsections 25.09.300.A.1, 25.09.300.B,  
7 25.09.300.E, and 25.09.300.F apply; and

8 d. ~~((In granting an exception to the development standards in Sections  
9 25.09.090, 25.09.160, and 25.09.200 the Director shall apply the mitigation standards in Section  
10 25.09.065 when imposing any conditions))~~ A light rail transit facility within a light rail transit  
11 system with the alignment, transit station locations, and maintenance base locations approved by  
12 the Council by ordinance or resolution is exempt from subsection 25.09.300.A.2.a. For  
13 mitigation sequencing under Section 25.09.065, the light rail transit facility is exempt from  
14 subsection 25.09.065.B.1.a and the Director shall consider subsection 25.09.065.B.1.b, prioritize  
15 subsections 25.09.065.B.1.c, 25.09.065.B.1.e, and 25.09.065.B.1.f, and prioritize the extent to  
16 which the proposal creates improved ecological function. If mitigation for a light rail transit  
17 facility will change the location of a wetland and wetland buffer and/or riparian management  
18 area, the wetland buffer and riparian management area shall not extend into or past an improved  
19 right-of-way unless that portion of the riparian management area provides significant biological  
20 or hydrological function in relation to the wetland or riparian watercourse. The light rail transit  
21 facility is exempt from the submittal requirements of subsections 25.09.300.B.1.d and  
22 25.09.300.B.1.e.

23 \* \* \*

1 Section ~~105~~ 106. Section 25.09.520 of the Seattle Municipal Code, last amended by  
2 Ordinance 126685, is amended as follows:

3 **25.09.520 Definitions**

4 \* \* \*

5 "Department" means the Seattle Department of Construction and Inspections or its  
6 successor department.

7 "Detached dwelling unit" means a detached dwelling unit as defined in Section  
8 23.84A.008.

9 \* \* \*

10 (~~"Single family residence" means single family dwelling unit as defined in Section~~  
11 ~~23.84A.032 in the definition of "residential use."~~)

12 \* \* \*

13 Section ~~106~~ 107. Section 25.11.010 of the Seattle Municipal Code, last amended by  
14 Ordinance 126821, is amended as follows:

15 **25.11.010 Purpose and intent**

16 The purpose and intent of this Chapter 25.11 is to:

17 \* \* \*

18 E. Protect Tier 2 and Tier 3 trees and other trees that because of their unique historical,  
19 ecological, public health, or aesthetic value constitute an important community resource, and  
20 require flexibility in design to protect these trees;

21 \* \* \*

22 Section ~~107~~ 108. A new Section 25.11.025 is added to the Seattle Municipal Code as  
23 follows:

1 **25.11.025 Essential public facilities**

2 If this Chapter 25.11 applies to a proposal for an essential public facility as defined in Section  
3 23.84A.010, review of the proposed facility is subject to the provisions of Chapter 23.80.

4 Section ~~108~~ 109. Section 25.11.060 of the Seattle Municipal Code, last amended by  
5 Ordinance 127099, is amended as follows:

6 **25.11.060 Requirements for trees when development is proposed**

7 A. Tree protection area

8 1. A tree protection area is required for all existing Tier 1, Tier 2, and Tier 3 trees  
9 that are not removed during development, as well as any tree relocated offsite if on private  
10 property or any tree planted onsite as part of required mitigation pursuant to this Chapter 25.11.

11 2. The tree protection area for Tier 1, Tier 2, and Tier 3 trees shall be determined  
12 by the Director pursuant to this subsection 25.11.060.A and any rules promulgated by the  
13 Director.

14 3. The tree protection area may be modified (~~((from the basic tree protection area))~~)  
15 based on species tolerance; expected impacts of construction activities; tree size, age, and health;  
16 and soil conditions not to exceed the area of the feeder root zone. The Director may require  
17 Master Use Permits or building permits to include measures to protect tree(s) during  
18 construction, including within the feeder root zone.

19 4. The tree protection area may be reduced by the Director pursuant to the  
20 provisions of Title 23 and this Chapter 25.11, as follows, provided that the Director may approve  
21 ~~((further))~~ additional modifications (~~((from those))~~) not listed in this subsection 25.11.060.A.4, if  
22 the Director finds the modifications do (~~((that are shown))~~) not (~~((to))~~) interfere with the overall  
23 health and stability of the retained tree:



1 a. Any new encroachment into the tree protection area may not be closer  
2 than one half of the tree protection radius. Existing encroachments closer than one half of the  
3 tree protection radius may remain or be replaced if no appreciable damage to the tree will result.

4 b. The tree protection area shall not be reduced more than 35 percent of  
5 the outer half of the tree protection radius unless an alternative tree protection area or  
6 construction method will provide equal or greater tree protection and result in long-term  
7 retention and viability of the tree as determined by a certified arborist.

8 c. Existing encroachments do not count toward the reduction.

9 d. The tree protection area may be temporarily reduced in size during a  
10 specific construction activity that is not likely to cause appreciable damage to the tree.

11 Appropriate mitigation measures shall be implemented per ANSI A300 standards or their  
12 successor, and the tree protection area shall be returned to its permanent size after the specific  
13 construction activity is complete.

14 5. The tree protection area is required to include fencing, signage, and other safety  
15 requirements as required in the Seattle Department of Construction and Inspections Tree and  
16 Vegetation Protection Detail.

17 \* \* \*

18 Section ~~109~~ 110. Section 25.11.070 of the Seattle Municipal Code, last amended by  
19 Ordinance 127099, is amended as follows:

20 **25.11.070 Tree protection on sites undergoing development in Neighborhood Residential,**  
21 **Lowrise, Midrise, commercial, and Seattle Mixed zones**

22 A. Neighborhood Residential zones

23 1. Tier 2 trees may be removed only if:

1 a. The maximum lot coverage permitted on the site pursuant to Title 23  
2 cannot be achieved without extending into the ((basic)) tree protection area as modified or  
3 reduced altered pursuant to subsection 25.11.060.A.3 or subsection 25.11.060.A.4 Section  
4 25.11.060 or into a required front and/or rear yard to an extent greater than provided for in  
5 subsection 25.11.070.A.2;

6 b. Avoiding development in the ((basic)) tree protection area would result  
7 in a portion of a principal dwelling unit, or an accessory dwelling unit, being less than ((15)) 10  
8 feet in width; or

9 c. Tree removal is necessary for the construction of new structures;  
10 required vehicle access((and)) ; required pedestrian access((;)) ; or utilities, Director-required  
11 retaining walls, or other similar improvements associated with development.

12 2. For purposes of retaining an existing on-site and off-site Tier 1, Tier 2, Tier 3,  
13 or Tier 4 tree((;)) :

14 a. ((extension)) Extension into front or rear ((yards)) setbacks is permitted  
15 but limited to an area equal to the amount of the tree protection area of those trees not located  
16 within required ((yards)) setbacks. ((The maximum projection into the required front or rear yard  
17 shall be 50 percent of the yard requirement.))

18 b. ((reduction)) Reduction of required amenity areas is permitted but  
19 limited to an area equal to the amount of the tree protection area of trees retained.

20 3. ((If the maximum lot coverage permitted on the site can be achieved or a  
21 structure will be less than 15 feet in width without extending into required front and/or rear  
22 yards, then no such extension into required yards shall be permitted.)) Reserved.

23 4. For the purposes of this subsection 25.11.070.A:

1 a. Lot coverage calculation shall not include any portion of a parcel  
2 containing a biodiversity area or corridor, riparian corridor, priority habitat, priority area setback,  
3 wetland, wetland buffer, or steep slope erosion hazard area, unless the Director has approved  
4 critical areas reduction, waiver, or modification pursuant to Chapter 25.09; and

5 b. The ~~((basic))~~ tree protection area ~~((cannot))~~ may be ~~((modified))~~ altered  
6 by the Director pursuant to subsection 25.11.060.A.3 and subsection 25.11.060.A.4.

7 B. Lowrise, Midrise, commercial, and Seattle Mixed zones

8 1. Tier 2 trees may be removed as follows:

9 a. If an otherwise allowable development area of 85 percent cannot be  
10 achieved without extending into the ~~((basic))~~ tree protection area, as follows:

11 1) Calculate the ~~((basic))~~ tree protection area on the lot. For the  
12 purposes of this subsection 25.11.070.B, the ~~((basic))~~ tree protection area ~~((cannot))~~ may be  
13 ~~((modified))~~ altered by the Director pursuant to subsection 25.11.060.A.3 and subsection  
14 25.11.060.A.4.

15 2) Subtract the ~~((basic))~~ tree protection area and the area of any  
16 portions of the lot between a property line and ~~((basic))~~ tree protection area when the portion of  
17 the lot is 15 feet or less measured from a lot line to a ~~((basic))~~ tree protection area from the lot  
18 area. If this number is less than 85 percent of the total lot area, Tier 2 trees may be removed.

19 3) When multiple Tier 2 trees are located on a lot, the minimum  
20 number of trees needed to reach 85 percent may be removed based on the evaluation required by  
21 subsection 25.11.060.C.



1                                   3) Landscaping and screening may be reduced by a maximum of  
2 ~~((75))~~ 100 percent; and

3                                   4) Structure width, structure depth, and facade length limits, if  
4 applicable, may be increased by a maximum of ~~((30))~~ 50 percent.

5                                   b. The following Type I modifications to standards are permitted for  
6 development that: i) Receives public funding or an allocation of federal low-income housing tax  
7 credits; and ii) is subject to a regulatory agreement, covenant, or other legal instrument recorded  
8 on the property title and enforceable by The City of Seattle, Washington State Housing Finance  
9 Commission, State of Washington, King County, or other similar entity as approved by the  
10 Director of Housing; and iii) either: restricts at least 40 percent of rental units to occupancy by  
11 households earning no greater than 60 percent of median income, and controls the rents that may  
12 be charged for a minimum period of 40 years: or restricts at least 40 percent of ownership  
13 dwelling units earning no greater than 80 percent of median income, and controls the sale price  
14 of the units for a minimum period of 50 years:

15                                   1) Setback, separation, amenity area, landscaping, and screening  
16 requirements, if applicable, may be reduced by a maximum of 100 percent; and

17                                   2) Structure width, structure depth, and facade length limits, if  
18 applicable, may be increased by a maximum of 100 percent.

19                                   c. For development subject to design review, the departures permitted in  
20 Section 23.41.012.

21                                   d. Parking reduction. A reduction in the parking quantity required by  
22 Section 23.54.015 and the modification of standards for safe access of any required parking of

1 Section 23.54.030 may be permitted in order to protect a Tier 2 tree, if the reduction would result  
2 in a project that would avoid the tree protection area.

3 e. In Lowrise zones, for a principal structure with a base height limit of 40  
4 feet that is subject to the pitched roof provisions of subsection 23.45.514.D, the Director may  
5 permit the ridge of a pitched roof with a minimum slope of 6:12 to extend up to a height of 50  
6 feet if the increase is needed to accommodate, on an additional story, the amount of floor area  
7 lost by avoiding development within the tree protection area and the amount of floor area on the  
8 additional story is limited to the amount of floor area lost by avoiding development within the  
9 tree protection area.

10 3. Tree removal required for development to achieve the allowable development  
11 area according to subsection 25.11.070.B.1 or height limits of the applicable zone includes, but is  
12 not limited to, the construction of new structures~~((§))~~ ~~((vehicles))~~ ; required vehicle access((§)) ;  
13 ~~((and))~~ required pedestrian access((§)) ; or utilities, Director-required retaining walls, or other  
14 similar improvements associated with development.

15 Section ~~110~~ 111. Section 25.11.090 of the Seattle Municipal Code, last amended by  
16 Ordinance 126821, is amended as follows:

17 **25.11.090 Tree replacement, maintenance, and site restoration**

18 A. In all zones, Tier 1, Tier 2, and Tier 3 trees removed in association with development  
19 or because they are hazardous, infested by insects, pests, or pathogens, or an invasive or nuisance  
20 tree, or in accordance with the removal criteria in subsection 25.11.050.D, shall be replaced by  
21 one or more new trees, the size and species of which shall be determined by the Director; the tree  
22 replacement required shall be designed to result, upon maturity, in a canopy cover that is at least  
23 roughly proportional to the canopy cover prior to tree removal. Site restoration where there is on-

1 site tree replacement in association with development shall include the removal of all invasive  
2 vegetation and shall prohibit replacement with invasive species. When on-site replacement is  
3 proposed, such trees count toward the Green Factor under ~~((SMC))~~ Section 23.86.019 and  
4 private property tree point requirements under Section 23.44.120. When off-site replacement is  
5 proposed, preference for the location shall be on public property.

6 \* \* \*

7 Section ~~111~~ 112. Section 25.11.130 of the Seattle Municipal Code, last amended by  
8 Ordinance 126281, is amended as follows:

9 **25.11.130 Definitions**

10 \* \* \*

11 "Tree protection area" means the area surrounding a tree defined by a specified distance,  
12 in which excavation and other construction-related activities ~~((must be avoided))~~ are prohibited,  
13 unless ~~((approved))~~ pre-authorized by the Director. The tree protection area~~((s))~~ is delineated  
14 using a radius that is equal to one foot for every inch DSH of the tree, ~~((is variable depending on~~  
15 species, age and health of the tree, soil conditions, and proposed construction)) except as  
16 modified/ altered through subsections ~~25.11.060.A.3~~ or subsection ~~25.11.060.A.4~~.

17 ~~(("Tree protection area, basic" means the area surrounding a tree in which excavation and~~  
18 ~~other construction-related activities must be avoided unless approved by the Director. This area~~  
19 ~~is delineated using a radius that is equal to one foot for every inch DSH of the tree.))~~

20 \* \* \*

21 Section ~~112~~ 113. Ordinance 127219, implementing interim controls to comply with  
22 various state laws and attached to this ordinance as Attachment 3, is repealed. This ordinance  
23 shows Seattle Municipal Code sections common to both ordinances as if the repealed ordinance  
24 did not take effect.

1 Section ~~113~~ 114. This ordinance shall take effect as provided by Seattle Municipal Code  
2 Sections 1.04.020 and 1.04.070.

3 Passed by the City Council the \_\_\_\_\_ day of \_\_\_\_\_, 2025,  
4 and signed by me in open session in authentication of its passage this \_\_\_\_\_ day of  
5 \_\_\_\_\_, 2025.

6 \_\_\_\_\_  
7 President \_\_\_\_\_ of the City Council

Approved / returned unsigned / vetoed this \_\_\_\_\_ day of \_\_\_\_\_, 2025.

8 \_\_\_\_\_  
9 Bruce A. Harrell, Mayor

10 Filed by me this \_\_\_\_\_ day of \_\_\_\_\_, 2025.

11 \_\_\_\_\_  
12 Scheereen Dedman, City Clerk

13 (Seal)

- 14 Attachments:  
15 Attachment 1 – Map of Specific Rezone Areas  
16 Attachment 2 – Repealed Text of Chapter 23.44  
17 Attachment 3 – Ordinance 127219



## Changes included in Version 4a of CB 120993

Version 4a of CB 120993 includes additional technical edits as shown underlined below. In Version 4a of the bill, edits that were included in Version 3a are shown in **red** and edits that are new to Version 4a are in **purple**.

Pages	Lines	Bill Section	Code Section	Description
1	28	Title		Add references to new Sections 23.42.047 and 23.45.560, which were added to CB 120993 through Council amendments <u>Renumber Sections 23.80.006, 23.80.008, and 23.80.010 to 23.80.010, 23.80.012, and 23.80.014, to recognize new Sections 23.80.006 and 23.80.008, which were added through Ordinance 127228.</u>
20	8	16	23.34.011	Replace “an urban center, urban village” with “a regional center, an urban center” to reflect new Comprehensive Plan designations
26	7-17	21	23.44.022	Clarify relationship between different Accessory Dwelling Unit (ADU) size limits, <u>use standard language (“regardless of number of bedrooms”)</u> , remove redundancy, <u>fix numbering</u>
26	12-13	21	23.44.022	Add clarity regarding ADU legacy homeowner bonus (“any number of bedrooms”)
26	16-17	21	23.42.022	Replaced “purchased for value” with a \$1000 threshold for clear implementation of legacy homeowner amendment
31	7-8	23	23.42.047	Add serial comma Clarify applicability of provision Remove the words “right-of-way” from railroad right-of-way. Railroads don’t always run in separate “rights-of-way.”
48	15	30	23.44.040	Remove reference to modifying the standards of section 23.44.140 when a unit is added to a building. 23.44.140 explicitly does not apply when units are added to an existing structure. Clarify that the Director can modify the standards of subsections 23.44.160.A and 23.44.160.B – parking requirements and limits on off-site parking, but not development standards regarding where

Pages	Lines	Bill Section	Code Section	Description
				parking is located on the lot if parking is provided.
49	4-5	30	23.44.050	Remove redundant reference to an exception that is directly referenced in Table A for 23.44.050.
49-50	Table A	30	23.44.050	Sentences restructured for clarity and consistency. Consolidates FAR tables and removes redundancies for clarity. Increased family housing near schools bonus to maintain consistency with other amendments that have passed, ensuring that family housing does not have a lower FAR limit than other stacked flat development.
50	9	30	23.44.050	Update reference to FAR table
51	Table B	30	23.44.050	Remove Table B, which was made redundant by amendments to Table A.
51	3	30	23.44.050	Clarify that the higher FAR limit for stacked dwelling units applies to the lot.
51	4-6	30	23.44.050	<del>Remove redundant FAR limit, and add a reference to the FAR limit in Table A.</del> Maintain FAR limit in both Table A and this subsection, simplify language generally.
51	8-9	30	23.44.060	Add commas to clarify phrases
51	16-17	30	23.44.060	Add commas to clarify phrases
54	18-19	30	23.44.060	Add commas to clarify phrases
56	3-4	30	23.44.070	Clarify relationship between subsections A.1 and A.2.
56	3-23	30	23.44.070	Reorganize provisions to clarify regulations, move language that was added to subsection C to subsection A. This would allow pitched roofs and other features to be allowed above the height limit for projects that preserve trees, meet higher green factor scores, or achieve tree point scores. <u>Further reorganize into three subsections: A1: basic height limit of 32 feet; A2: height limit of 42 feet for certain types of development; A3: height limit is 12 feet for some accessory structures; fix numbering</u>

Pages	Lines	Bill Section	Code Section	Description
				<u>and capitalization. Move the word “or” to the end of subsection d from the end of subsection b</u>
56	13-20	30	23.44.070	Reconcile redundant provisions in subsections c and d, allowing 42 foot tall structures on lots where a Tier 1 or Tier 2 tree would be preserved or on lots that achieve a tree point score that would result in at least ten percent tree canopy coverage, and allowing 42 foot tall stacked dwelling units on lots that meet a Green Factor score of 0.6 or higher.
62	4-12	30	23.44.070	Move these provisions to subsection 23.44.070.A to maintain consistency with other height limits.
63	20-223	30	23.44.080	Clarify how lot coverage of ADA accessible units should be calculated in stacked dwellings, limit to projects with up to ten stacked dwelling units.
64	13-17	30	23.44.080	Add commas, remove dashes, and otherwise clarify sentence.
64-65	Table A	30	23.44.090	Rewrite table to provide clearer requirements. Apply setback requirements to nonresidential structures that are equivalent to requirements for denser residential structures. <u>Simplify language.</u> Continue to allow ADUs in rear setbacks.
<u>72-73</u>	<u>10, 1</u>	<u>30</u>	<u>23.44.090</u>	<u>Fix font size of title on exhibit</u>
73	3	30	23.44.090	Remove reference to specific tree types, which are listed in Section 25.11.070 to reduce confusion. Section 25.11.070 includes provisions that apply to more than Tier 1 and Tier 2 trees.
74	5-8	30	23.44.110	Clarify amenity area requirement for stacked flats versus attached/detached units.
75	20	30	23.44.110	Spell out a number
<u>80-81</u>	<u>9, 1</u>	<u>30</u>	<u>23.44.140</u>	<u>Fix font size of title on exhibit</u>
88	3	30	23.44.170	<u>Correct cross-reference</u>

Pages	Lines	Bill Section	Code Section	Description
95 96	12-23, 1-8	33	23.45.504	In LR zones, allow stores to be located on any lot to match amendments made to NR zones.
151	3	47	23.45.560	Correct reference
151	9 and 11	47	23.45.560	Capitalize
152	8	47	23.45.560	Add a period
153	Table A	47	23.45.560	Correct title Replace “urban centers and urban villages” with “regional centers and urban centers”
153	Table B	47	23.45.560	Correct title Remove redundant line in the table
<u>180</u> <u>181</u>	<u>17-18, 22</u> <u>1</u>	<u>55</u>	<u>23.51A.002</u>	<u>Incorporate changes from Ord. 127228</u>
<u>185</u>	<u>18-21</u>	<u>55</u>	<u>23.51A.002</u>	<u>Incorporate changes from Ord. 127228</u>
<u>206</u>	<u>3</u>	<u>59</u>	<u>23.54.015</u>	<u>Reflect adoption of Ord. 127228</u>
<u>207</u>	<u>19</u>	<u>59</u>	<u>23.54.015</u>	<u>Incorporate changes from Ord. 127228</u>
210	4	59	23.54.015	Clarify that this subsection applies to the change of use from a nonresidential use to a residential use
212 and 214	Table A - B.10.a & Footnote 4	59	23.54.015	Move requirement for loading and unloading space for pet daycare centers to footnote 4.
214	Table B – B	59	23.54.015	Use standard language
217	Table B Footnotes 2 and 3	59	23.54.015	Put amendatory language in the correct location, remove redundant language.
220 and 223	Table C – B and footnotes 3 and 4	59	23.54.015	Use standard language Edit and remove footnotes to reflect that child care would no longer have a parking requirement.
<u>229-</u> <u>231</u>	Table A	<u>59</u>	<u>23.54.037</u>	<u>Incorporate changes from Ord. 127228; fix references to footnotes</u>
<u>288</u>	<u>11-19</u>	<u>71</u>	<u>23.80.004</u>	<u>Incorporate changes from Ord. 127228</u>
<u>288</u>	<u>20</u>	<u>72</u>	<u>23.80.008</u>	<u>Add Section 23.80.008, which was enacted through Ord. 127228 to the bill as Section 72</u>
<u>290</u>	<u>11</u>	<u>72</u>	<u>23.80.008</u>	<u>Update cross-reference in new Section</u>
<u>291</u>	<u>4</u>	<u>72</u>	<u>23.80.008</u>	<u>Update cross-reference in new Section</u>
<u>294</u>	<u>9-11</u>	<u>72</u>	<u>23.80.008</u>	<u>Update cross-reference in new Section</u>

Pages	Lines	Bill Section	Code Section	Description
<u>288</u> on	Multiple	<u>73</u> on	Multiple	Update Bill Section numbers based on the new Section 72
<u>294</u>	<u>21-23</u>	<u>73</u>	Was <u>23.80.006</u> now <u>23.80.010</u>	Update code section number due to new Sections 23.80.006 and 23.80.008
<u>295</u>	<u>14-16</u>	<u>74</u>	Was <u>23.80.008</u> now <u>23.80.012</u>	Update numbering of code section number due to new Sections 23.80.006 and 23.80.008
<u>296</u>	<u>3-5</u>	<u>75</u>	Was <u>23.80.010</u> now <u>23.80.014</u>	Update numbering of code section number due to new Sections 23.80.006 and 23.80.008
<u>297</u>	<u>7-9</u>	<u>75</u>	<u>23.80.014</u>	Update code Section number
<u>298</u>	<u>1</u>	<u>75</u>	<u>23.80.014</u>	Update code Section number
<u>300</u>	<u>18</u>	<u>79</u>	<u>23.84A.010</u>	Update code Section number
<u>312-313</u>	<u>14-23, 1-2</u>	<u>85</u>	<u>23.84A.036</u>	Reflect adoption of Ord. 127285 Use lower-case letters Clarify RCW section
<u>314</u>	<u>1-2</u>	<u>87</u>	<u>23.84A.038</u>	Reflect adoption of Ord. 127228
<u>372</u>	<u>9</u>	<u>105</u>	<u>25.09.300</u>	Reflect adoption of Ord. 127228
<u>373</u> <u>374</u>	<u>5-22</u> <u>1-3</u>	<u>105</u>	<u>25.09.300</u>	Incorporate changes from Ord. 127228
<u>374</u>	<u>19</u>	<u>107</u>	<u>25.11.010</u>	Add a comma
<u>375</u>	<u>21-22</u>	<u>109</u>	<u>25.11.060</u>	Clarify language
<u>377</u>	<u>3-4</u>	<u>110</u>	<u>25.11.070</u>	Add the word “section” before a section number, update “yard” to “setback” Use consistent language across the tree regulations when referring to altering tree protection areas Remove a phrase “or into a required front and/or rear yard...” that was deleted under Amendment 102, but had inadvertently not been deleted in Version 3a
<u>377</u>	<u>9-10</u>	<u>110</u>	<u>25.11.070</u>	Clarify language
<u>377</u>	<u>13, 17</u>	<u>110</u>	<u>25.11.070</u>	Capitalize
<u>381</u>	<u>12-14</u>	<u>110</u>	<u>25.11.070</u>	Clarify language
<u>382</u>	<u>12-16</u>	<u>112</u>	<u>25.11.130</u>	Clarify language

<b>Pages</b>	<b>Lines</b>	<b>Bill Section</b>	<b>Code Section</b>	<b>Description</b>
				<u>Use consistent language across the tree regulations when referring to altering tree protection areas</u>



Legislation Text

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**File #:** Inf 2610, **Version:** 1

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City Council Agenda (2025)



# CITY OF SEATTLE

## City Council

### Agenda

#### Special Meeting\*

Tuesday, December 16, 2025

1:00 PM

Council Chamber, City Hall  
600 4th Avenue  
Seattle, WA 98104

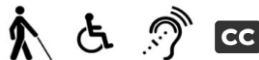
Sara Nelson, Council President  
Joy Hollingsworth, Member  
Debora Juarez, Member  
Robert Kettle, Member  
Eddie Lin, Member  
Alexis Mercedes Rinck, Member  
Maritza Rivera, Member  
Rob Saka, Member  
Dan Strauss, Member

Chair Info: 206-684-8809; [Sara.Nelson@seattle.gov](mailto:Sara.Nelson@seattle.gov)

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**CITY OF SEATTLE**  
**City Council**  
**Agenda**  
**Special Meeting\***  
**December 16, 2025 - 1:00 PM**

**Meeting Location:**

Council Chamber, City Hall, 600 4th Avenue, Seattle, WA 98104

**Committee Website:**

<http://www.seattle.gov/council>

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\*The City Council will meet in a special session from 1:00 p.m. to 2:00 p.m. At the conclusion of the special session, the City Council will move into its regularly scheduled 2:00 p.m. meeting. This is a combined agenda for the special and regular meeting.

Members of the public may register for remote or in-person Public Comment to address the Council. Speakers must be registered in order to be recognized by the Chair. Details on how to register for Public Comment are listed below:

Remote Public Comment - Register online to speak during the Public Comment period at <https://www.seattle.gov/council/committees/public-comment>. Online registration to speak will begin one hour before the meeting start time, and registration will end at the conclusion of the Public Comment period during the meeting.

In-Person Public Comment - Register to speak on the public comment sign-up sheet located inside Council Chambers at least 15 minutes prior to the meeting start time. Registration will end at the conclusion of the Public Comment period during the meeting.

Written comments must be submitted prior to 10 a.m. to ensure that they are distributed to Councilmembers prior to the start of the meeting. Comments may be submitted at [Council@seattle.gov](mailto:Council@seattle.gov) or at Seattle City Hall, Attn: Council Public Comment, 600 4th Ave., Floor 2, Seattle, WA 98104. Comments received after 10 a.m. will be distributed after the meeting to Councilmembers and included as part of the public record.

**A. CALL TO ORDER**

**B. ROLL CALL****C. PRESENTATIONS****D. PUBLIC COMMENT**

*Members of the public may sign up to address the Council for up to 2 minutes on matters on this agenda; total time allotted to public comment at this meeting is 20 minutes.*

**E. PUBLIC HEARING**[Inf 2813](#)

Public Hearing on Council Bill 121094 / Ordinance 127329, relating to negative use restrictions that limit a property's use a grocery store or pharmacy.

The Public Hearing will begin at 2 p.m. or at the conclusion of the public comment period, should it conclude after 2 p.m.

**F. ADOPTION OF INTRODUCTION AND REFERRAL CALENDAR:**

Introduction and referral to Council committees of Council Bills (CB), Resolutions (Res), Appointments (Appt), and Clerk Files (CF) for committee recommendation.

**G. APPROVAL OF THE AGENDA****H. APPROVAL OF CONSENT CALENDAR**

The Consent Calendar consists of routine items. A Councilmember may request that an item be removed from the Consent Calendar and placed on the regular agenda.

**Journal:****Bills:****Appointments:****GOVERNANCE, ACCOUNTABILITY, AND ECONOMIC DEVELOPMENT  
COMMITTEE:**

[Appt 03107](#)

Reappointment of Thomas J. Kelly as member, Joint Apprenticeship Training Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**

**Opposed: None**

[Appt 03108](#)

Reappointment of Lisa Reager as member, Joint Apprenticeship Training Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**

**Opposed: None**

[Appt 03437](#)

Reappointment of Jonathan Schirmer as member, Seattle Ethics and Elections Commission, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**

**Opposed: None**

**HOUSING AND HUMAN SERVICES COMMITTEE:**[Appt 03362](#)

Appointment of Zachary Dar as member, Seattle Disability Commission, for a term to October 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03363](#) Appointment of Linda W. Finkle as member, Seattle Disability Commission, for a term to October 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03364](#) Appointment of Sawyer Stearns as member, Seattle Disability Commission, for a term to April 30, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03365](#) Appointment of Sierra Miles as member, Seattle Women's Commission, for a term to July 1, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03366](#) Appointment of Sonia Hitchcock as member, Seattle Women's Commission, for a term to July 1, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03367](#) Appointment of Elizabeth Perez as member, Seattle Women's Commission, for a term to July 1, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03370](#) Reappointment of Martine Pierre-Louis as member, Pacific Hospital Preservation and Development Authority Governing Council, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03371](#) Appointment of Carl G. Harris as member, Seattle LGBTQ Commission, for a term to October 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03372](#) Appointment of Scott Humphreys as member, Seattle LGBTQ Commission, for a term to October 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03373](#) Appointment of Roger W. Long as member, Community Roots Housing Public Development Authority Governing Council, for a term to March 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03374](#) Appointment of Aaron Pritchard as member, Community Roots Housing Public Development Authority Governing Council, for a term to March 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03375](#) Appointment of Diane Pietrowski as member, Pacific Hospital Preservation and Development Authority Governing Council, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03376](#) Appointment of Brandon Monson as member, Seattle Human Rights Commission, for a term to July 22, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03377](#) Appointment of Julia Thompson as member, Seattle Disability Commission, for a term to April 30, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03390](#) Appointment of Leah Martin as member, Housing Levy Oversight Committee, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03391](#) Appointment of Yuvika Tolani as member, Housing Levy Oversight Committee, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**

**Opposed: None**

[Appt 03392](#) Reappointment of Febben Fekadu as member, Housing Levy Oversight Committee, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03393](#) Reappointment of Sunaree Marshall as member, Housing Levy Oversight Committee, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03394](#) Reappointment of Dan Wise as member, Housing Levy Oversight Committee, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03395](#) Appointment of Balvina E. Cortez as member, Seattle Disability Commission, for a term to April 30, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03396](#) Appointment of Naomi Snow as member, Seattle Disability Commission, for a term to April 30, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

- [Appt 03397](#) Appointment of Marc G. Cote as member, Seattle Disability Commission, for a term to April 30, 2027.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**  
**Opposed: None**
- [Appt 03398](#) Appointment of Elliott McMurray as member, Seattle Disability Commission, for a term to April 30, 2027.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**  
**Opposed: None**
- [Appt 03399](#) Appointment of Dominic Del Castillo as member, Seattle LGBTQ Commission, for a term to October 31, 2027.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**  
**Opposed: None**
- [Appt 03400](#) Reappointment of Kody L. Allen as member, Seattle LGBTQ Commission, for a term to October 31, 2027.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**  
**Opposed: None**
- [Appt 03401](#) Reappointment of Chris Curia as member, Seattle LGBTQ Commission, for a term to October 31, 2027.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka**  
**Opposed: None**



[Appt 03402](#) Reappointment of Jennifer Tran as member, Seattle Women's Commission, for a term to July 1, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03403](#) Reappointment of Kate Faoro Wright as member, Seattle Women's Commission, for a term to July 1, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

[Appt 03404](#) Appointment of Avery Hultgren as member, Seattle Human Rights Commission, for a term to July 22, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Juarez, Lin, Nelson, Rinck, Saka  
Opposed: None**

**LIBRARIES, EDUCATION, AND NEIGHBORHOODS COMMITTEE:**

[Appt 03417](#) Appointment of Gabriel de los Angeles as member, Community Involvement Commission, for a term to May 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin  
Opposed: None**

[Appt 03418](#) Appointment of Timothy Turner as member, Community Involvement Commission, for a term to May 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin  
Opposed: None**

[Appt 03419](#) Appointment of Chelsea Affleck as member, Community Involvement Commission, for a term to May 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03420](#) Appointment of Elise A. Herwig as member, Community Involvement Commission, for a term to May 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03421](#) Appointment of Nada Ramadan as member, Community Involvement Commission, for a term to May 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03422](#) Appointment of Jalen M. Smith as member, Community Involvement Commission, for a term to May 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03423](#) Appointment of Max Chandrasekaran as member, Seattle Youth Commission, for a term to August 31, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

- [Appt 03424](#) Appointment of Koreb Tadesse as member, Seattle Youth Commission, for a term to August 31, 2026.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**
- [Appt 03425](#) Appointment of Audrey Mai Trinen as member, Seattle Youth Commission, for a term to August 31, 2026.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**
- [Appt 03426](#) Appointment of Sean O'Brien as member, Pioneer Square Preservation Board, for a term to March 1, 2026.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**
- [Appt 03427](#) Appointment of Katrina Plewinski as member, Pioneer Square Preservation Board, for a term to March 1, 2028.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**
- [Appt 03428](#) Appointment of Catherine Walker as member, Pioneer Square Preservation Board, for a term to March 1, 2026.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**

[Appt 03429](#) Appointment of Maya Spotted Bear as member, Pioneer Square Preservation Board, for a term to March 1, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03430](#) Reappointment of Maureen R. Elenga as member, Pioneer Square Preservation Board, for a term to March 1, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03431](#) Reappointment of Sohyun Kim as member, Pioneer Square Preservation Board, for a term to March 1, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03432](#) Appointment of Joe Krentz as member, Ballard Avenue Landmark District Board, for a term to June 30, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03433](#) Reappointment of Anthony R. Salazar as member, Ballard Avenue Landmark District Board, for a term to June 30, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03434](#)

Appointment of Ras Peynado as member, Pike Place Market Historical Commission, for a term to December 1, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03435](#)

Appointment of Alex Rasmussen as member, Pike Place Market Preservation and Development Authority Governing Council, for a term to June 30, 2029.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

[Appt 03436](#)

Appointment of Matthew D. Zinski as member, Landmarks Preservation Board, for a term to August 14, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**

**Opposed: None**

**PARKS, PUBLIC UTILITIES, AND TECHNOLOGY COMMITTEE:**

[Appt 03405](#)

Reappointment of Katie Garrow as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03406](#)

Reappointment of Emily Hooper George as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03407](#)

Reappointment of Eric R. Howard as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03408](#)

Reappointment of Shawn Jackson as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03409](#)

Reappointment of Gerry Johnson as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03410](#)

Reappointment of Edward M. Leigh as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03411](#)

Reappointment of Pamela Quadros as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03412](#)

Reappointment of Jim Rowe as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03413](#)

Reappointment of Michelle A. Rusk as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03414](#)

Reappointment of Ryan Smith as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03415](#)

Reappointment of Donny Stevenson as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

[Appt 03416](#)

Reappointment of Leslie Veloz as member, Central Waterfront Oversight Committee, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

**PUBLIC SAFETY COMMITTEE:**

[Appt 03378](#)

Reappointment of Richard Greene as member, Public Safety Civil Service Commission, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 5 - Kettle, Saka, Hollingsworth, Juarez, Nelson**

**Opposed: None**

**SUSTAINABILITY, CITY LIGHT, ARTS AND CULTURE COMMITTEE:**



[Appt 03348](#) Appointment of Fatima Kabba as member, Green New Deal Oversight Board, for a term to April 30, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03349](#) Reappointment of Douglas E. Norberg as member, Museum Development Authority Governing Council, for a term to July 13, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03350](#) Reappointment of Stephanie Ellis-Smith as member, Museum Development Authority Governing Council, for a term to July 11, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03351](#) Reappointment of Robert Strong as member, Museum Development Authority Governing Council, for a term to July 11, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03352](#) Reappointment of Robert D. Kaplan as member, Museum Development Authority Governing Council, for a term to July 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03353](#) Reappointment of Rosita I. Romero as member, Museum Development Authority Governing Council, for a term to July 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03354](#) Appointment of Raymond Tymas-Jones as member, Museum Development Authority Governing Council, for a term to July 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03359](#) Appointment of Gina Cristina Sima as member, City Light Review Panel, for a term to April 11, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03360](#) Reappointment of Toyin Olowu as member, City Light Review Panel, for a term to April 11, 2026.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03361](#) Reappointment of Leo L. Lam as member, City Light Review Panel, for a term to April 11, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03379](#) Appointment of Leslie Bain as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03380](#) Appointment of Aaron Nathaniel Carr as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03381](#) Appointment of M. Angela Castañeda as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03382](#) Appointment of Gulgun Kayim as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03383](#) Appointment of Nick Ronan as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03384](#) Appointment of Obadiah Stephens-Terry as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2027.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03385](#) Reappointment of Nyema Clark as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

[Appt 03386](#) Reappointment of Afua Kouyate as member, Cultural Space Agency Public Development Authority Governing Council, for a term to December 31, 2028.

**The Committee recommends that City Council confirm the Appointment (Appt).**

**In Favor: 4 - Rinck, Lin, Saka, Strauss**

**Opposed: None**

## H. COMMITTEE REPORTS

*Discussion and vote on Council Bills (CB), Resolutions (Res), Appointments (Appt), and Clerk Files (CF).*

### CITY COUNCIL:

1. [CB 121126](#) AN ORDINANCE amending Ordinance 127156, which adopted the 2025 Budget; making appropriations from the General Fund for food programs and reserves; and ratifying and confirming certain prior acts; all by a 3/4 vote of the City Council.

### LIBRARIES, EDUCATION, AND NEIGHBORHOODS COMMITTEE:

2. [Appt 03294](#) Reappointment of Dwane Chappelle as Director of the Department of Education and Early Learning, for a term to August 1, 2029.  
**The Committee recommends that City Council confirm the Appointment (Appt).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**
3. [CB 121139](#) AN ORDINANCE relating to historic preservation; imposing controls upon the Stewart House, a landmark designated by the Landmarks Preservation Board under Chapter 25.12 of the Seattle Municipal Code, and adding it to the Table of Historical Landmarks contained in Chapter 25.32 of the Seattle Municipal Code.  
**The Committee recommends that City Council pass the Council Bill (CB).**  
**In Favor: 3 - Rivera, Rinck, Hollingsworth**  
**Opposed: None**  
**Abstain: 1 - Lin**
4. [CB 121140](#) AN ORDINANCE relating to historic preservation; imposing controls upon lîq'təd - Licton Springs Park, a landmark designated by the Landmarks Preservation Board under Chapter 25.12 of the Seattle Municipal Code, and adding it to the Table of Historical Landmarks contained in Chapter 25.32 of the Seattle Municipal Code.  
**The Committee recommends that City Council pass the Council Bill (CB).**  
**In Favor: 4 - Rivera, Rinck, Hollingsworth, Lin**  
**Opposed: None**

**GOVERNANCE, ACCOUNTABILITY, AND ECONOMIC DEVELOPMENT COMMITTEE:**

5. [CB 121073](#) AN ORDINANCE relating to the SODO Parking and Business Improvement Area; changing the assessment rate and exemptions; and amending Ordinance 125678, as previously amended by Ordinance 126191.  
**The Committee recommends that City Council pass as amended the Council Bill (CB).**  
**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**  
**Opposed: None**

6. [CB 121130](#) AN ORDINANCE relating to the Ethics Code; prohibiting political consultants from concurrently performing consulting work with the City; establishing a cooling-off period for political consultants; adding a new Section 4.16.072 to the Seattle Municipal Code; and amending Section 4.16.030 of the Seattle Municipal Code.
- The Committee recommends that City Council pass as amended the Council Bill (CB).**  
**In Favor: 3 - Nelson, Kettle, Rivera**  
**Opposed: 1 - Hollingsworth**  
**Abstain: 1 - Lin**
7. [CB 121141](#) AN ORDINANCE relating to City employment; exempting positions from the civil service system; returning positions to the civil service system; and amending Section 4.13.010 of the Seattle Municipal Code; all by a 2/3 vote of the City Council.
- The Committee recommends that City Council pass the Council Bill (CB).**  
**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**  
**Opposed: None**
8. [CB 121142](#) AN ORDINANCE relating to City employment; and establishing the Deferred Compensation Analyst classification title series and corresponding rates of pay in the Seattle City Employees' Retirement System.
- The Committee recommends that City Council pass the Council Bill (CB).**  
**In Favor: 5 - Nelson, Kettle, Hollingsworth, Lin, Rivera**  
**Opposed: None**

**SELECT COMMITTEE ON THE COMPREHENSIVE PLAN:**

9. [CB 120985](#) AN ORDINANCE relating to land use and zoning; repealing and replacing the Seattle Comprehensive Plan pursuant to a major update, with new goals, policies, and elements and a new Future Land Use Map; amending Sections 5.72.020, 5.72.030, 5.73.030, 6.600.040, 22.805.070, 23.34.007, 23.34.008, 23.34.009, 23.34.010, 23.34.011, 23.34.012, 23.34.014, 23.34.018, 23.34.020, 23.34.024, 23.34.028, 23.34.074, 23.34.076, 23.34.078, 23.34.080, 23.34.082, 23.34.086, 23.34.099, 23.34.100, 23.34.108, 23.34.110, 23.34.128, 23.40.070, 23.41.004, 23.41.012, 23.42.058, 23.44.019, 23.45.509, 23.45.510, 23.45.514, 23.45.516, 23.45.527, 23.45.530, 23.45.532, 23.45.550, 23.47A.004, 23.47A.005, 23.47A.008, 23.47A.009, 23.47A.012, 23.47A.013, 23.48.002, 23.48.021, 23.48.220, 23.48.221, 23.48.225, 23.48.245, 23.48.250, 23.48.285, 23.48.290, 23.48.602, 23.48.605, 23.48.610, 23.48.623, 23.48.690, 23.48.710, 23.48.720, 23.48.723, 23.48.740, 23.48.780, 23.48.785, 23.48.802, 23.48.905, 23.48.940, 23.49.012, 23.49.019, 23.49.036, 23.50.012, 23.50A.040, 23.50A.190, 23.50A.360, 23.51A.004, 23.52.004, 23.52.008, 23.53.006, 23.54.015, 23.54.016, 23.54.020, 23.54.035, 23.58A.014, 23.58A.024, 23.58A.040, 23.58A.042, 23.58B.040, 23.58B.050, 23.58C.040, 23.58C.050, 23.69.022, 23.69.026, 23.69.035, 23.71.020, 23.74.002, 23.84A.025, 23.84A.026, 23.84A.032, 23.84A.038, 23.84A.040, 23.84A.042, 23.86.006, 25.05.164, 25.05.665, and 25.05.800 of the Seattle Municipal Code; and amending the title of Sections 23.48.230, 23.48.235, 23.48.240, 23.48.255, and 23.48.280 of the Seattle Municipal Code.

**The Committee recommends that City Council pass as amended the Council Bill (CB).**

**In Favor: 8 - Hollingsworth, Kettle, Juarez, Nelson, Rinck, Rivera, Saka, Strauss**

**Opposed: None**

10. [CB 120993](#) AN ORDINANCE relating to land use and zoning; implementing a major update of Neighborhood Residential zones and modifying development standards in other zones to comply with various state laws; amending Chapter 23.32 of the Seattle Municipal Code at pages 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 98, 99, 100, 102, 103, 104, 105, 106, 107, 111, 112, 113, 114, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 216, 217, 219, 220, and 221 of the Official Land Use Map; amending Chapters 6.600, 14.08, 14.09, 15.32, 21.49, 22.214, 22.801, 22.907, 23.22, 23.24, 23.28, 23.30, 23.34, 23.42, 23.45, 23.47A, 23.48, 23.49, 23.50, 23.51A, 23.51B, 23.53, 23.54, 23.58C, 23.60A, 23.66, 23.72, 23.75, 23.76, 23.80, 23.84A, 23.86, 23.90, 23.91, 25.09, and 25.11 of the Seattle Municipal Code; renumbering existing subsection 23.54.015.K of the Seattle Municipal Code as Section 23.54.037 and further amending the section; renumbering existing subsections 23.54.030.F, 23.54.030.G, 23.54.030.K, and 23.54.030.L as Sections 23.54.031, 23.54.032, 23.54.033, and 23.54.034 and further amending the sections; repealing Chapter 23.44 and Sections 23.34.010, 23.34.012, 23.34.013, 23.34.072, 23.42.130, 23.45.512, 23.45.531, 23.86.010, and 25.09.260 of the Seattle Municipal Code; adding a new Chapter 23.44 and new Sections 23.42.024, 23.42.132, 23.45.519, 23.80.006, 23.80.008, 23.80.010, 25.09.055, and 25.11.025 to the Seattle Municipal Code; and repealing Ordinance 127219.

**The Committee recommends that City Council pass as amended the Council Bill (CB).**

**In Favor: 9 - Hollingsworth, Solomon, Kettle, Juarez, Nelson, Rinck, Rivera, Saka, Strauss**

**Opposed: None**



11. [Res 32183](#) A RESOLUTION relating to the One Seattle Plan Comprehensive Plan update; calling for additional measures by City departments to further advance the goals of the One Seattle Plan; and requesting that the Office of Planning and Community Development and other City departments develop additional amendments to the Comprehensive Plan in 2026.

**The Committee recommends that City Council adopt as amended the Resolution (Res).**

**In Favor: 9 - Hollingsworth, Solomon, Kettle, Juarez, Nelson, Rinck, Rivera, Saka, Strauss**

**Opposed: None**

**PARKS, PUBLIC UTILITIES, AND TECHNOLOGY COMMITTEE:**

12. [CB 121137](#) AN ORDINANCE amending the Amended Landscape Conservation and Infrastructure Program Funding Plan for the South Lake Union and Downtown as adopted by Ordinance 127252; amending Ordinance 127156, which adopted the 2025 Budget, including the 2025-2030 Capital Improvement Program; changing appropriations to various departments and budget control levels, and from various funds in the Budget; making an appropriation from the Park and Recreation Fund for related costs; and ratifying and confirming certain prior acts; all by a 3/4 vote of the City Council.

**The Committee recommends that City Council pass the Council Bill (CB).**

**In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**

**Opposed: None**

13. [CB 121138](#) AN ORDINANCE relating to Seattle Public Utilities; authorizing the General Manager/CEO of Seattle Public Utilities or designee to acquire by negotiation or eminent domain (condemnation) a parcel of land in Seattle, for utility purposes identified as King County Parcel Number 152504-9010 located at 4000 NE 41st St. and to execute, accept, and record deeds, conveyance documents, and agreements deemed by the General Manager/CEO to be necessary to this transaction on behalf of the City; placing the real property under the jurisdiction of Seattle Public Utilities; authorizing payment of all other costs associated with the acquisition; amending Ordinance 127362, which adopted the 2026 Budget including the 2026-2031 Capital Improvement Program; and ratifying and confirming certain prior acts; all by a 3/4 vote of the City Council.
- The Committee recommends that City Council pass the Council Bill (CB).**
- In Favor: 5 - Hollingsworth, Nelson, Kettle, Rivera, Strauss**
- Opposed: None**

#### I. ITEMS REMOVED FROM CONSENT CALENDAR

#### J. ADOPTION OF OTHER RESOLUTIONS

#### K. OTHER BUSINESS

Motion to adopt a 2026 Council President Pro Tem and succession starting January 1, 2026, and expire with the adoption of a 2026 and 2027 Council President Pro Tem Resolution. The Council President Pro Tem and succession will be in the following order: Councilmember Strauss, Councilmember Hollingsworth, Councilmember Kettle, Councilmember Rivera, Councilmember Saka, Councilmember Rinck, Councilmember Juarez, Councilmember Lin, and Councilmember Foster.

#### L. ADJOURNMENT